

SHEET NO.

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M3	TYPICAL TRAFFIC CONTROL PLAN--SINGLE LANE CLOSURE USING AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD)
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STANDARD PLANS

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922-R11	(2 SHEETS) TRAFFIC CONTROL FOR ASPHALT SURFACING
941-R1	(2 SHEETS) PAVEMENT MARKING
943	(4 SHEETS) TEMPORARY PAVEMENT MARKING

STATE OF NEBRASKA
DEPARTMENT OF TRANSPORTATION
PLANS FOR CONSTRUCTION
ASHLAND EAST
SAUNDERS COUNTY

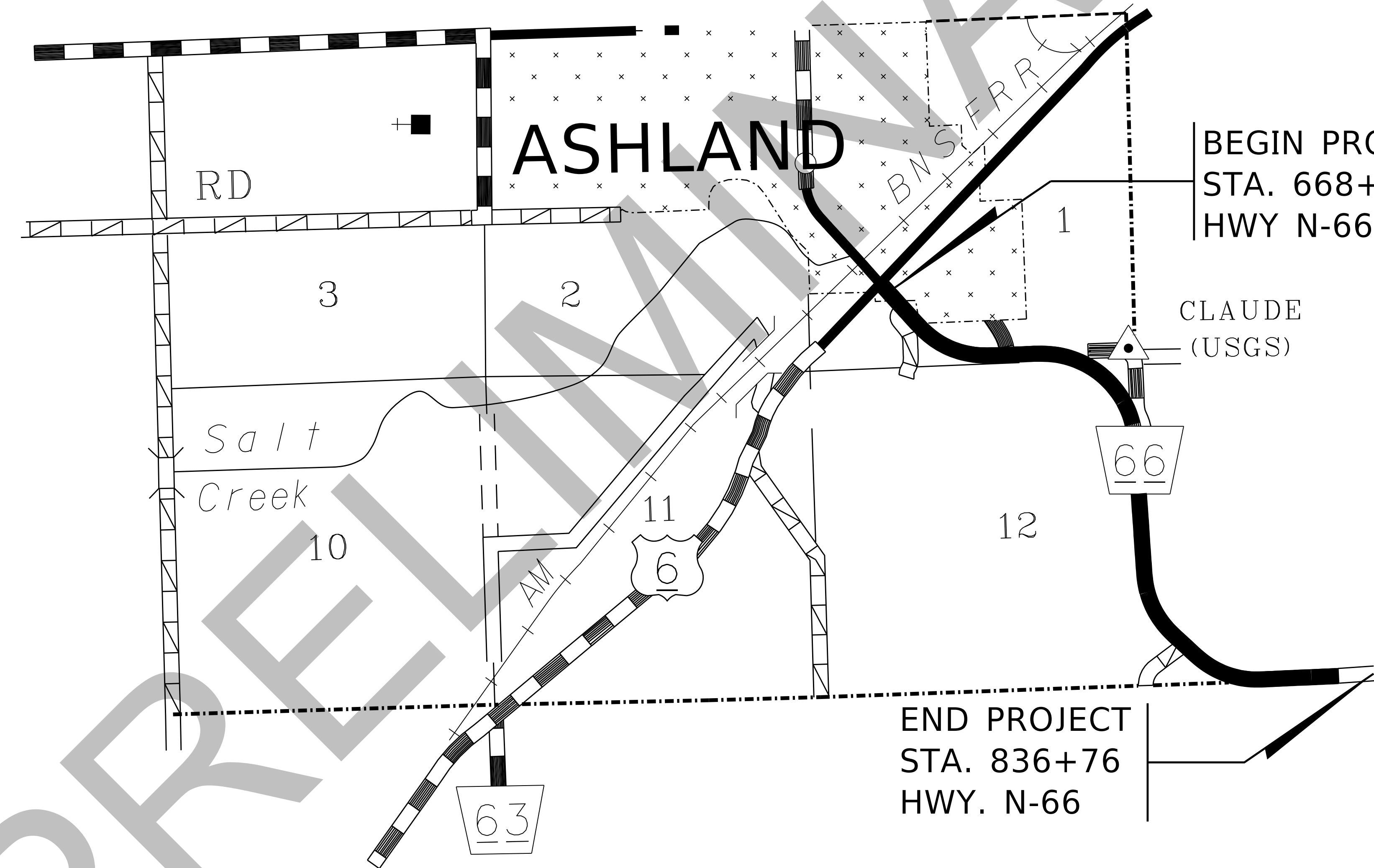
THE WORK ON THIS PROJECT CONSISTS OF GROUPS	
9 - BITUMINOUS & 10 - GENERAL	
▲ GROUPS 9 & 10 ARE INCLUDED IN THE LETTING OF SEPTEMBER 28, 2023	
▲ GROUPS _____ ARE INCLUDED IN THE LETTING OF _____	
■ GROUPS _____ ARE INCLUDED IN THE LETTING OF _____	

THE 2017 EDITION OF THE NEBRASKA STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS APPLY TO THIS PROJECT.

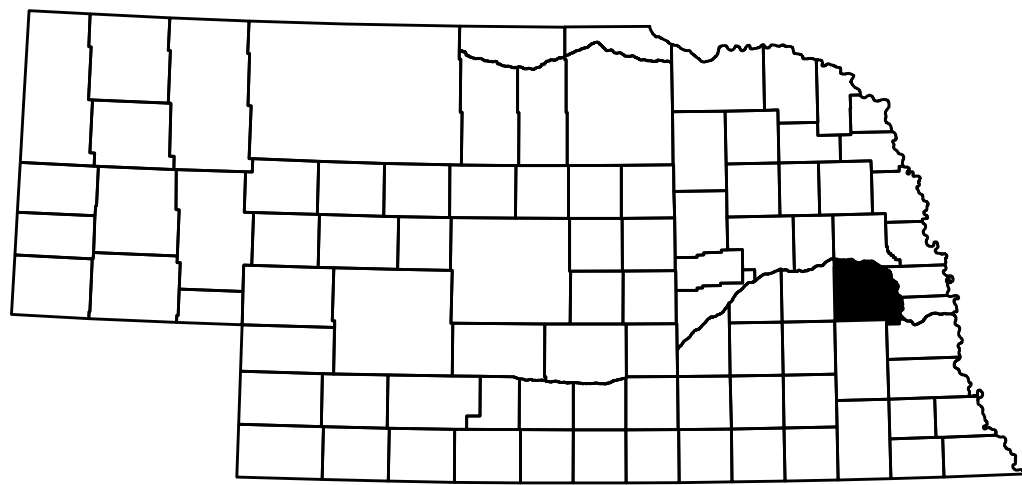
DESIGN DESIGNATION
MAINTENANCE
TRAFFIC
YEAR: 2024
ADT: 1720

T 12 N

R 10 E



Project Raw Materials (Tons)	11,620
Post Consumer Recycle Content in Project Raw Materials (Tons)	3,652
Post Consumer Recycle Content	31%
Estimated Value of Post Consumer Content Recycled	\$283,327



A1	
Project Number	STP-66-6(110)
C.N.	13392
▲ C.N.	
▲ C.N.	
■ C.N.	

TO REFERENCE POST NO. 100.96 3.12 MILES
 REFERENCE POST NO. 97.84 16473.6 FEET
 TOTAL NET LENGTH OF PROJECT:

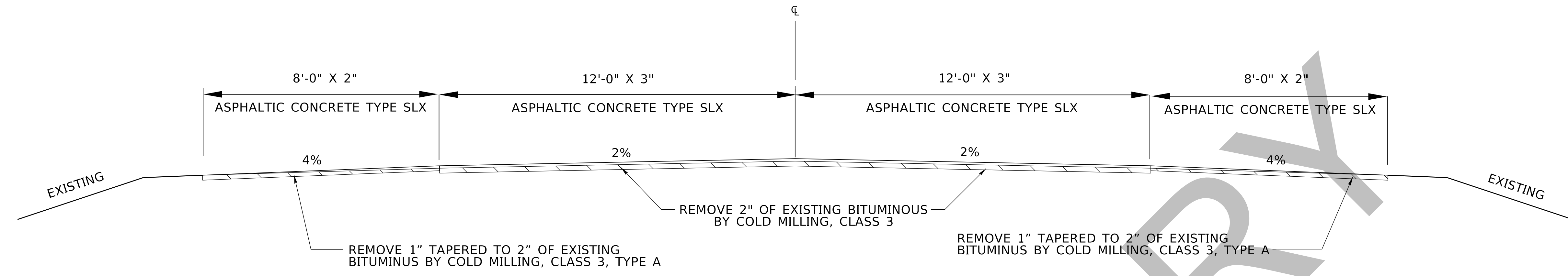


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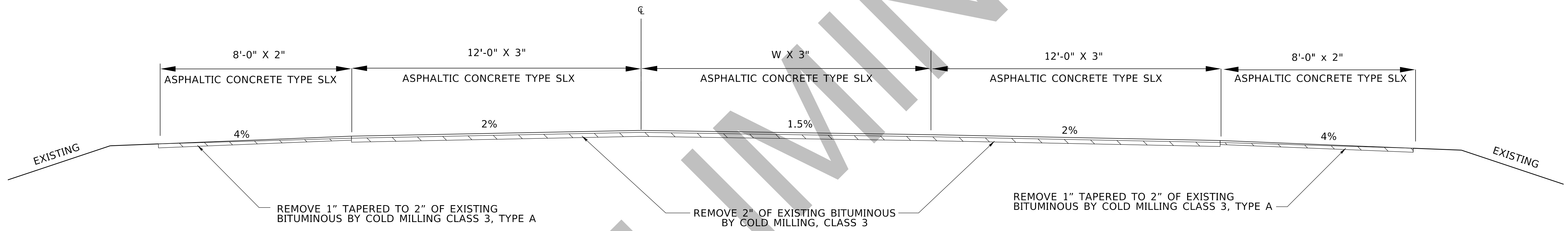
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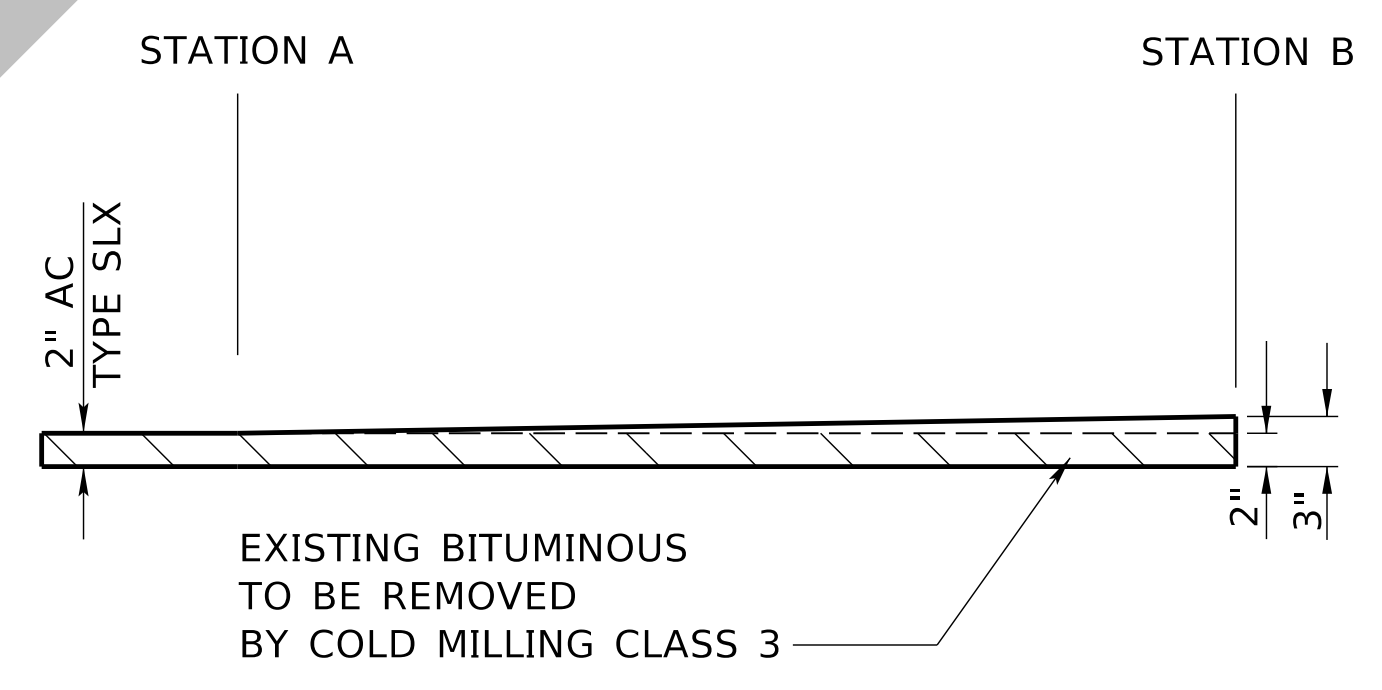
i. Mick Syslo, am the Coordinating Professional on the Ashland East project.



STATION TO STATION	
668+71	- 821+49



STATION TO STATION		W
821+49	- 828+40	0'-0" to 12'-0"
828+40	- 836+76	12'-0"



INLAY DETAIL

STATION A	STATION B
668+21 837+26	668+71 836+76

PRELIMINARY

C1

Project Number

66-6(110)

C.N. 13392

SUMMARY OF QUANTITIES

NEBRASKA
Good Life. Great Journey.
DEPARTMENT OF TRANSPORTATION

Roadway
Design
Division

PRELIMINARY

C2

Project Number

66-6(110)

C.N. 13392

SUMMARY OF QUANTITIES

NEBRASKA
Good Life. Great Journey.
DEPARTMENT OF TRANSPORTATION

Roadway
Design
Division

LEGEND

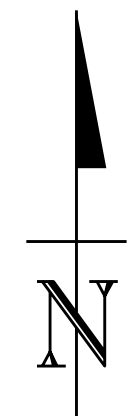
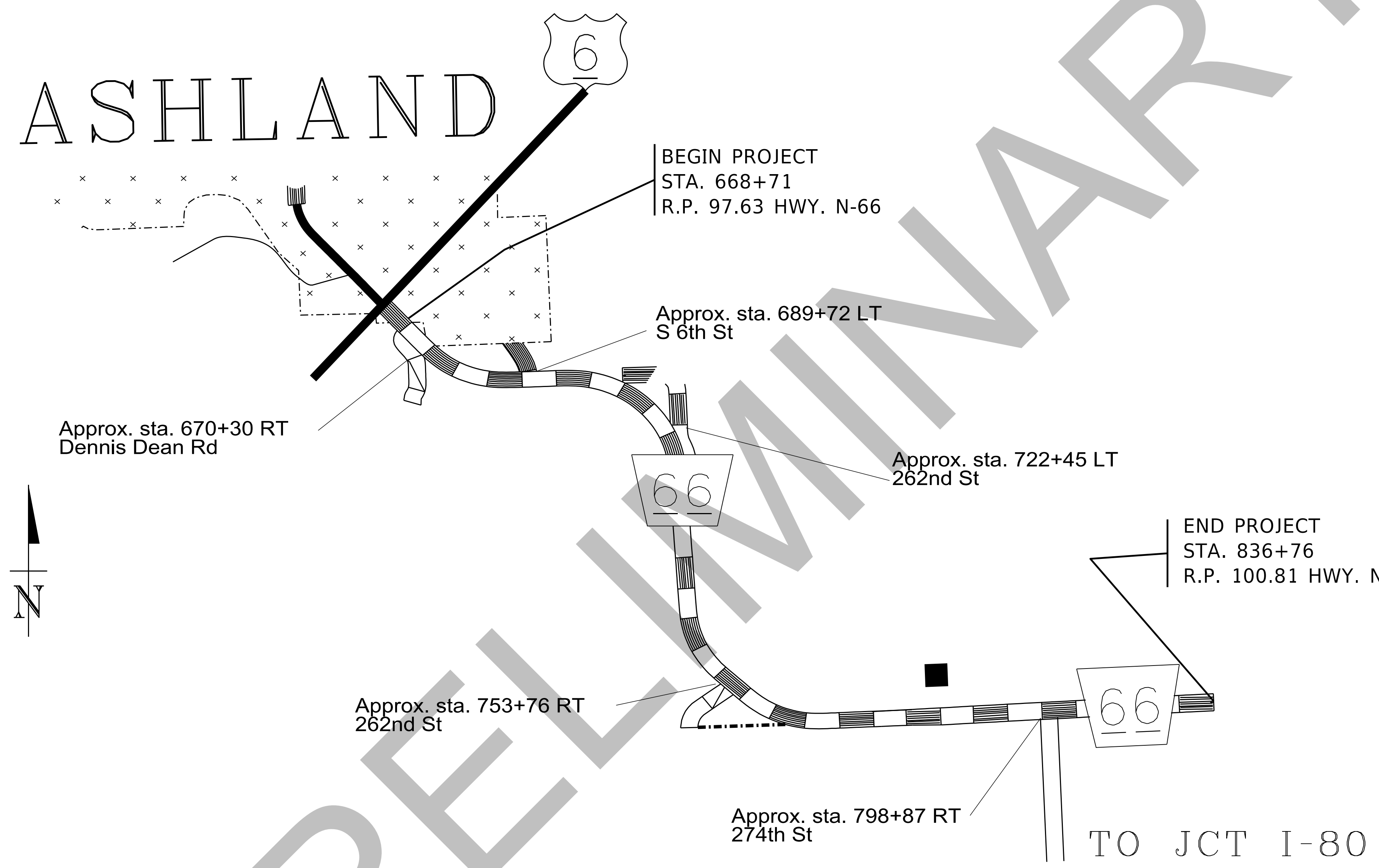
- G --- GAS LINE
- E --- ELECTRICAL SERVICE
- P --- POWER LINE
- OP --- OVERHEAD POWER LINE
- SAN --- SANITARY SEWER
- SS --- STORM SEWER
- T --- TELEPHONE LINE
- TFO --- FIBER OPTIC TELE. LINE
- OT --- OVERHEAD TELEPHONE LINE
- TV --- CABLE TV LINE
- OTV --- OVERHEAD CABLE TV LINE
- W --- WATER LINE
- o --- FENCE - CHAIN LINK
- x --- FENCE - R.O.W. OR WIRE
- □ --- FENCE - WOOD
- --- FLOWLINE
- --- CENTER LINE DRIVE
- ⊕ BENCH MARK
- ⊙ CENTER PIVOT
- CONTROL POINT
- ▭ CULVERT
- XXXXXX DIKE
- ⊙ GAS METER
- ⊗ GAS VALVE
- ⊕ GRID TICK
- ▬ GUARDRAIL
- GUARD POST
- ⊥ GUY POLE
- ⊥ GUY WIRE
- ⊗ LIGHT POLE
- MAILBOX
- ⊙ MANHOLE
- ▬ MARSH
- OIL WELL
- ⬡ PHOTO CODE POINT
- ⬡ POWER BOX
- ⬡ POWER POLE
- ⬡ POWER PULL BOX
- ⬡ PROPANE TANK
- ⊙ R.O.W. MARKER
- ⊗ ADVANCED R.R. WARNING SIGN
- ⊗ RAILROAD WARNING
- ▬ RAILROAD TRACKS
- ▬ RETAINING WALL
- ☾ SATELLITE DISH
- ⊙ SIGN
- ⊗ TRAFFIC SIGNAL
- ⊗ TRAFFIC SIGNAL/ST. LIGHT
- ⊕ TELEPHONE BOX
- ⊕ TELE. FIBER OPTICS BOX
- ⊕ TELEPHONE PULL BOX
- ⊕ TELEPHONE POLE
- ⊕ TELEVISION BOX
- TRAVELED WAY
- ☀ TREE - CONIFEROUS
- ☀ TREE - DECIDUOUS
- ⊕ TREE STUMP
- ⊕ WATER (FIRE) HYDRANT
- ⊕ WATER VALVE
- ⊕ WATER METER
- ⊕ WELL
- ⊕ WINDMILL

NOTES

- The locations of all aerial and underground utility facilities may not be indicated in these plans. Underground utilities, whether indicated or not will be located and flagged by the Utilities at the request of the Contractor.

