

WELCOME!

I-680, FORT ST. AND BLAIR HIGH RD. IMPROVEMENTS

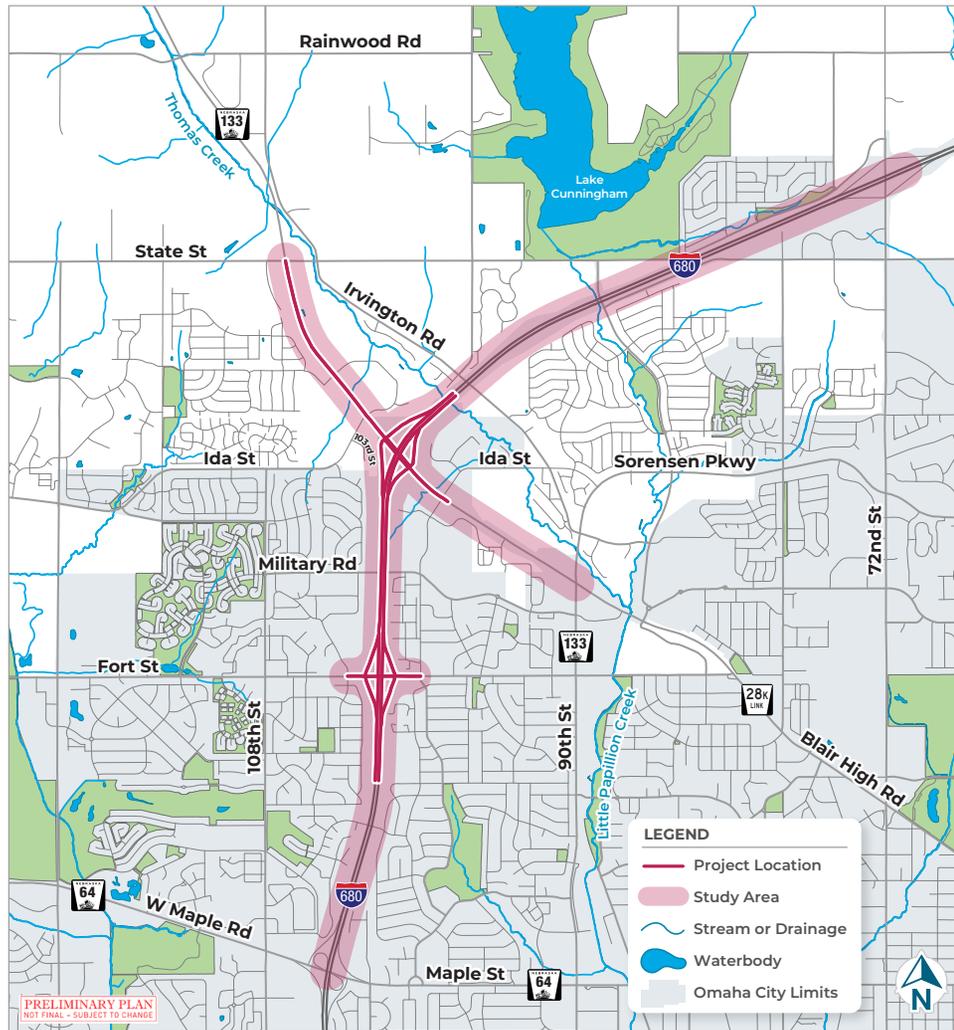
PUBLIC INFORMATION MEETING

NH-MTIS-680-9(53) FORT ST. – BLAIR HIGH RD., OMAHA; C.N. 22860

MARCH 19, 2026

PLEASE SIGN IN

PROJECT LOCATION



SCOPE OF WORK

The proposed improvements would:



Widen I-680 from the existing four-lane interstate to a six-lane interstate with auxiliary lanes between Fort St. and Blair High Rd. (N-133).



The Fort St. Interchange would be reconstructed to a half partial cloverleaf interchange.



The I-680 bridge over Fort St. would be replaced.



Blair High Rd. (N-133) Interchange would be reconstructed to a partial cloverleaf interchange.



All other bridges would be used in place with repairs.

PROJECT PURPOSE & NEED



THE PURPOSE OF THE PROJECT IS TO:

Preserve the transportation asset, improve the reliability of the transportation system and perpetuate the mobility of the traveling public.



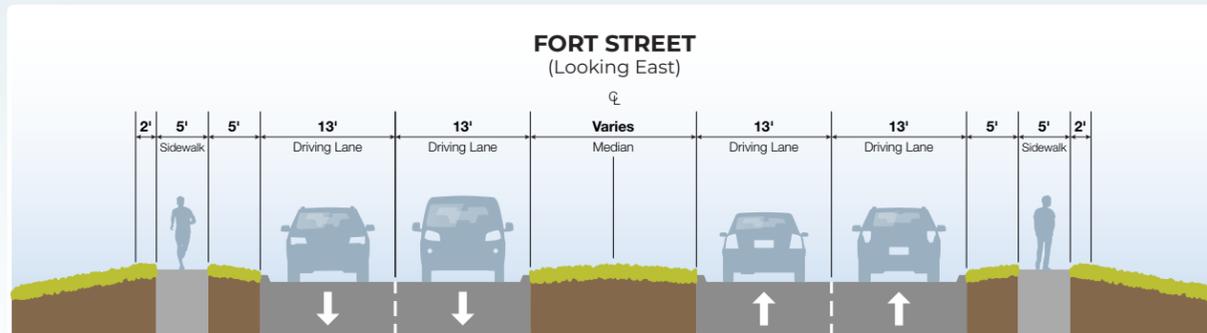
THE NEED FOR THE PROJECT IS:

The need for pavement work on this project is based on information from the NDOT's Pavement Management System, Materials & Research Pavement Design section, District 2, and the Metro Area Travel Improvement Study (MTIS). These entities have determined a need for interchange reconfiguration at Fort St. and Blair High Rd. (N-133) interchanges and additional northbound and southbound lanes to improve operations and safety and reduce delay.