No excavation will be permitted in the area of underground utility facilities until all such facilities have been located and identified to the satisfaction of all parties. The excavation must be accomplished with extreme care in order to avoid any possibility of damage to the utility facility.

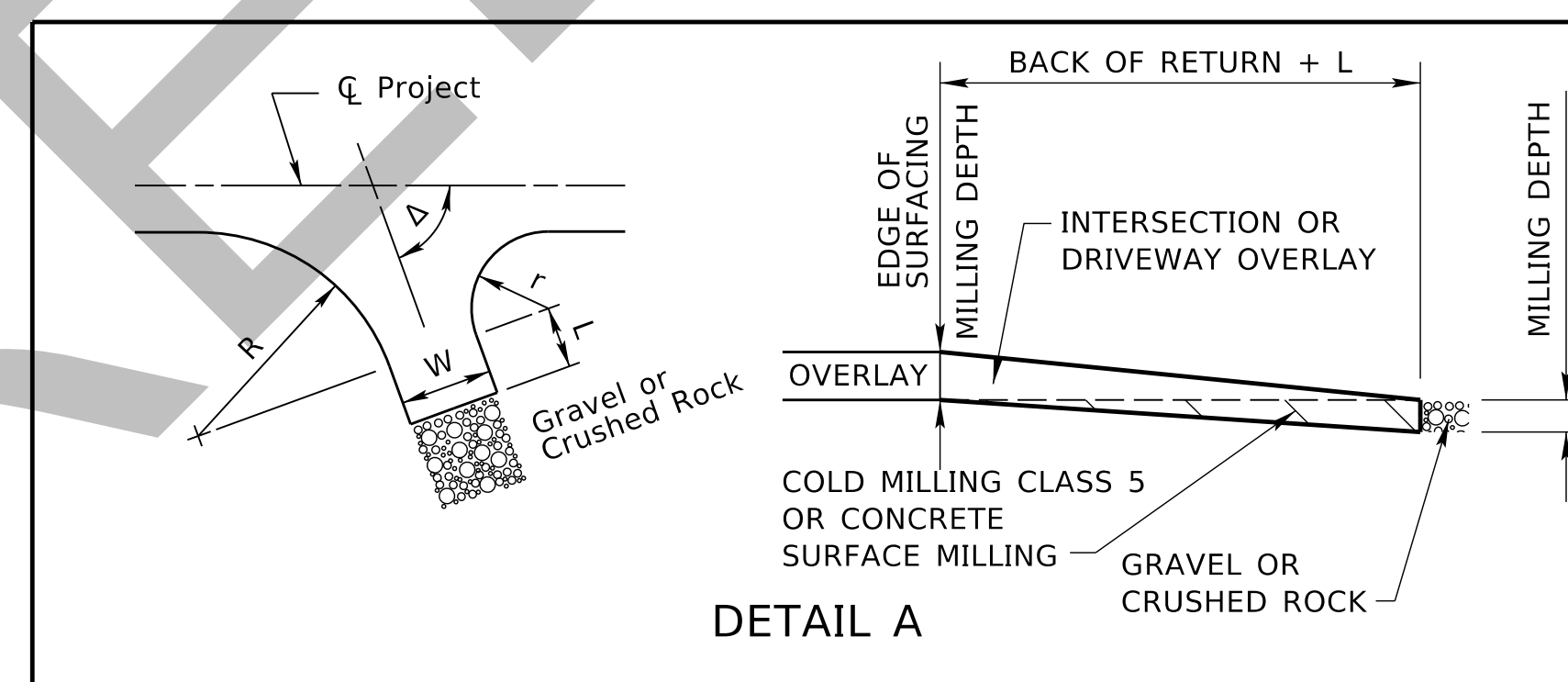
Stations taken from
As Built Project CN: 11880
and converted from meters to feet
1 m = 3.2808 ft



BUILD RUMBLE STRIPS, SPECIAL PLAN 1C						
STATION	TO	STATION	CENTERLINE	SHOULDER		EDGELINE
			(STA.)	(STA.)	SIDE	(STA.)
668+21	-	837+26	-	169	Lt.	Lt.
668+21	-	837+26	-	169	Rt.	Rt.
668+21	-	837+26	169			
TOTAL			169	338		

SURFACE DRIVEWAY, SEE DETAIL A															
STATION	SIDE	R (ft.)	r (ft.)	W (ft.)	L (ft.)	Δ (°)	PLACEMENT (SY)	ASPHALTIC CONCRETE TYPE	OVERLAY DEPTH	ASPHALTIC CONCRETE (TON)	MILLING DEPTH	COLD MILLING CLASS 5 (SY)	CONCRETE SURFACE MILLING (SY)	GRAVEL (CY)	CRUSHED ROCK (CY)
692+37	Lt.	25	-	24	0	*	102	SLX	2"	24	2"	102	-	-	-
697+24	Rt.	25	-	24	0		96	SLX	2"	11	2"	96	-	-	-
722+45	Rt.	49	-	24	0		100	SLX	2"	11	2"	100	-	-	-
730+06	Rt.	25	-	24	0		104	SLX	2"	11	2"	104	-	-	-
731+31	Lt.	25	39	24	0		128	SLX	2"	14	2"	128	-	-	-
737+83	Rt.	25	-	24	0		86	SLX	2"	23	2"	86	-	-	-
771+93	Rt.	25	-	24	0		93	SLX	2"	10	2"	93	-	-	-
782+51	Lt.	25	-	24	0		93	SLX	2"	10	2"	93	-	-	-
794+29	Rt.	25	-	24	0		93	SLX	2"	10	2"	93	-	-	-
819+42	Rt.	25	-	24	0		101	SLX	2"	11	2"	101	-	-	-
833+25	Lt.	25	25	64	0		222	SLX	2"	32	2"	222	-	-	-
833+77	Rt.	25	-	24	0		93	SLX	2"	10	2"	93	-	-	-

SURFACE INTERSECTION, SEE DETAIL A															
STATION	SIDE	R (ft.)	r (ft.)	W (ft.)	L (ft.)	Δ (°)	PLACEMENT (SY)	ASPHALTIC CONCRETE TYPE	OVERLAY DEPTH	ASPHALTIC CONCRETE (TON)	MILLING DEPTH	COLD MILLING CLASS 5 (SY)	CONCRETE SURFACE MILLING (SY)	GRAVEL (CY)	CRUSHED ROCK (CY)
670+30	Rt.	49	-	20	-	*	133	SLX	2"	11	1" to 2"	-	133	-	-
689+72	Lt.	49	-	24	-		245	SLX	2"	20	2"	245	-	-	-
722+45	Lt.	49	-	24	-		245	SLX	2"	20	2"	245	-	-	-
753+76	Rt.	49	-	24	-		318	SLX	2"	26	2"	318	-	-	-
798+87	Rt.	49	-	24	-		245	SLX	2"	20	2"	245	-	-	-



PRELIMINARY

M1

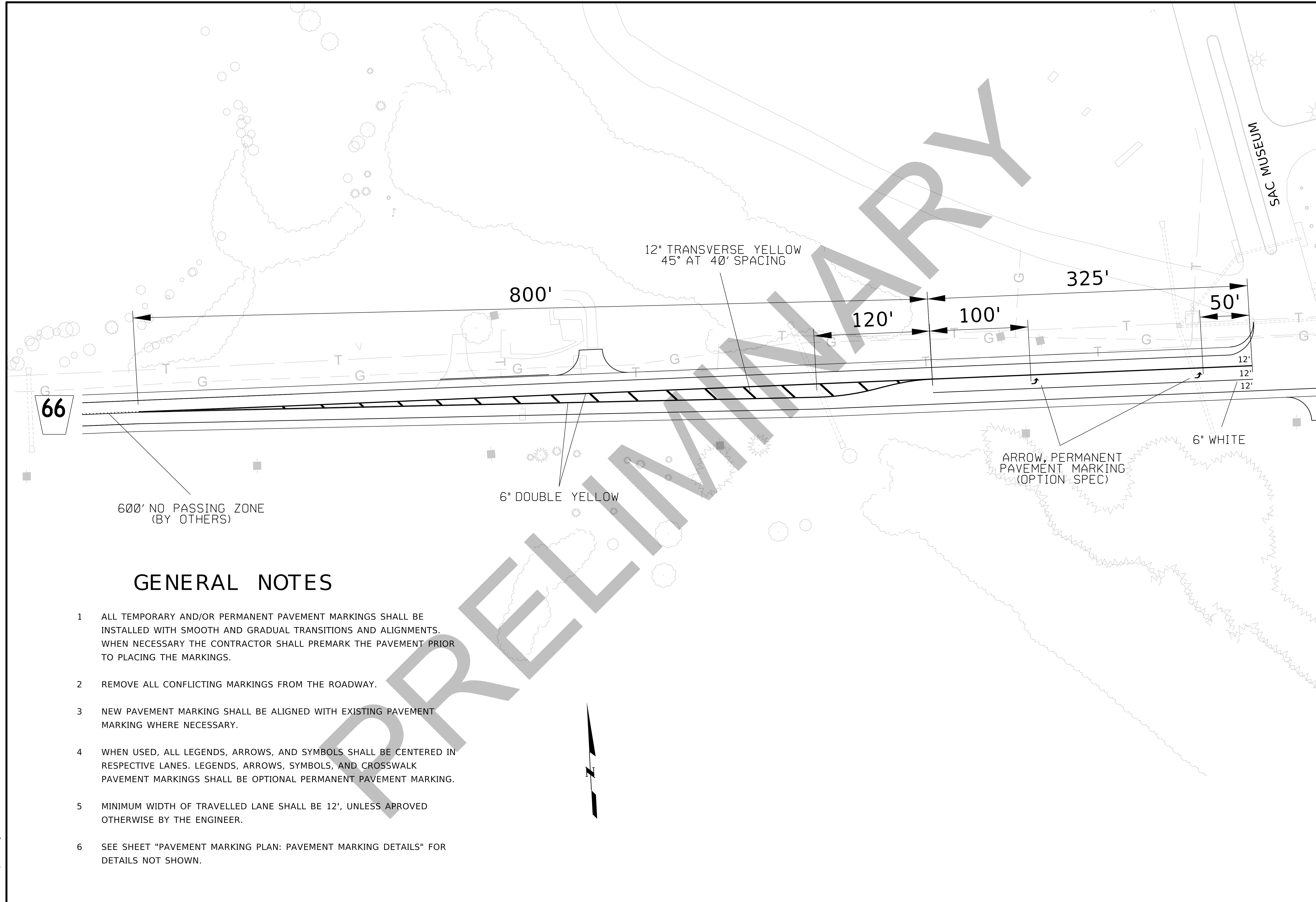
Project Number
66-6(110)

C.N. 13392

PAVEMENT MARKING PLAN
ASHLAND EAST
(WEST OF SAC MUSEUM)

DESIGNED BY AJM
DATE 04/23
NEBRASKA DEPARTMENT OF TRANSPORTATION - TRAFFIC ENGINEERING DIVISION

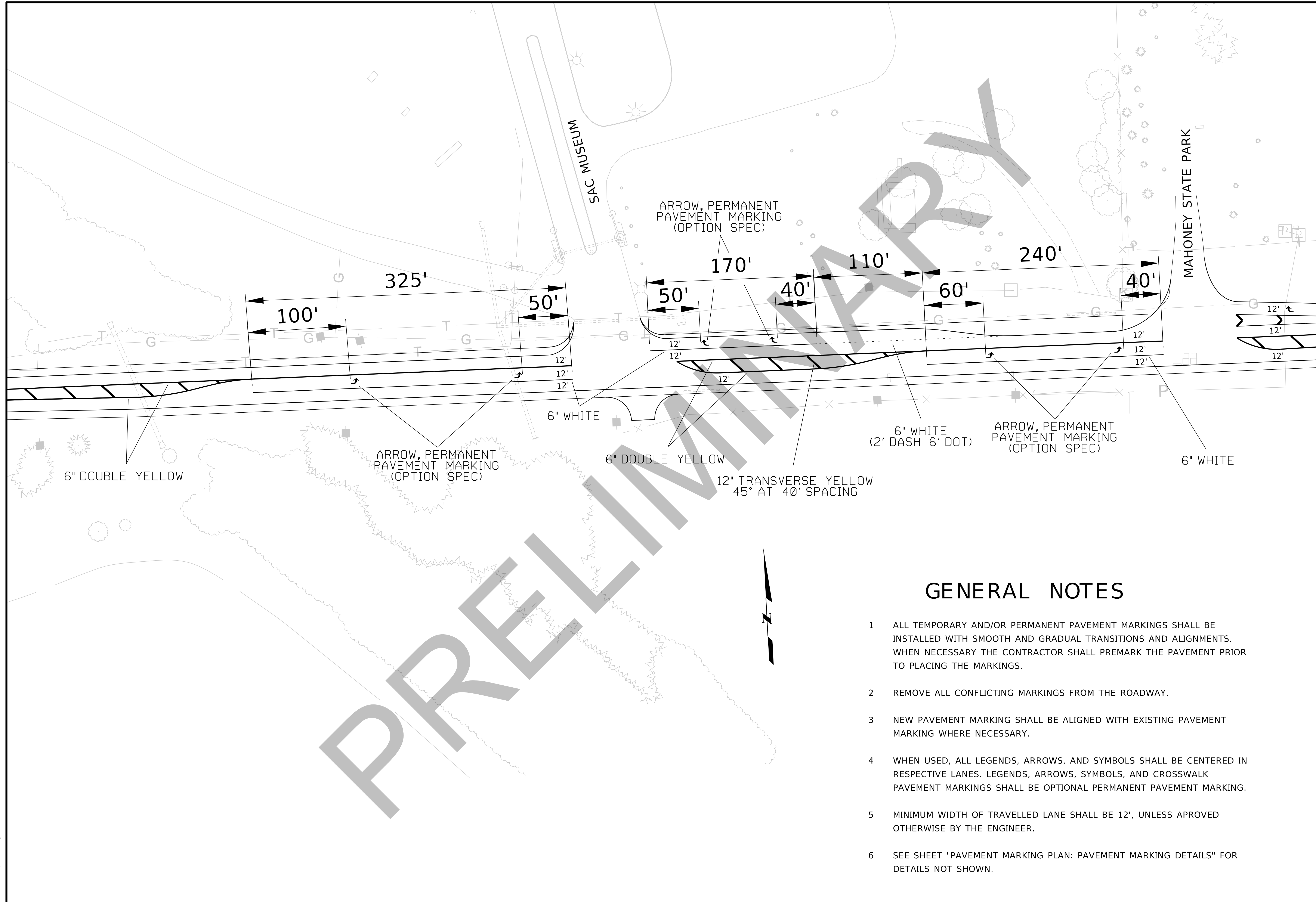
PLAN SHEET NUMBER
1
2



GENERAL NOTES

- 1 ALL TEMPORARY AND/OR PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED WITH SMOOTH AND GRADUAL TRANSITIONS AND ALIGNMENTS. WHEN NECESSARY THE CONTRACTOR SHALL PREMARK THE PAVEMENT PRIOR TO PLACING THE MARKINGS.
- 2 REMOVE ALL CONFLICTING MARKINGS FROM THE ROADWAY.
- 3 NEW PAVEMENT MARKING SHALL BE ALIGNED WITH EXISTING PAVEMENT MARKING WHERE NECESSARY.
- 4 WHEN USED, ALL LEGENDS, ARROWS, AND SYMBOLS SHALL BE CENTERED IN RESPECTIVE LANES. LEGENDS, ARROWS, SYMBOLS, AND CROSSWALK PAVEMENT MARKINGS SHALL BE OPTIONAL PERMANENT PAVEMENT MARKING.
- 5 MINIMUM WIDTH OF TRAVELLED LANE SHALL BE 12', UNLESS APPROVED OTHERWISE BY THE ENGINEER.
- 6 SEE SHEET "PAVEMENT MARKING PLAN: PAVEMENT MARKING DETAILS" FOR DETAILS NOT SHOWN.

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DATE: 3-APR-2023 08:50
FILE: p13392.dgn



GENERAL NOTES

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- 6 SEE SHEET "PAVEMENT MARKING PLAN: PAVEMENT MARKING DETAILS" FOR DETAILS NOT SHOWN.

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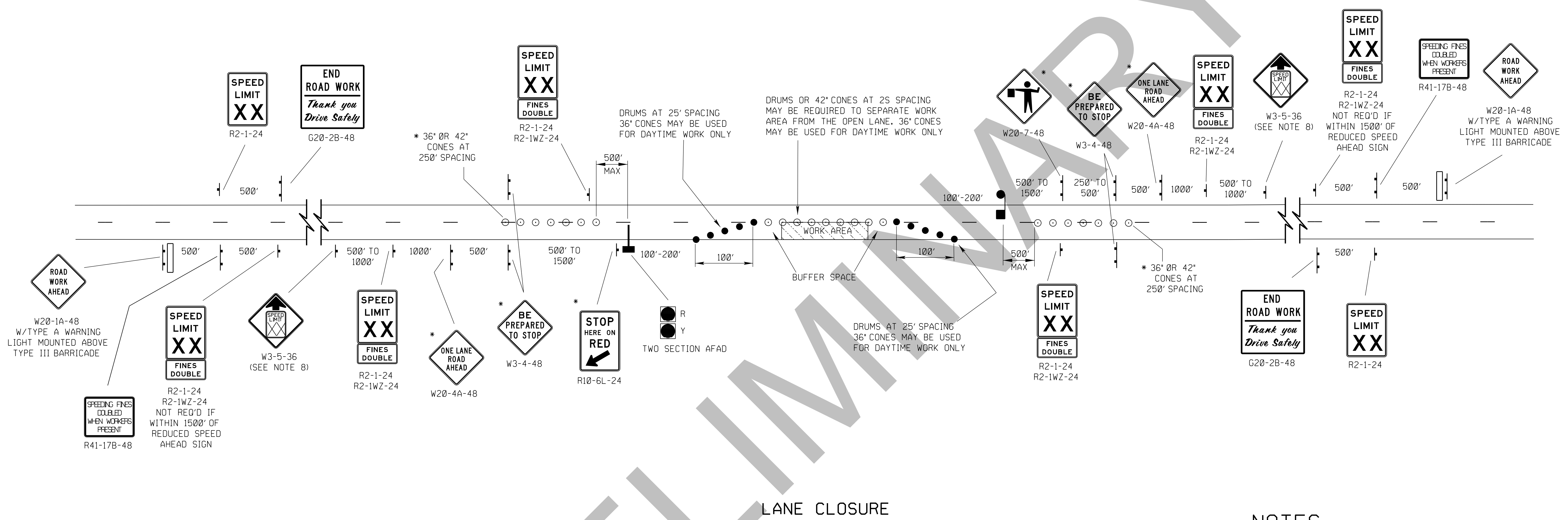
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DATE: 3-APR-2023 08:50

FILE: Flagger Assistance Device (AFAD).dgn



LANE CLOSURE

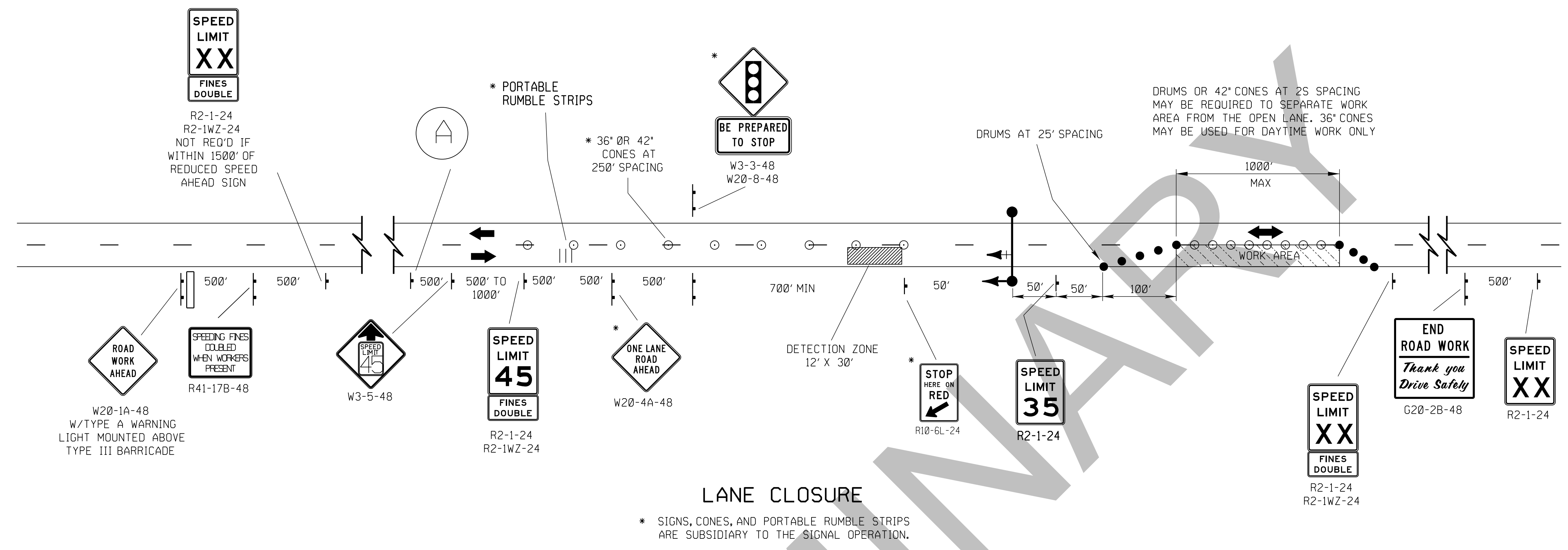
NOTES

* SIGNS AND CONES ARE SUBSIDIARY TO THE FLAGGING/AFAD OPERATION.

LEGEND

- FLAGGER
- REFLECTORIZED PLASTIC DRUM
- REFLECTORIZED PLASTIC DRUM OR 42" CONE
- TYPE III BARRICADE
- SINGLE POSTED SIGN
- DOUBLE POSTED SIGN
- AFAD

1. "FLAGGER AHEAD SYMBOL" SIGN (W20-7) SHALL BE USED WHEN A FLAGGER IS PRESENT, AND REMOVED WHEN NOT APPLICABLE.
2. THE CONTRACTOR SHALL INSTALL, MAINTAIN, AND REMOVE ALL SIGNS IN ACCORDANCE WITH THE DETAILS OF AND AT THE LOCATIONS SHOWN IN THE PLANS. SIGNS INSTALLED BY THE DEPARTMENT OF TRANSPORTATION OR OTHER GOVERNMENT AGENCY SHALL BE MAINTAINED AND REMOVED BY THEIR FORCES.
3. WHEN MESSAGE IS NOT PERTINENT, SIGNS SHALL BE TAKEN DOWN, COVERED OR FOLDED. TAPE IS NOT PERMITTED ON THE FACE OF THE SIGN.
4. VEHICLES OR EQUIPMENT SHALL NOT BE PARKED SO AS TO OBSCURE OR DISTRACT FROM TRAFFIC CONTROL DEVICES.
5. ORANGE FLAGS MAY BE USED TO CALL ATTENTION TO WARNING SIGNS.
6. REFER TO STANDARD PLAN 920 FOR GENERAL INFORMATION NOT SHOWN.
7. A MINIMUM OF 7-36" OR 42" CONES SHALL BE PLACED ON THE CENTERLINE IN ADVANCE OF THE FLAGGER. THE CONES SHOULD BE SPACED AT 250 FEET.
8. THE SPEED IN FLAGGING/PILOT CAR OPERATIONS IS GENERALLY CONTROLLED BY THE PILOT CAR, A SPEED REDUCTION MAY NOT BE NECESSARY IF THE WORK ZONE CONDITIONS WILL NOT EXIST UPON COMPLETION OF EACH DAYS WORK. W3-5 SIGN IS NOT NEEDED IF SPEED LIMIT IS NOT REDUCED.



GENERAL NOTES

- SIGNS SHOWN ARE USUALLY FOR ONE DIRECTION OF TRAVEL ONLY.
- THE CONTRACTOR SHALL INSTALL, MAINTAIN, AND REMOVE ALL SIGNS IN ACCORDANCE WITH THE DETAILS OF AND AT THE LOCATIONS SHOWN IN THE PLANS. SIGNS INSTALLED BY THE DEPARTMENT OF TRANSPORTATION OR OTHER GOVERNMENT AGENCIES SHALL BE MAINTAINED AND REMOVED BY THEIR FORCES.
- WHEN MESSAGE IS NOT PERTINENT, SIGNS SHALL BE TAKEN DOWN, COVERED OR FOLDED. TAPE WILL NOT BE PERMITTED ON THE FACE OF THE SIGN.
- ALL BARRICADE AND SIGN LOCATIONS ARE APPROXIMATE, AND MAY BE ADJUSTED TO FIT FIELD CONDITIONS. THE SIGNS SHALL BE INSTALLED SO AS TO NOT OBSCURE THE VIEW OF OTHER TRAFFIC CONTROL DEVICES FROM MOTORISTS.
- VEHICLES OR EQUIPMENT SHALL NOT BE PARKED AS TO OBSCURE OR DISTRACT FROM TRAFFIC CONTROL DEVICES.
- "NO PASSING ZONES NOT MARKED" SIGN (W25-6-48) SHOULD BE INSTALLED AT EACH END OF THE PROJECT WHENEVER THE EXISTING NO PASSING ZONE PAVEMENT MARKINGS HAVE BEEN REMOVED OR COVERED AND NO PASSING ZONE PAVEMENT MARKINGS ARE NOT INCLUDED IN THE PROJECT.
- SPEED LIMIT SIGN IS NOT REQUIRED IF WITHIN 1500 FT OF A REDUCED SPEED AHEAD SIGN.
- WORK ZONE SPEED LIMITS SHALL NOT BE INSTALLED WITHOUT A SPEED ZONE AUTHORIZATION COMPLETED BY THE DEPARTMENT. THE WORK ZONE SPEED LIMIT SHALL BE ESTABLISHED ACCORDING TO DOT-01 60-18. SEE WORK ZONE SPEED LIMIT NOTES ON STANDARD PLAN 920.
- A SPEED LIMIT SIGN ENDING THE REDUCED SPEED ZONE SHALL BE INSTALLED AT THE END OF EACH ZONE.
- PLACE TYPE II BARRICADES, REFLECTORIZED PLASTIC DRUMS, OR 42" CONES ON THE TRAFFIC SIDE OF THE DROP-OFF WHERE SUFFICIENT LATERAL DISTANCE EXISTS BETWEEN THE TRAVEL LANE AND THE DROP-OFF (DROP-OFF DETAIL ON STANDARD PLAN 922).
- THE LEAD SIGNS ARE NOT NEEDED IF TWO PROJECTS ARE LESS THAN 1 MILE APART. THE "END CONSTRUCTION" SIGN (G20-28-48) SHOULD NOT BE INSTALLED BETWEEN THE PROJECTS.
- ON ARMOR COAT SURFACING, A "LOOSE GRAVEL" SIGN (W8-7-36) IS REQUIRED AT THE BEGINNING OF THE DAYS WORK AND SHALL REMAIN IN PLACE UNTIL THE LOOSE GRAVEL HAS BEEN SWEEPED OFF.
- SIGN SIZES SHOWN ARE FOR TYPICAL SITUATIONS- REFER TO NEBRASKA SUPPLEMENT TO THE MUTCD FOR FURTHER SIZE INFORMATION.
- REFER TO STANDARD PLAN 920 FOR GENERAL INFORMATION NOT SHOWN.
- A MINIMUM OF 7-36" OR 42" CONES SHALL BE PLACED ON CENTERLINE IN ADVANCE OF THE TEMPORARY SIGNAL. THE CONES SHOULD BE SPACED AT 250 FEET.

LEGEND

- FLAGGER
- REFLECTORIZED PLASTIC DRUM
- REFLECTORIZED PLASTIC DRUM OR 42" CONE
- TYPE III BARRICADE
- SINGLE POSTED SIGN
- DOUBLE POSTED SIGN
- TRAFFIC SIGNAL

TAPER FORMULA

$L = S \times W$ FOR SPEEDS OF 45 MPH OR MORE.
 $L = \frac{WS^2}{60}$ FOR SPEEDS OF 40 MPH OR LESS.

WHERE:
 L = MINIMUM LENGTH OF TAPER.
 S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK.
 W = WIDTH OF OFFSET (LANE WIDTH).

- FRESH OIL
W21-2-36
- REDUCE SPEED LOOSE GRAVEL ON SURFACE
W8-7C-48
- MILLED SURFACE
W41-24-36
- LOOSE GRAVEL
W8-7-36 WHERE REQUIRED BY THE ENGINEER

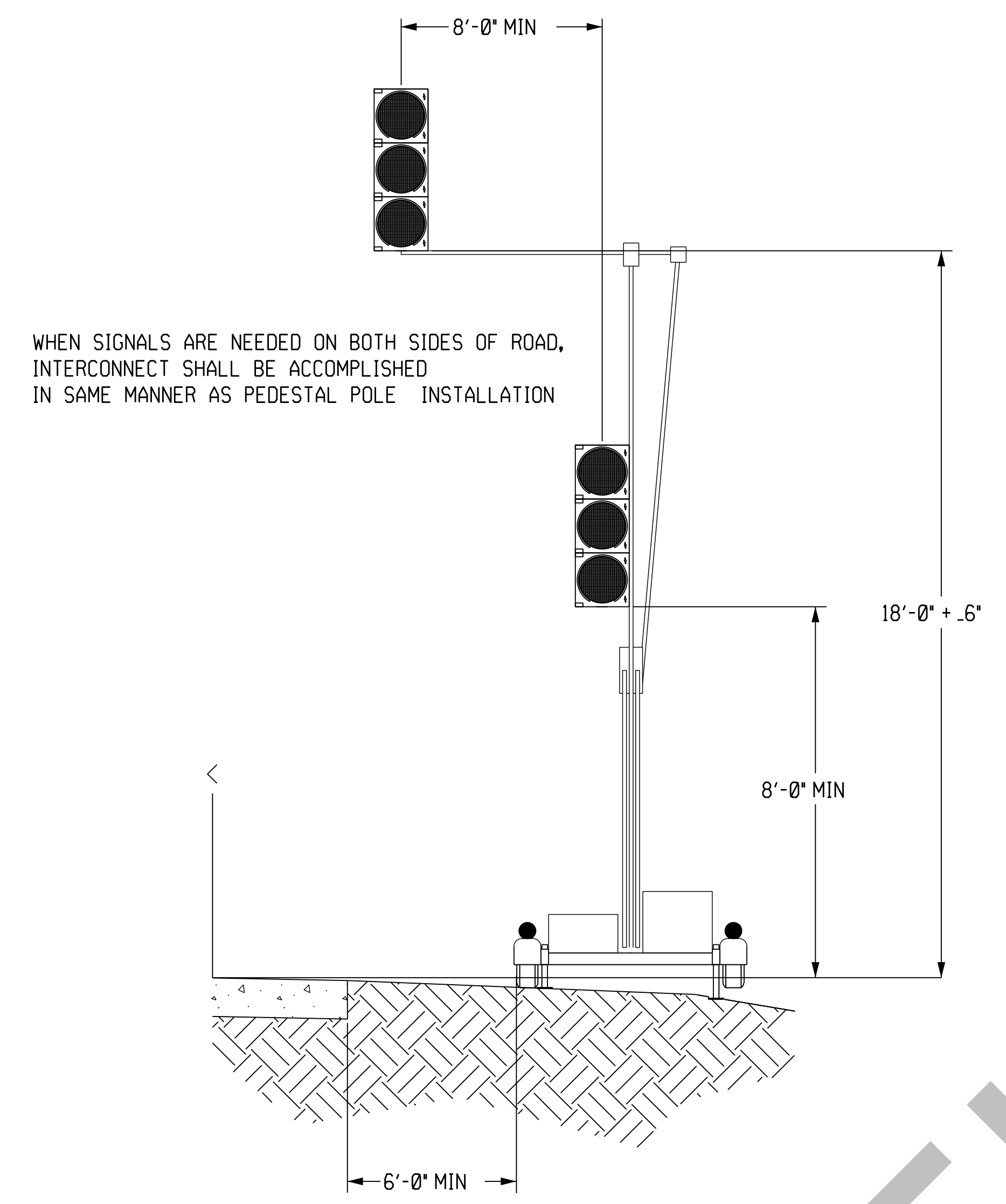
TABLE A: TEMPORARY TRAFFIC SIGNAL TIMING

RECOMMENDED SETTINGS (SEC.)	DISTANCE BETWEEN "STOP HERE ON RED" SIGN LOCATIONS (FT)	ALL RED (SEC.)
INITIAL GREEN - 15	500' - 700'	19
EXTENSION - 2.5	700' - 850'	24
MAX. GREEN - 45	850' - 1,000'	28
YELLOW - 5	1,000' - 1,250'	34
	1,250' - 1,500'	41

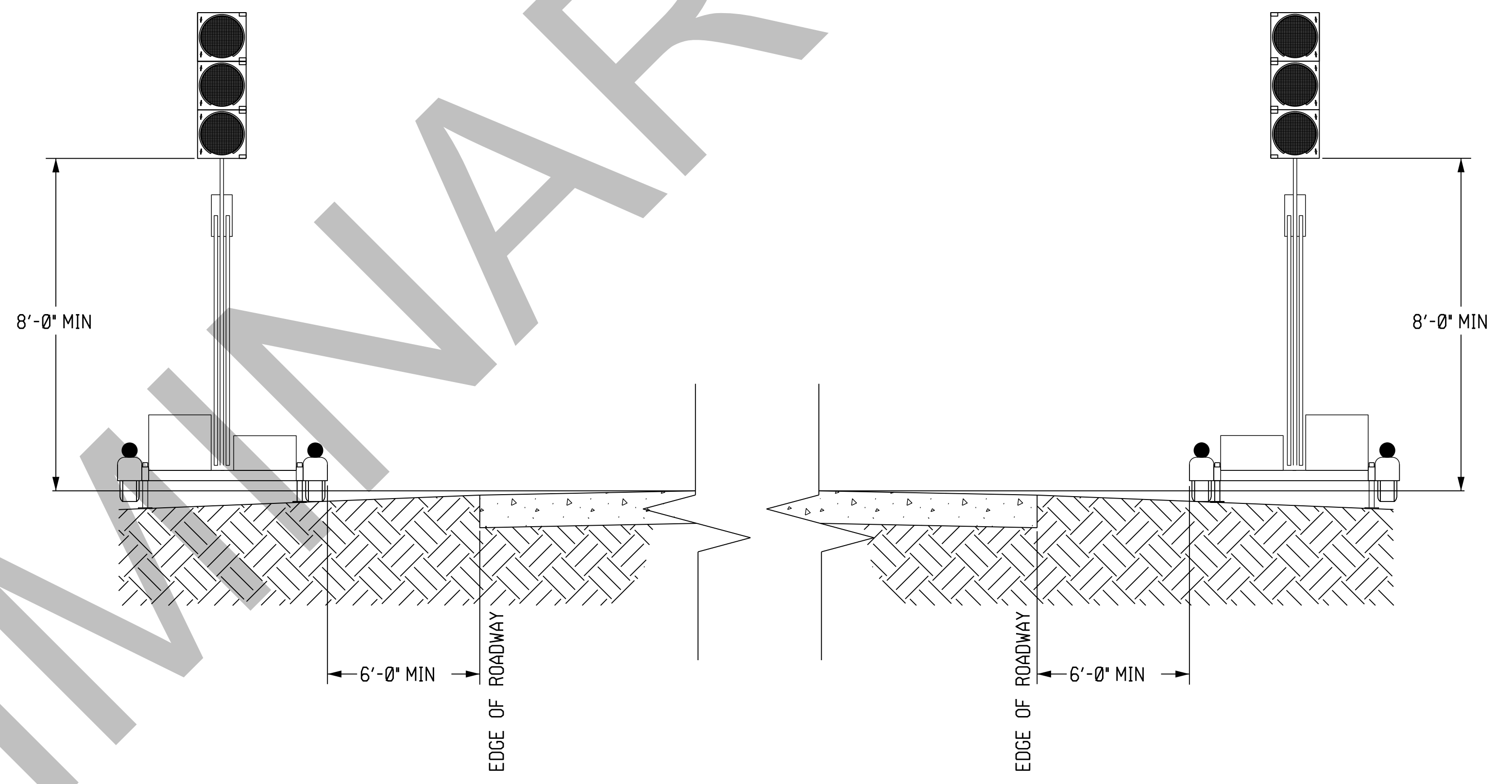
ADDITIONAL SIGNS
 USE WHERE APPLICABLE

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PORTABLE SIGNAL TRAILER (MULTI SIGNAL UNIT) INSTALLATION



PORTABLE SIGNAL TRAILER (SINGLE SIGNAL UNIT) INSTALLATION



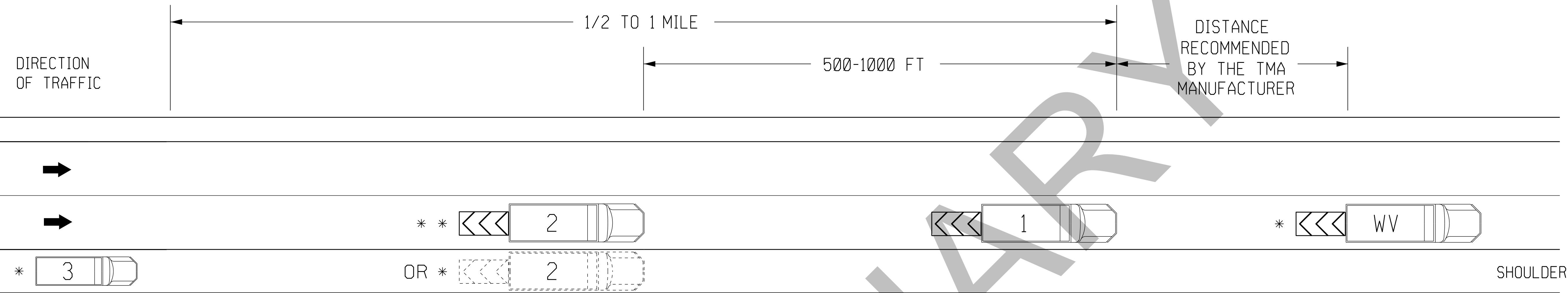
NOTES

1. THE CONTRACTOR SHALL FURNISH ALL EQUIPMENT AND MATERIAL REQUIRED FOR THE TEMPORARY SIGNAL UNLESS OTHERWISE SPECIFIED. ALL EQUIPMENT AND MATERIAL FURNISHED BY THE CONTRACTOR SHALL REMAIN HIS PROPERTY.
2. ANY STATE SUPPLIED EQUIPMENT OR MATERIAL SHALL REMAIN THE PROPERTY OF THE STATE OF NEBRASKA.
3. THE SIGNAL HEAD LENSES SHALL BE A MINIMUM OF 12 INCHES IN DIAMETER.
4. ALL SIGNAL LAMPS SHALL BE EXTENDED ANGLE LED.
5. MAINTENANCE OF THE TEMPORARY SIGNAL SHALL BE THE FULL RESPONSIBILITY OF THE CONTRACTOR.
6. TRAFFIC SIGNALS POWERED BY MEANS OF A PORTABLE GENERATOR SHALL HAVE STANDBY BATTERY POWER CAPABLE OF OPERATING THE SIGNAL NOT LESS THAN 26 HOURS. PORTABLE TRAFFIC SIGNALS SHALL BE CHECKED EVERY 24 HOURS TO INSURE PROPER OPERATION.
7. THE SIGNAL SHALL BE PLACED INTO FLASHING AMBER OPERATION FOR BOTH DIRECTIONS DURING PERIODS WHEN THE LANE ARE OPEN TO TWO-WAY TRAFFIC. UNDER NO CIRCUMSTANCES SHALL AN INOPERATIVE TRAFFIC SIGNAL BE LEFT UNCOVERED ON AN OPEN ROAD TO THE PUBLIC.
8. INSTALLATION OF THE TEMPORARY SIGNAL SHALL BE IN COMPLIANCE WITH THE NATIONAL ELECTRIC CODE (LATEST EDITION), THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, ALL LOCAL ORDINANCES AND REGULATIONS, THE SPECIFICATIONS AND THE PROJECT PLANS.
9. SEE SIGNING STANDARD FOR REQUIRED SIGNING TO ACCOMPANY SIGNAL.
10. WHEN REQUIRED, THE CONTRACTOR SHALL FURNISH ALL EQUIPMENT AND MATERIAL REQUIRED FOR VEHICLE DETECTION ON ALL APPROACHES.

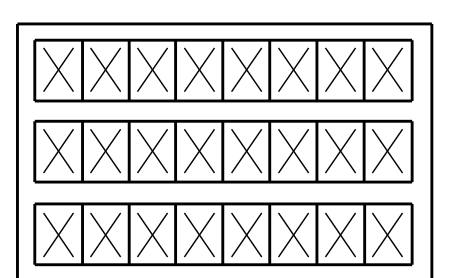
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MOBILE OPERATIONS ON RURAL MULTI-LANE ROADWAY

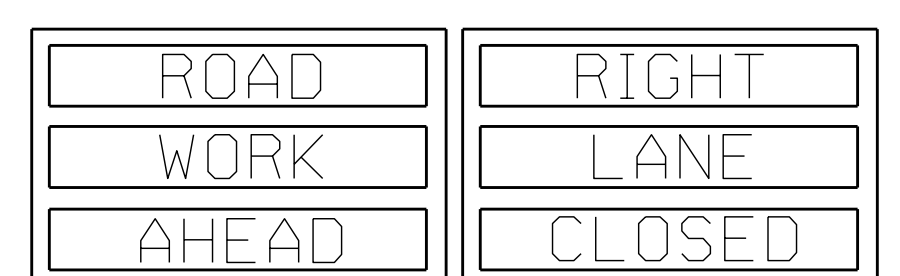
M6
Project Number
66-6(110)
C.N. 13392



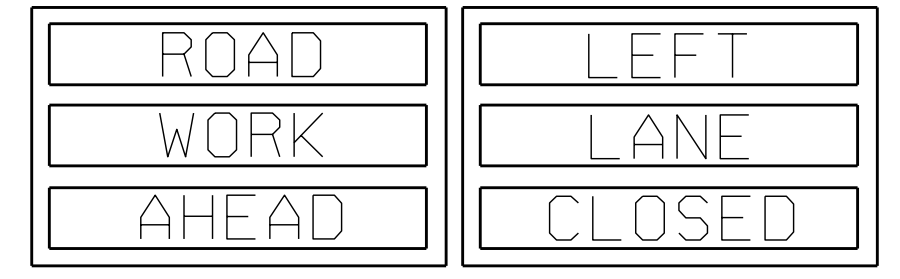
SHADOW VEHICLE 3
& PORTABLE DYNAMIC MESSAGE SIGN
WITH 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT
* TMA OPTIONAL



PORTABLE DYNAMIC MESSAGE SIGN

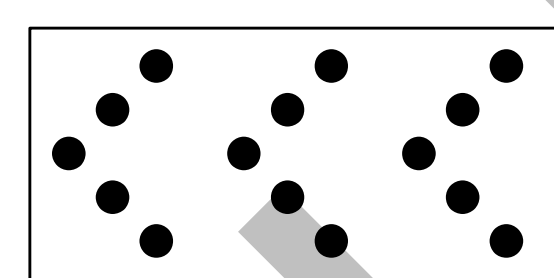


-OR-



TWO MESSAGES (1.6 - 2.0 SECONDS PER MESSAGE)

OPTIONAL SHADOW VEHICLE 2
& FLASHING ARROW PANEL ON REAR
WITH 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT
* TMA OPTIONAL IF COMPLETELY ON
SHOULDER AND NOT STRADDLING EDGE LINE.
* * TMA REQUIRED IF STRADDLING EDGE
LINE OR IN CLOSED LANE.

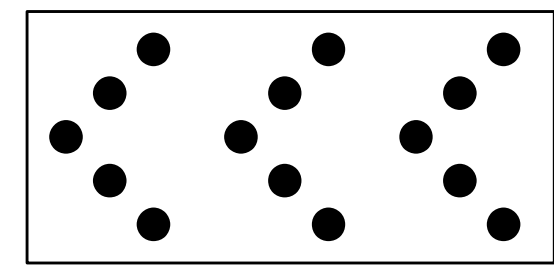


FLASHING ARROW PANEL ON REAR

TRUCK MOUNTED ATTENUATOR SYSTEM:

1. THE CONTACTOR SHALL FURNISH A FEDERALLY APPROVED TRUCK MOUNTED ATTENUATOR SYSTEM, MOUNTED ON A MINIMUM 16,000 POUND TRUCK. THE TRUCK SHALL BE EQUIPPED WITH 60" X 30" FLASHING ARROW PANEL, SECURLY MOUNTED ON THE TRUCK. THE TMA SYSTEM SHALL BE LOCATED IN THE FIELD AS REQUIRED BY THE MANUFACTURER. A COMPLETE SET OF REPLACEMENT MODULES SHALL BE AVAILABLE NEAR THE PROJECT SITE IN THE EVENT OF DAMAGE TO THE INSTALLED TMA. DAMAGED TMA'S SHALL BE REMOVED FROM THE ROADWAY AND PROJECT WORK STOPPED UNTIL REPAIRS TO THE UNIT HAVE BEEN COMPLETED.
2. THE TRUCK MOUNTED ATTENUATOR SHALL BE AN NCHRP 350 TEST LEVEL 3 (OR MASH EQUIVALENT) APPROVED TMA FOR 100 km PER HOUR (60 MPH).
3. THE TRUCK SHALL BE A 16,000 TO 35,000 POUND (GVW) VEHICLE AS REQUIRED BY THE TMA MANUFACTURER.
4. THE FLASHING ARROW PANEL SHALL BE SECURLY MOUNTED AS HIGH AS PRACTICABLE ON THE VEHICLE. THE ARROW PANEL SHALL NOT COME LOOSE UPON IMPACT TO THE TMA.

SHADOW VEHICLE 1 WITH TMA
& FLASHING ARROW PANEL ON REAR
WITH 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT



FLASHING ARROW PANEL ON REAR

NOTES:

1. SHADOW VEHICLE 1 SHALL HAVE A TRUCK MOUNTED ATTENUATOR. SHADOW VEHICLE 2 IS OPTIONAL AND SHALL HAVE A TRUCK-MOUNTED ATTENUATOR IF IN THE CLOSED LANE OR STRADDLING THE EDGE LINE. SHADOW VEHICLE 3 MAY HAVE A TMA.
2. SHADOW VEHICLE 1 AND WHEN USED, SHADOW VEHICLE 2 SHALL HAVE A FLASHING ARROW PANEL.
3. SHADOW VEHICLE NUMBER 3 SHALL HAVE A PORTABLE DYNAMIC MESSAGE SIGN.
4. WHEN USED, SHADOW VEHICLE 2 SHALL BE PLACED IN ADVANCE OF HORIZONTAL OR VERTICAL CURVES TO PROVIDE ADVANCE WARNING FOR WORK OPERATIONS HIDDEN BY CURVES.
5. FOR LEFT LANE CLOSURES, SHADOW VEHICLE 3 WILL REMAIN ON RIGHT SHOULDER WHEN AN 8' OR WIDER PAVED INSIDE (LEFT) SHOULDER DOES NOT EXIST.
6. IN URBAN AREAS THE DISTANCE MAINTAINED BETWEEN VEHICLES MAY BE DECREASED AS NEEDED.
7. VEHICLE HAZARD LIGHT WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING OSCILLATING, OR STROBE LIGHTS.
8. DETAIL MAY BE USED FOR LEFT OR RIGHT LANE/SHOULDER WORK.

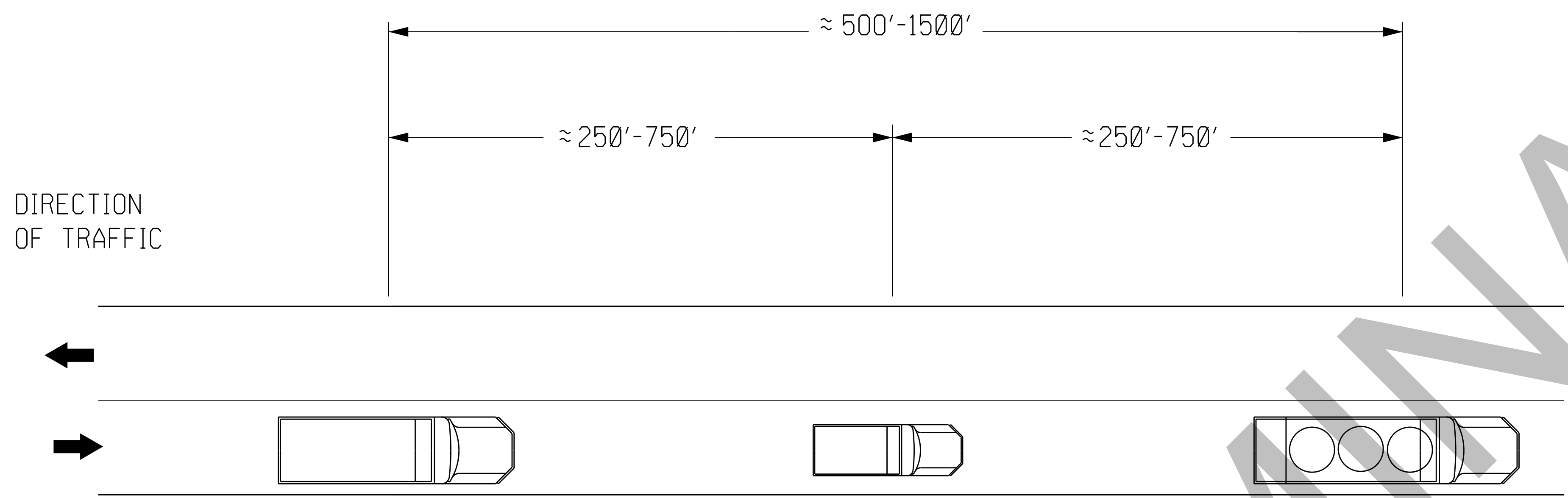
MOBILE OPERATIONS
TWO-LANE AND MULTI-LANE
NEBRASKA DEPARTMENT OF TRANSPORTATION - TRAFFIC ENGINEERING DIVISION

DESIGNED BY NRI
DATE 12/22

PLAN SHEET NUMBER
1 / 2

COMPUTER: BG0419M687
DATE: 3-APR-2023 08:50
FILE: Mobile Operations.dgn

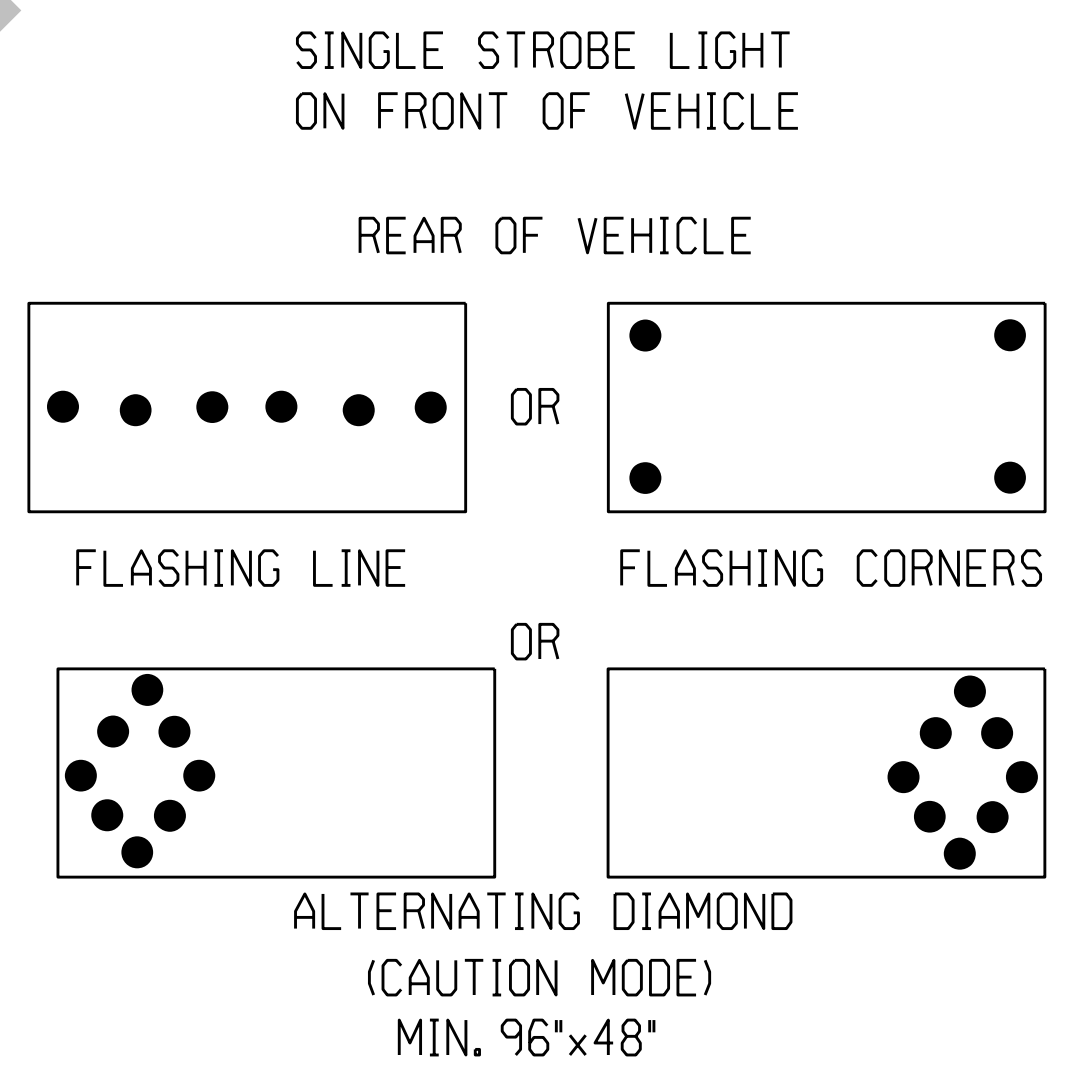
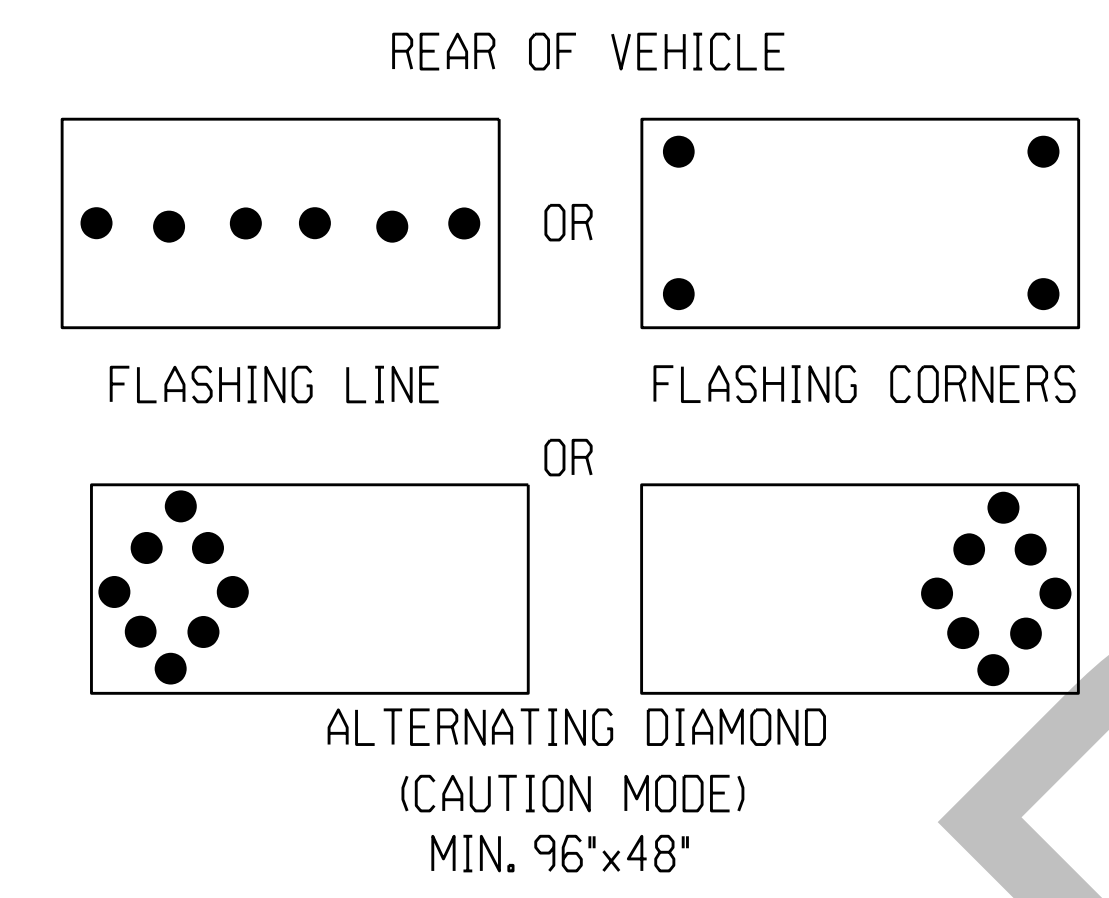
MOBILE OPERATIONS RURAL TWO-LANE TWO-WAY ROADS



REAR VEHICLE
WITH 2 HIGH INTENSITY
FLASHING LIGHTS
MOUNTED ON THE REAR
AND 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT
(TMA OPTIONAL)

OPTIONAL VEHICLE
WITH 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT

WORK VEHICLE
WITH 4 HIGH INTENSITY
FLASHING LIGHTS MOUNTED
ON REAR AND 2-360°
BEACONS OR APPROVED
MINI-BAR LIGHT



NOTES:

1. CAUTION MODE ON WORK VEHICLE AND REAR VEHICLE SHALL BE ALTERNATING DIAMOND OR THE FLASHING 4 CORNER LIGHTS IF THE DIAMOND MODE IS NOT AVAILABLE.
2. REAR VEHICLE SHALL BE PLACED IN ADVANCE OF HORIZONTAL OR VERTICAL CURVES TO PROVIDE ADVANCE WARNING FOR WORK OPERATIONS HIDDEN BY CURVES.

COMPUTER: BG0419M687
DATE: 3-APR-2023 08:50
FILE: Mobile Operations.dgn

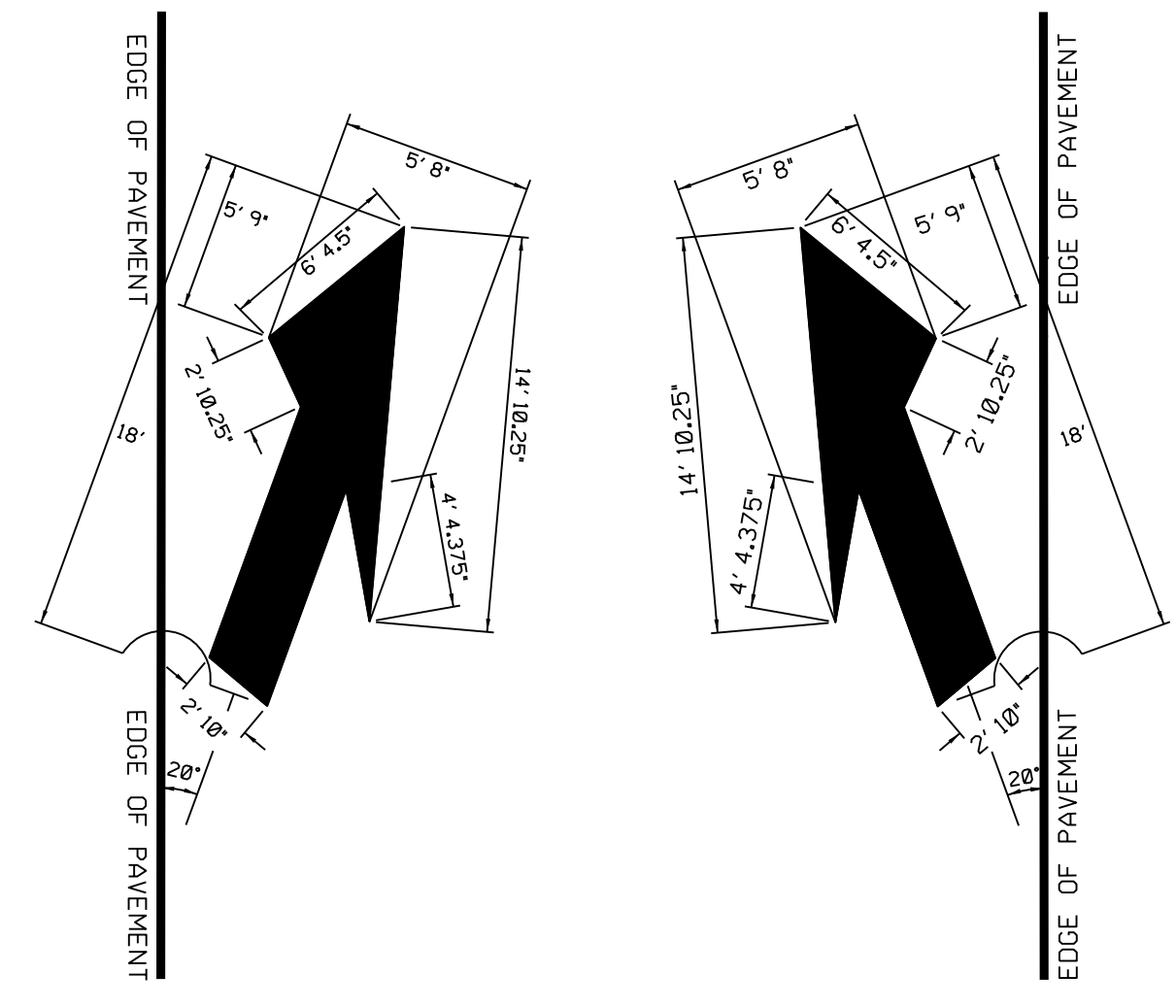
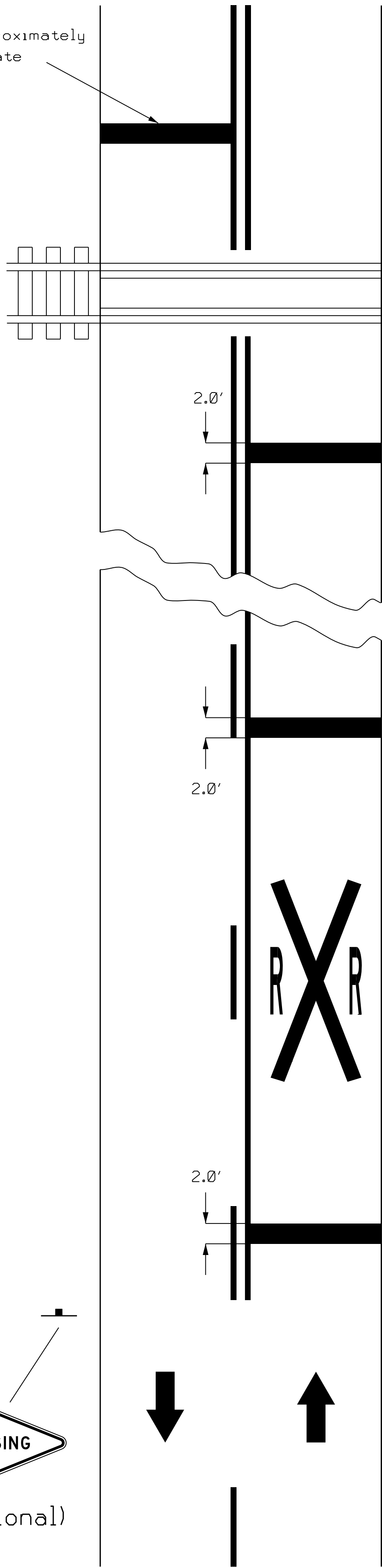
MOBILE OPERATIONS
TWO-LANE AND MULTI-LANE

DESIGNED BY: NRL
DATE: 12/22

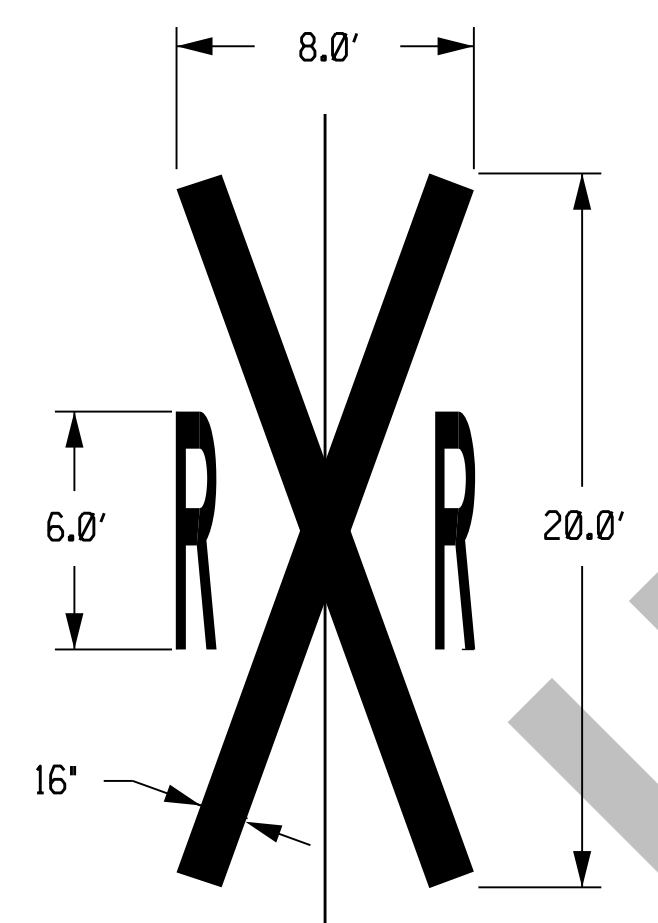
NEBRASKA DEPARTMENT OF TRANSPORTATION - TRAFFIC ENGINEERING DIVISION

Stop line approximately 8.0 ft from gate (if present)

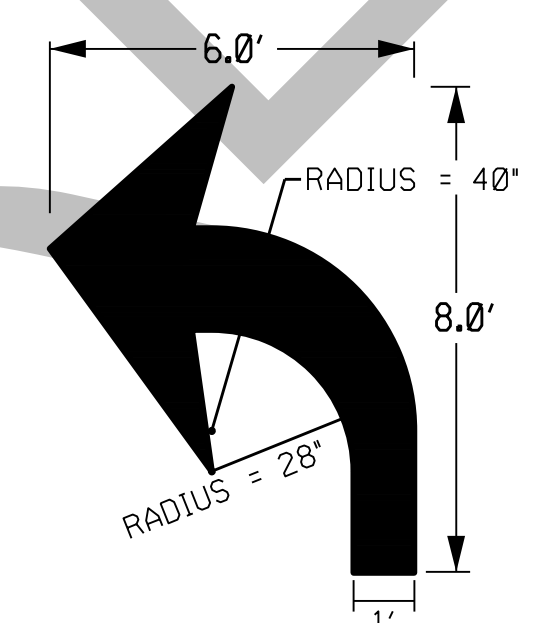
Note: See Figure 8B-6 and 8B-7 in the MUTCD for more information.



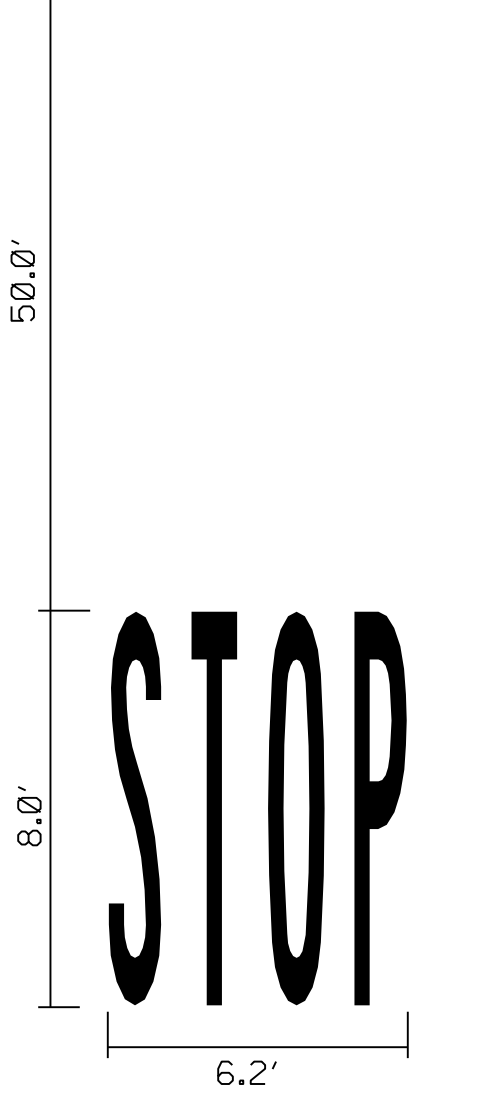
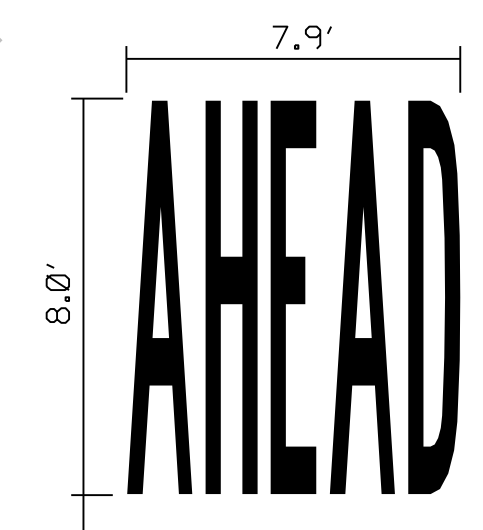
LANE REDUCTION ARROW DETAIL



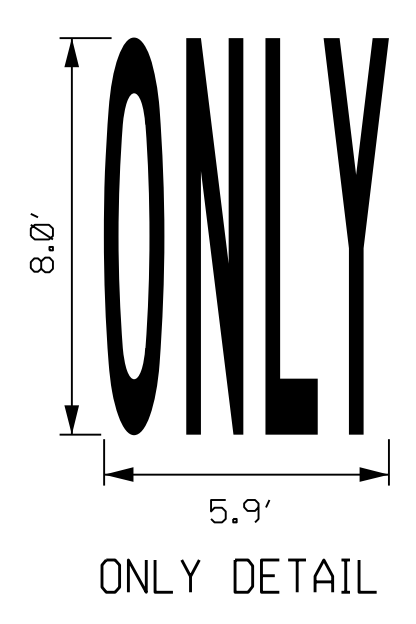
RAILROAD CROSSING SYMBOL



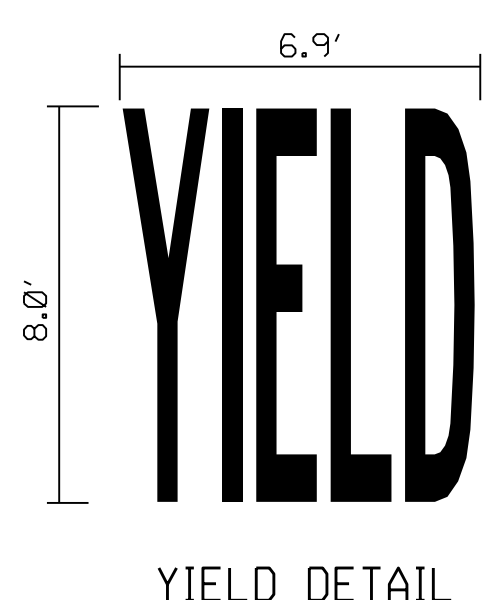
ARROW DETAIL



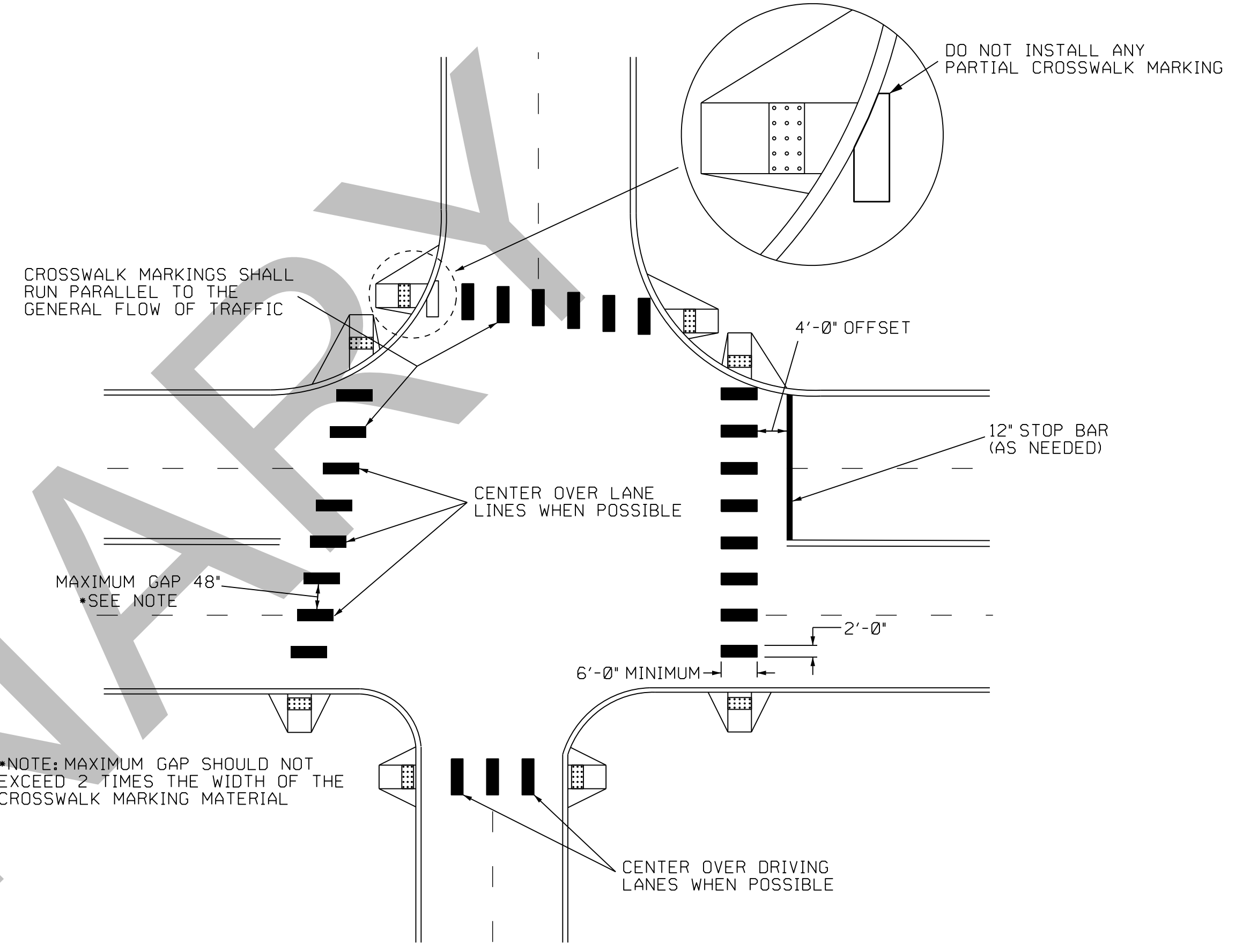
STOP AHEAD DETAIL



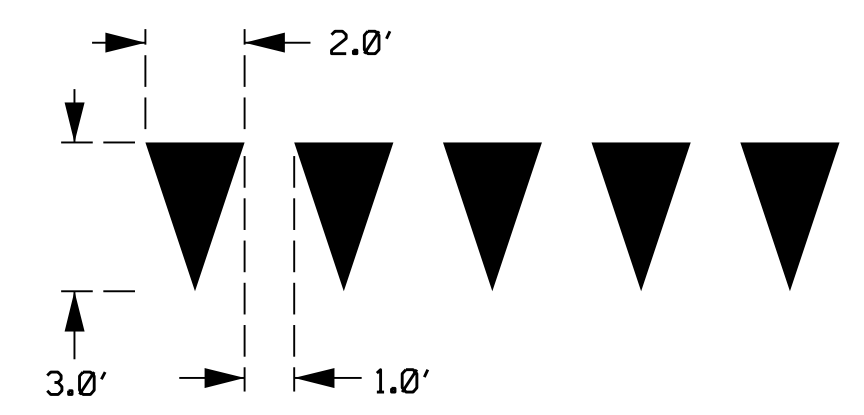
ONLY DETAIL



YIELD DETAIL

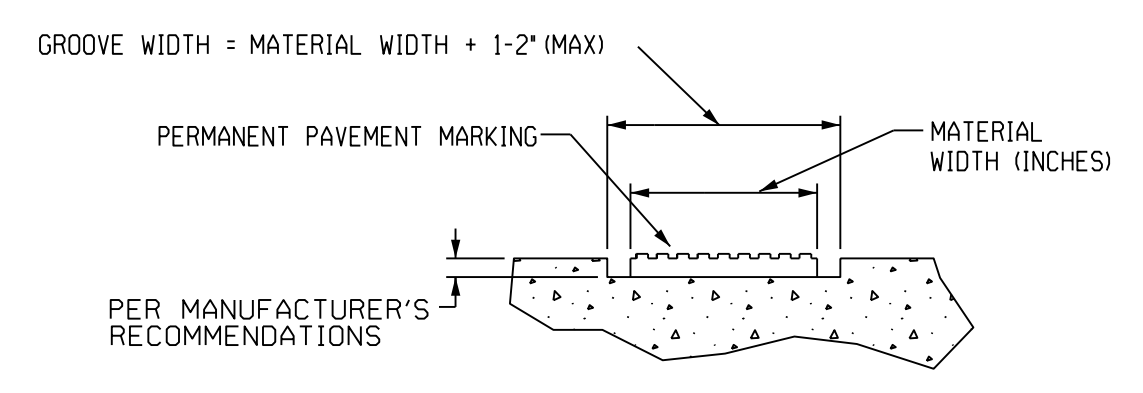


TYPICAL CROSSWALK DETAIL



YIELD LINE DETAIL

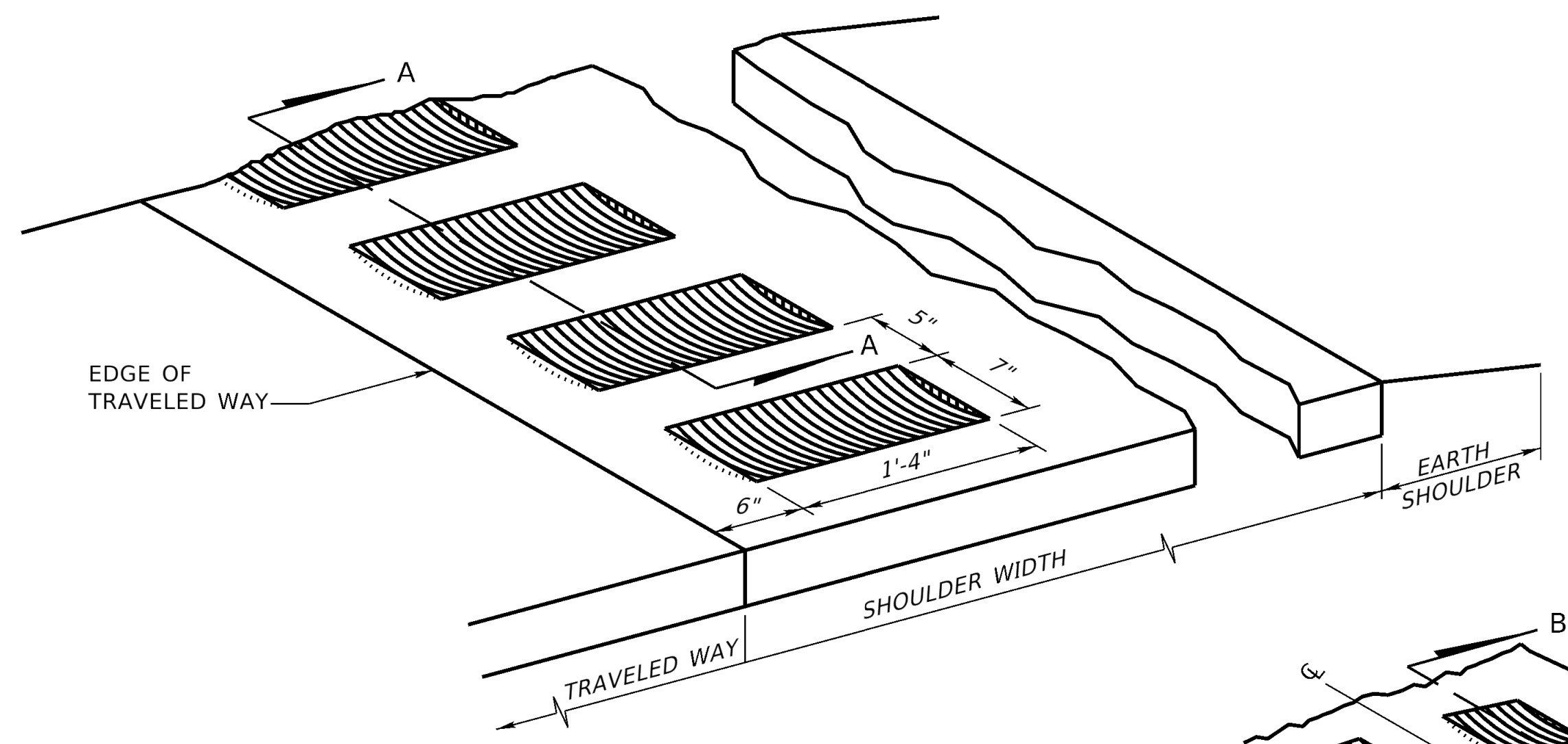
PERMANENT PAVEMENT MARKINGS INSTALLED IN GROOVES



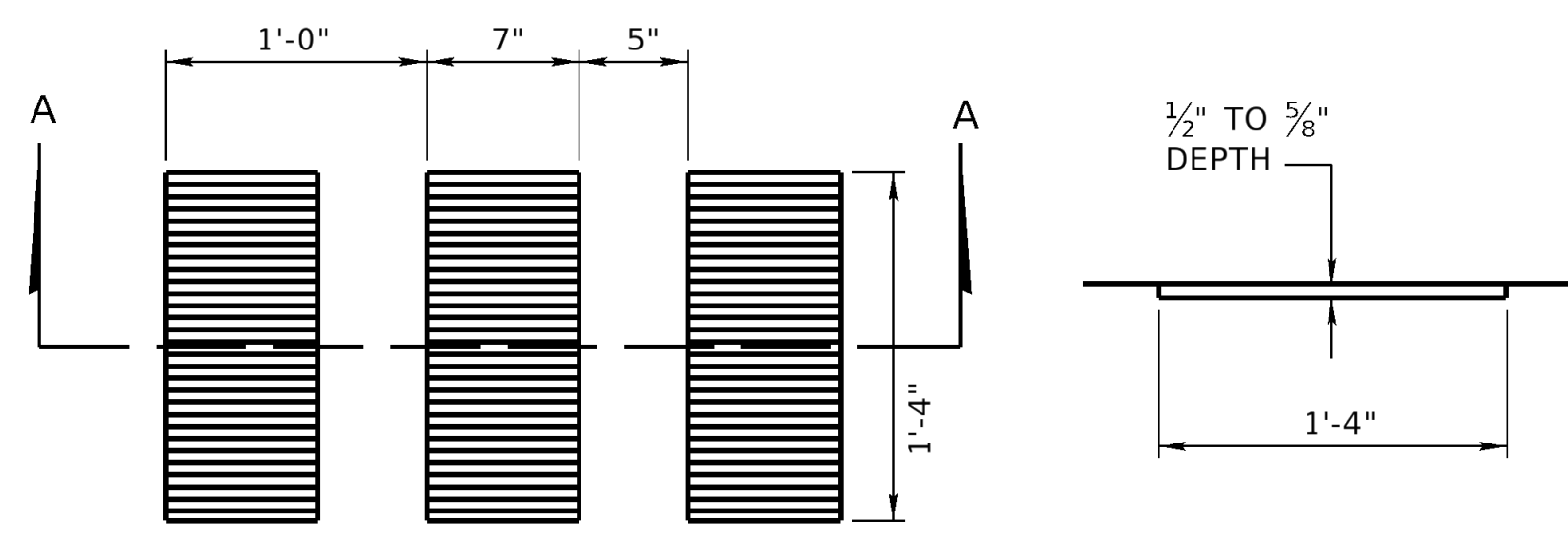
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DATE: 3-APR-2023 08:50

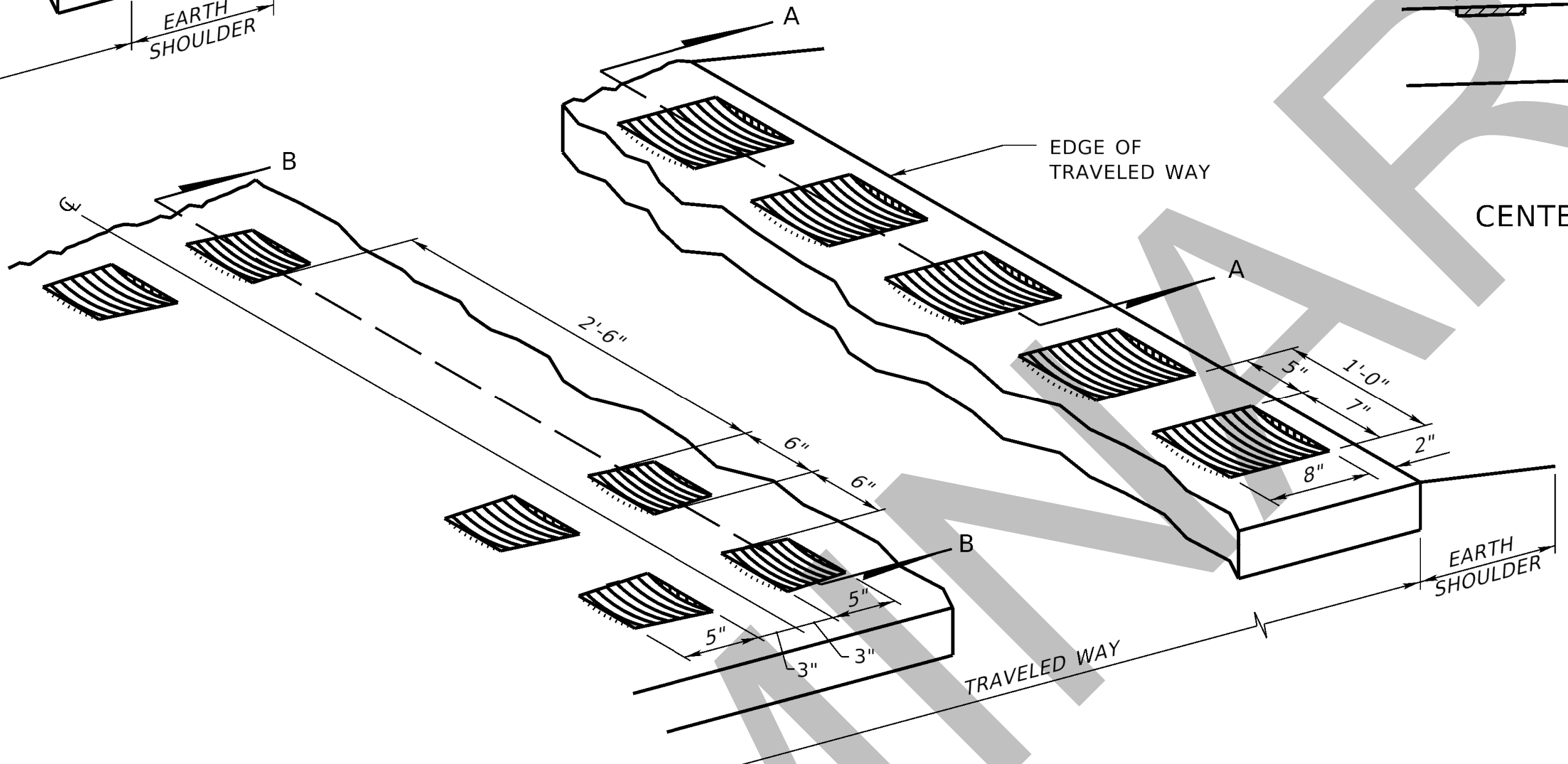
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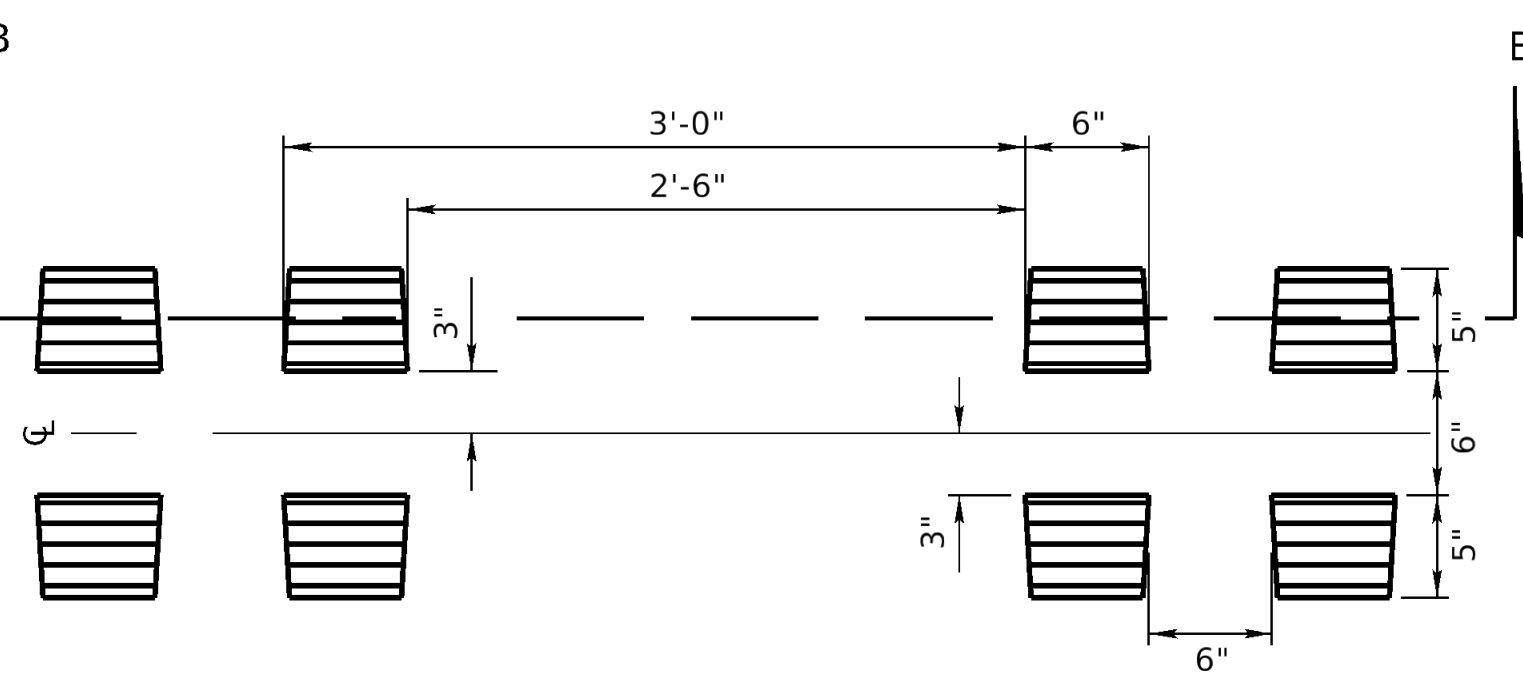
SHOULDER RUMBLE STRIPS DETAIL



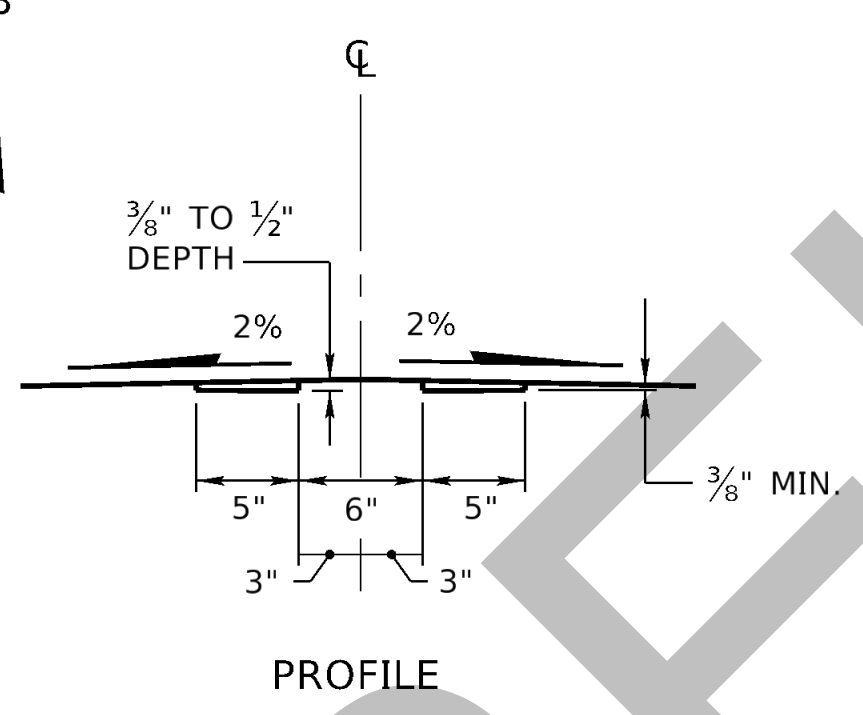
SHOULDER RUMBLE STRIPS SHAPE



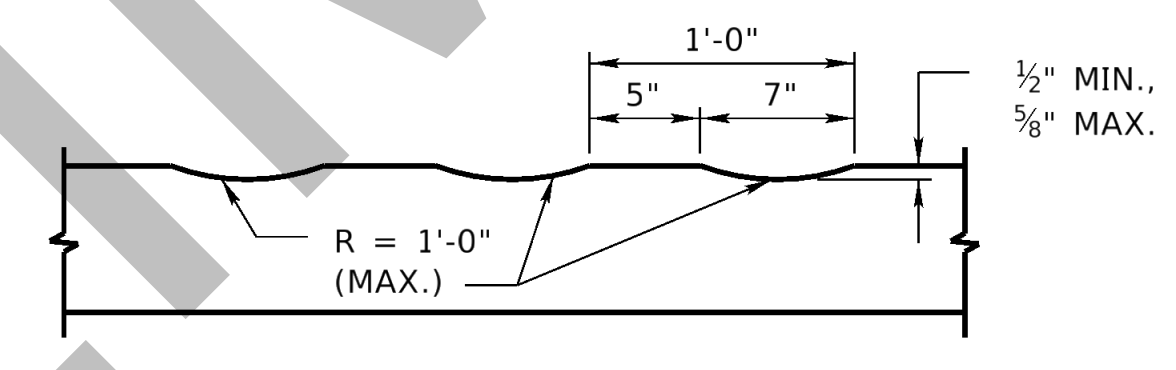
CENTERLINE RUMBLE STRIPS DETAIL



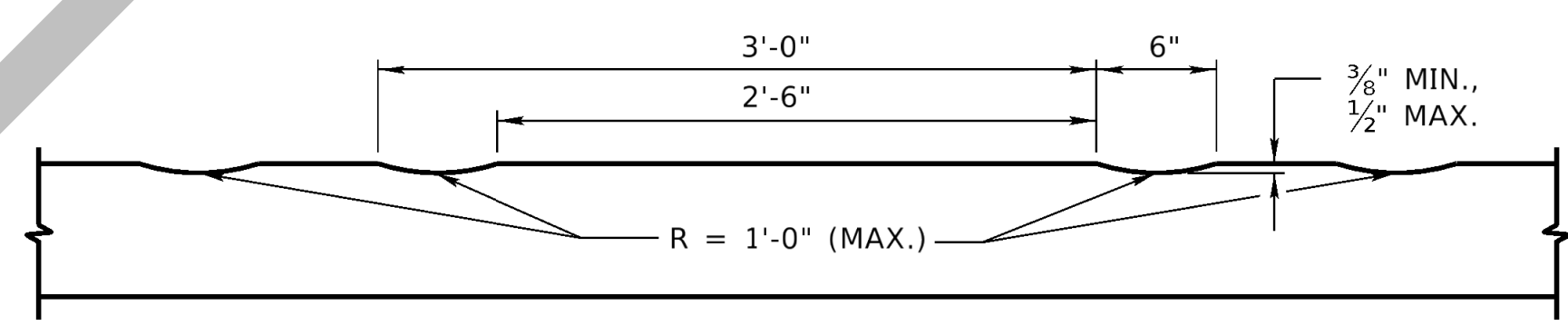
CENTERLINE RUMBLE STRIPS SHAPE



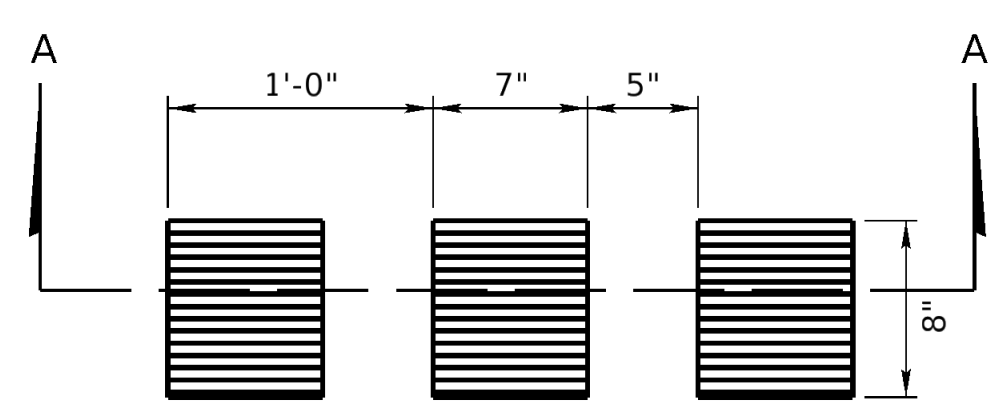
SHOULDER AND EDGELINE RUMBLE STRIPS SECTION A-A



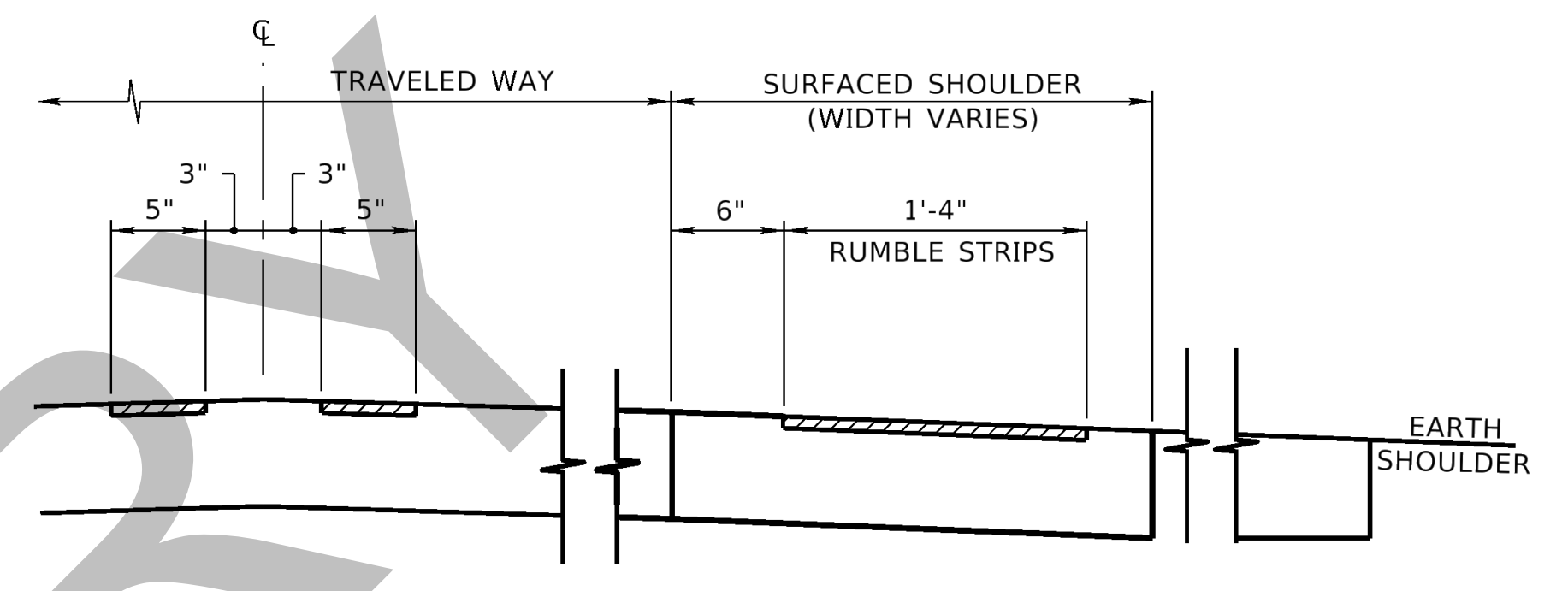
EDGELINE RUMBLE STRIPS DETAIL



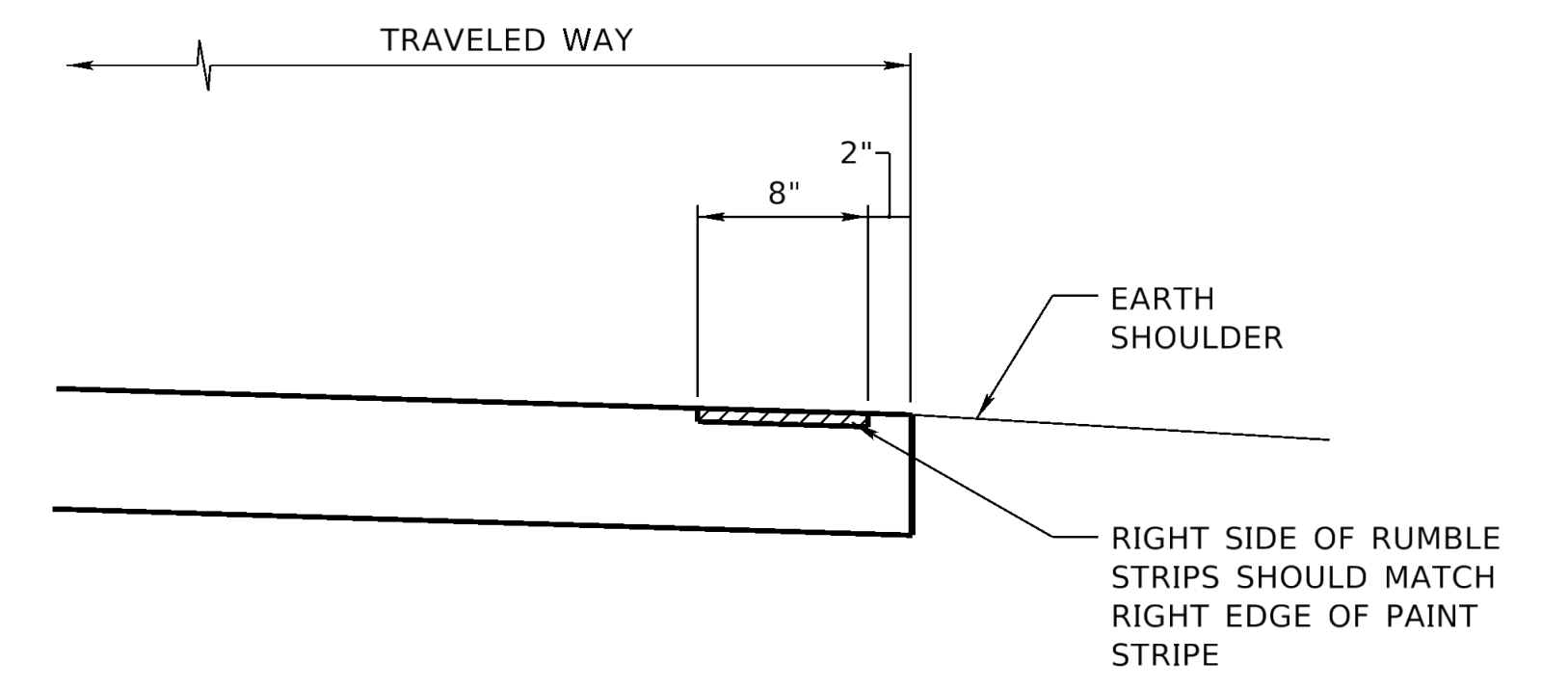
CENTERLINE RUMBLE STRIPS SECTION B-B



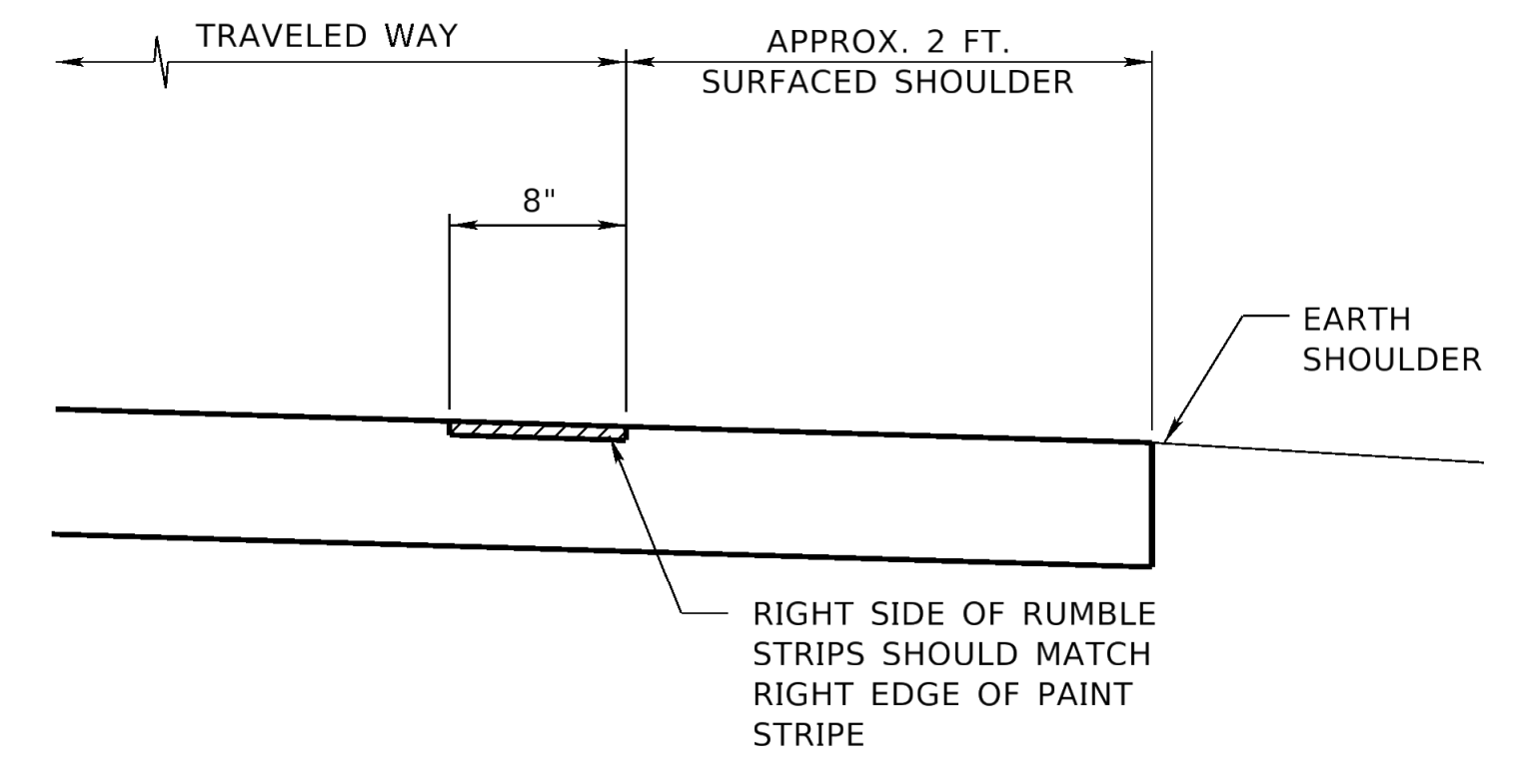
EDGELINE RUMBLE STRIPS SHAPE



CENTERLINE SHOULDER



EDGELINE ON 24 FEET ROADWAY



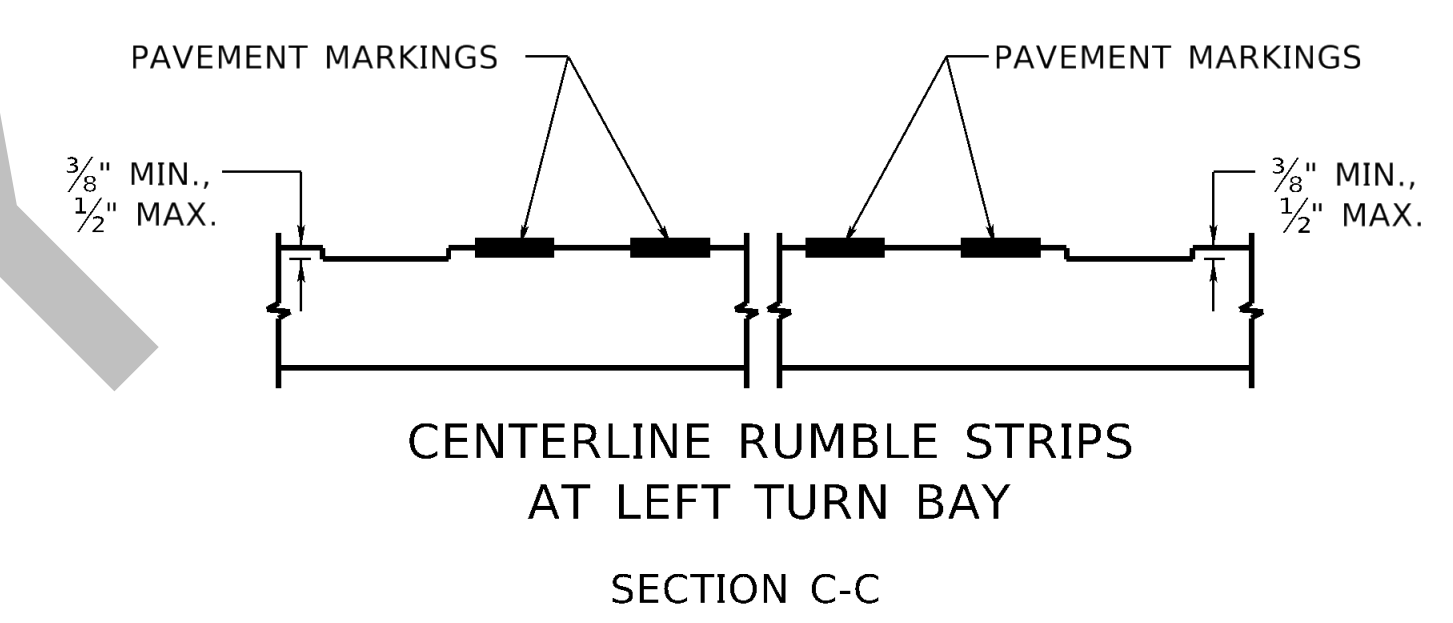
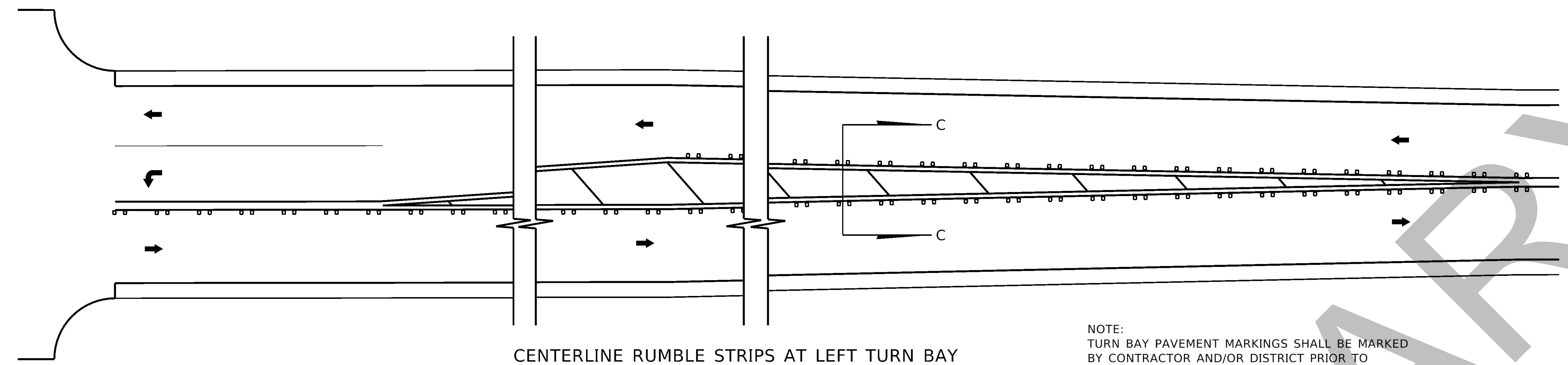
EDGELINE ON 28 FEET ROADWAY

NOTES:

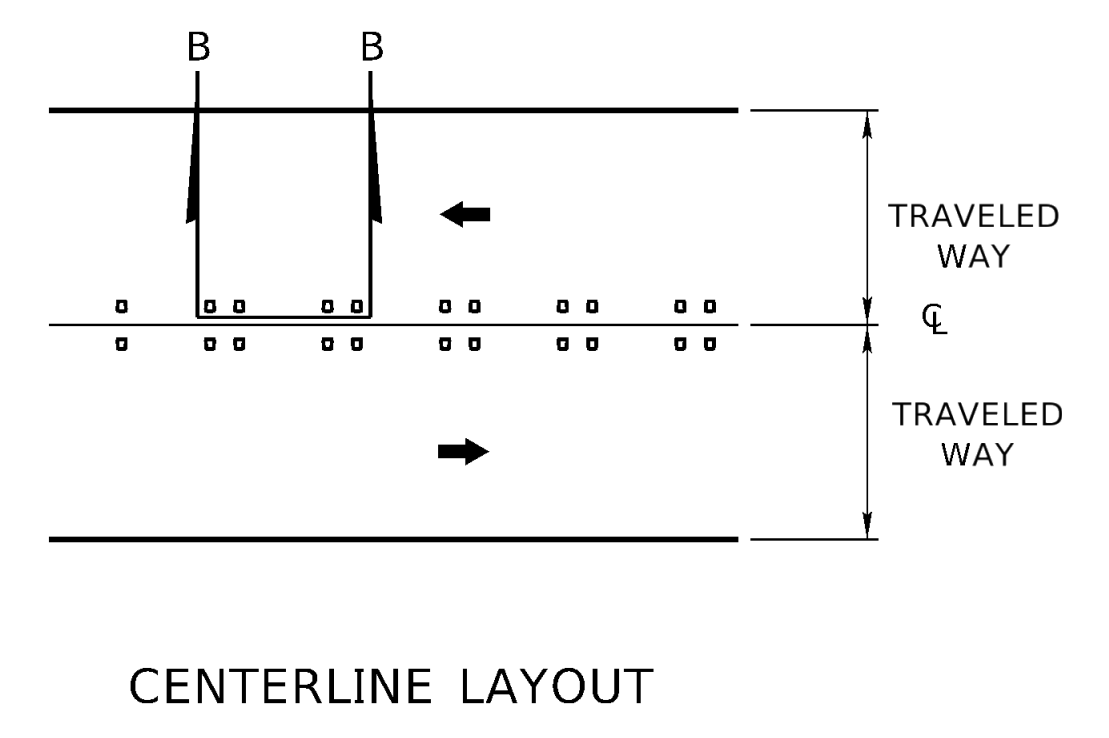
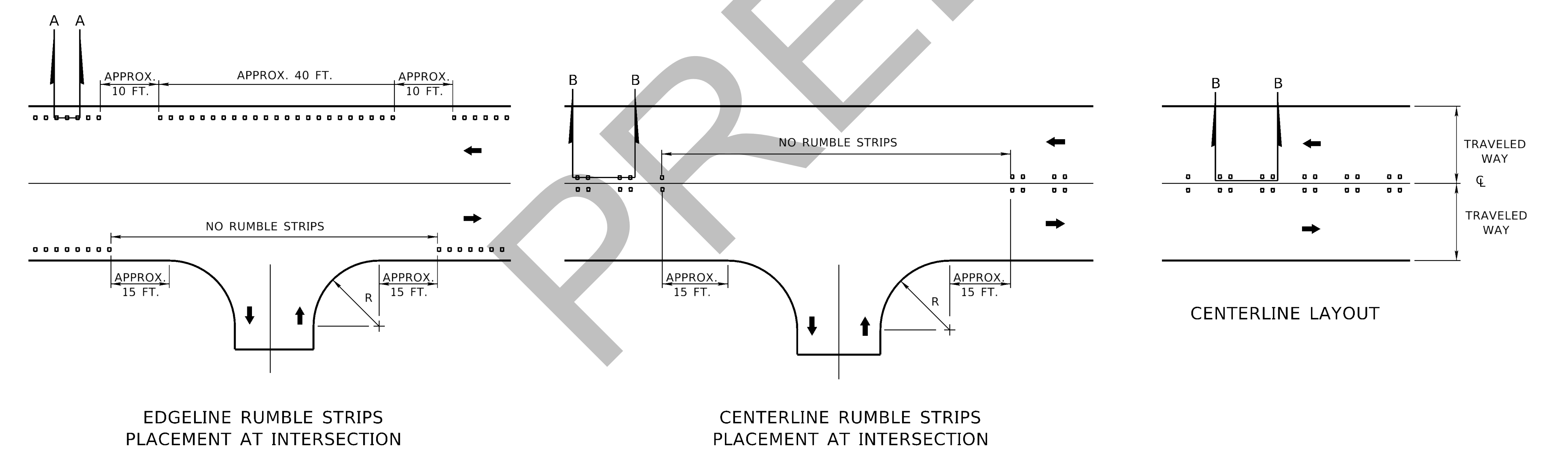
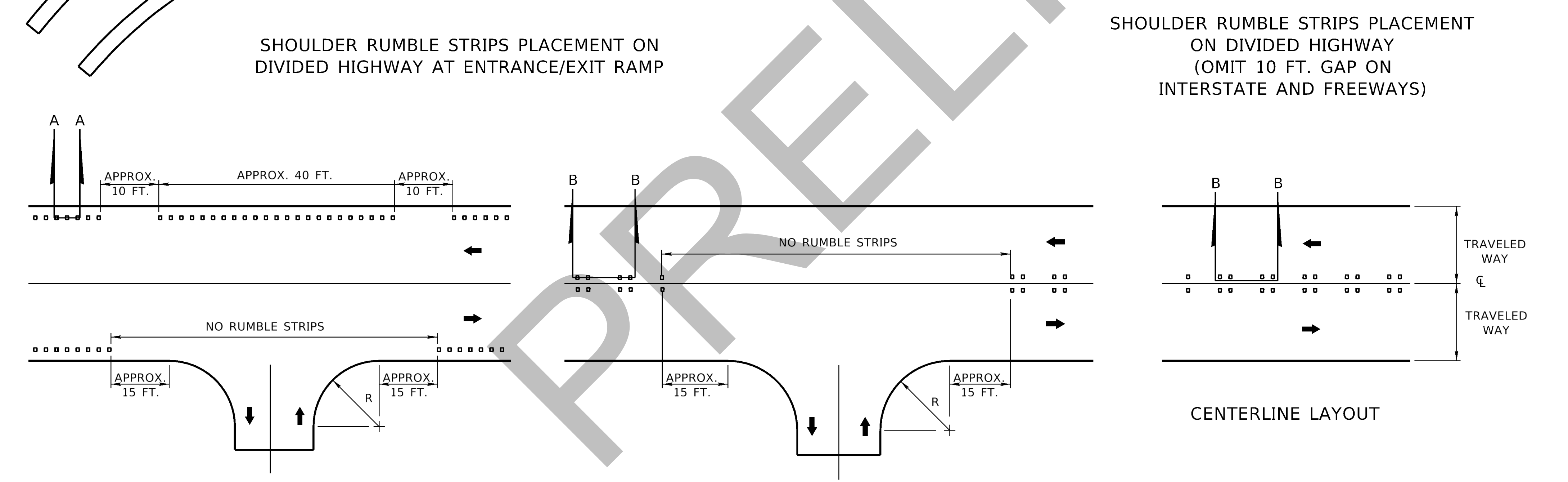
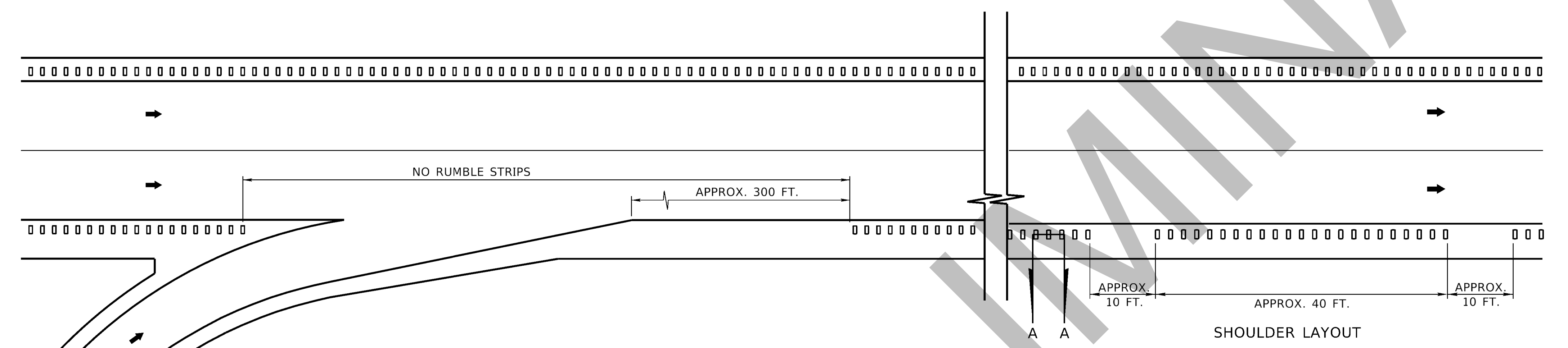
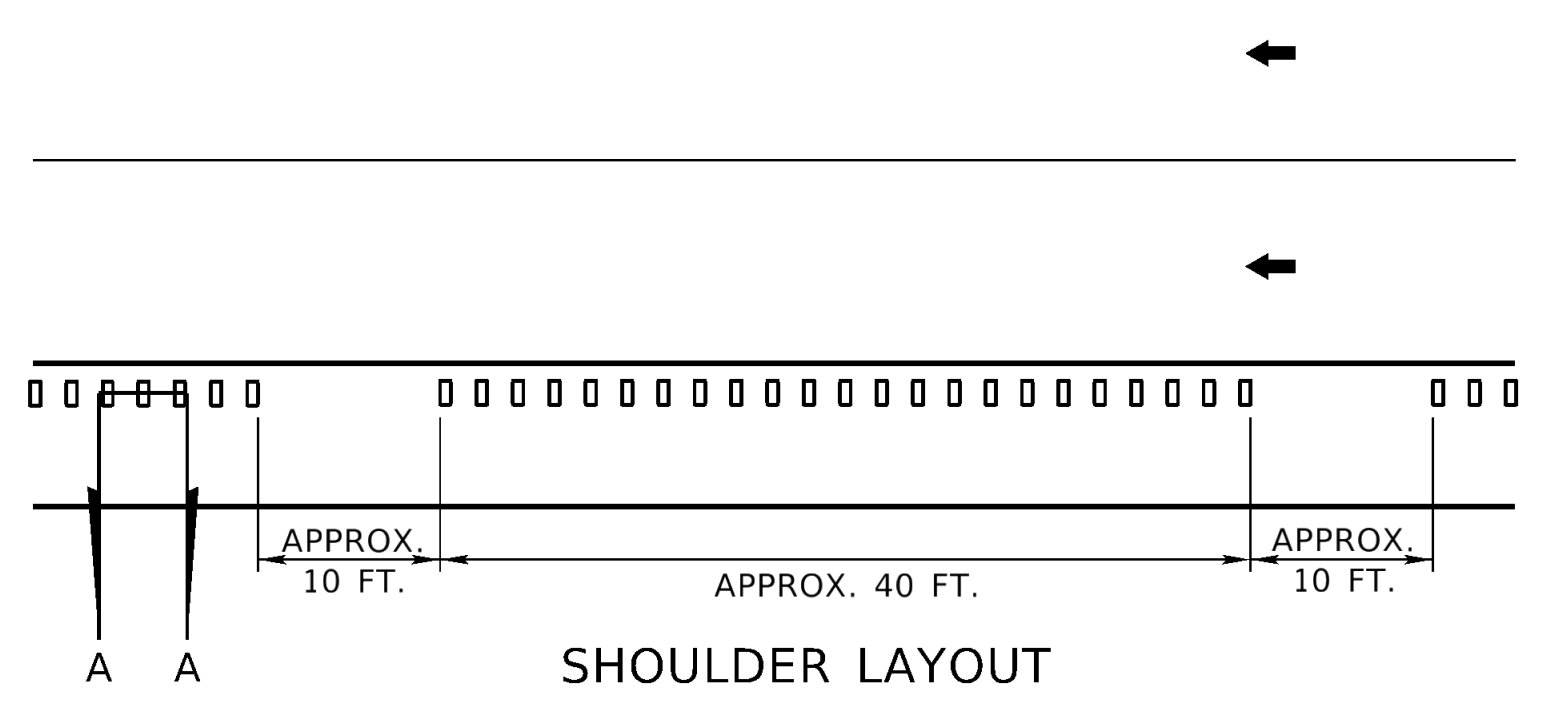
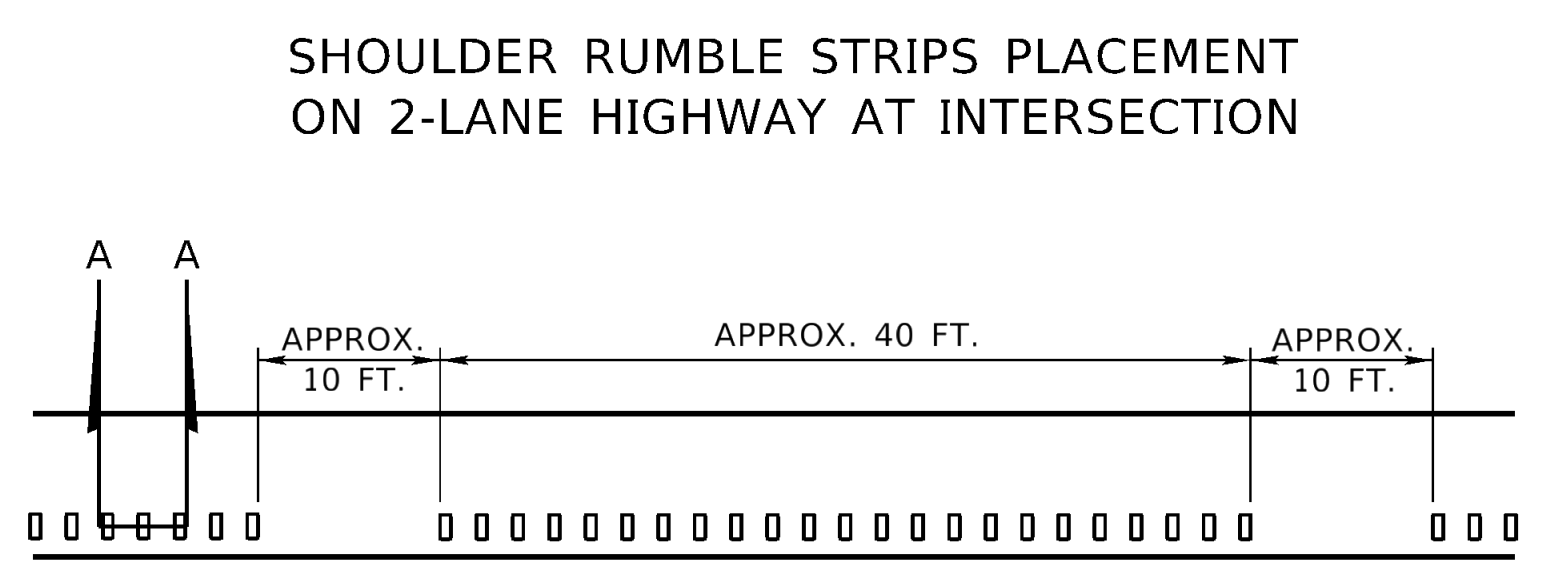
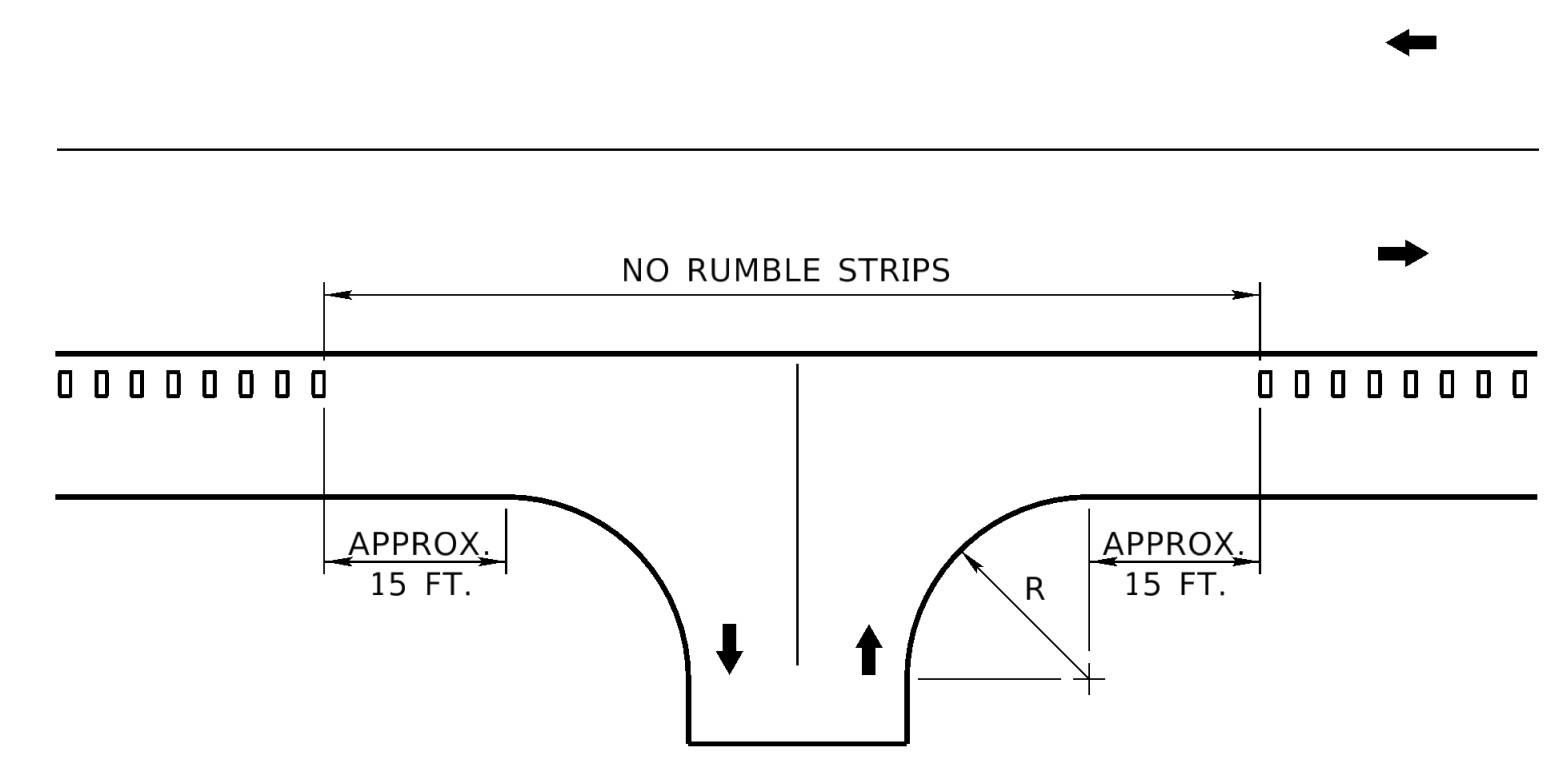
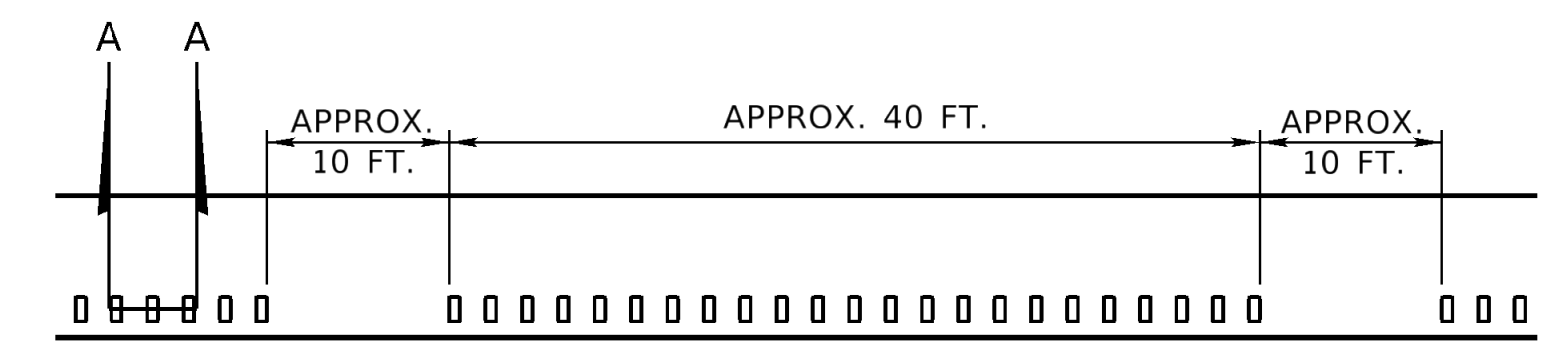
RUMBLE STRIPS SHALL BE PLACED ON SHOULDERS AS INDICATED IN THIS PLAN AND IN ACCORDANCE WITH THE PROJECT PLANS. RUMBLE STRIPS ARE NOT NORMALLY REQUIRED ON CITY STREETS AND OTHER URBAN SHOULDERS ADJACENT TO CURB AND GUTTER UNLESS SPECIFICALLY NOTED IN THE PLANS.

RUMBLE STRIPS MAY BE CONTINUOUS THROUGH DRIVEWAYS AND SHALL BE OMITTED ACROSS INTERSECTING ROADWAYS AND BRIDGES.

COMPUTER: BG0419M593
DATE: 3-MAY-2023 13:02
FILE: 13392_Sheets_Specials.dgn
1 OF 2



NOTE:
TURN BAY PAVEMENT MARKINGS SHALL BE MARKED BY CONTRACTOR AND/OR DISTRICT PRIOR TO CONSTRUCTION OF RUMBLE STRIPS.



COMPUTER: BG0419M593

DATE: 4-MAY-2023 07:47

FILE: 13392_Sheets_Specials.dgn