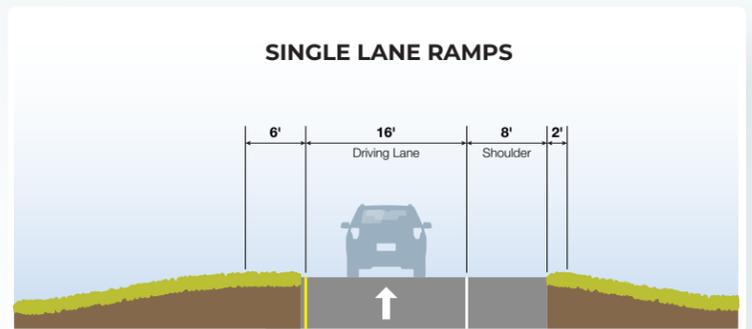
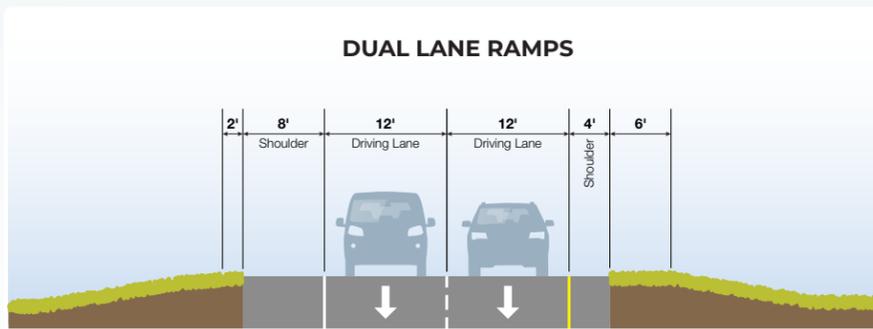
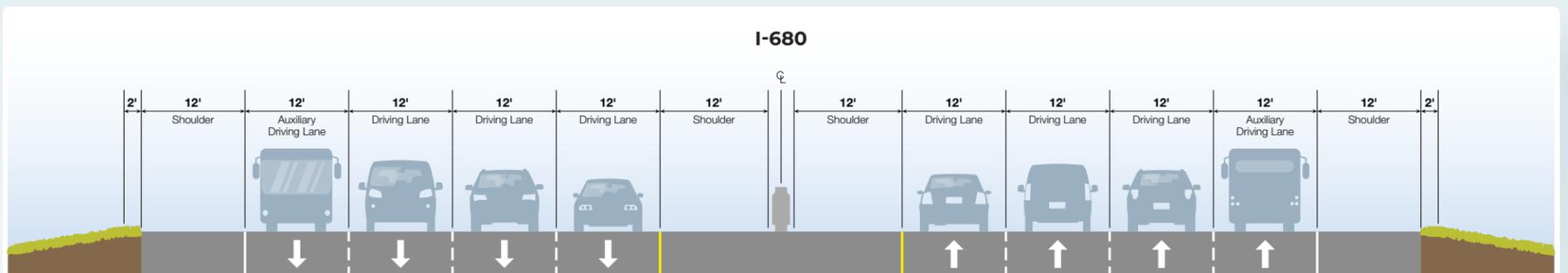
The need for bridge work on this project is based on information from the NDOT's Bridge Inventory Rating System, Bridge Division, and District 2.

TYPICAL SECTIONS

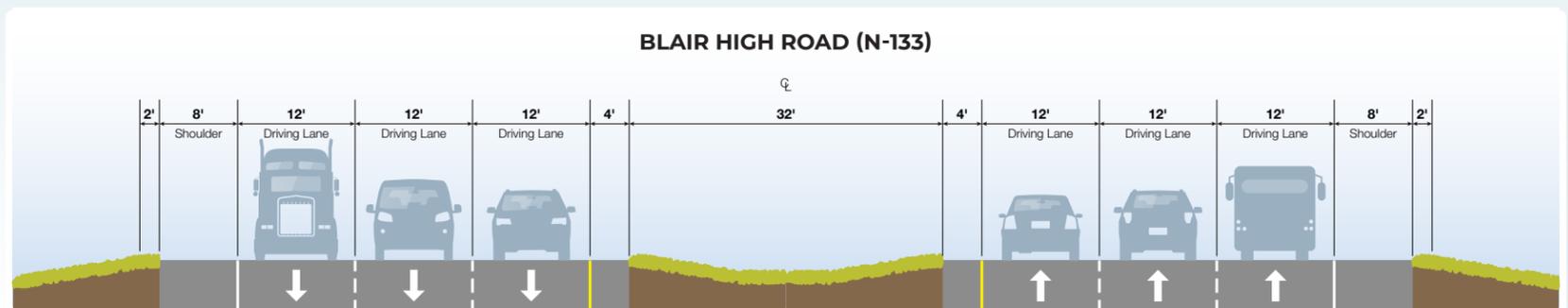
Fort St.



I-680



Blair High Rd. (N-133)



PRELIMINARY PLAN
NOT FINAL - SUBJECT TO CHANGE

ENVIRONMENTAL CONSIDERATIONS

NEPA DOCUMENTATION



Per the National Environmental Policy Act, project impacts on the natural and human environment are evaluated and documented. Measures would be taken to avoid, minimize or mitigate impacts.

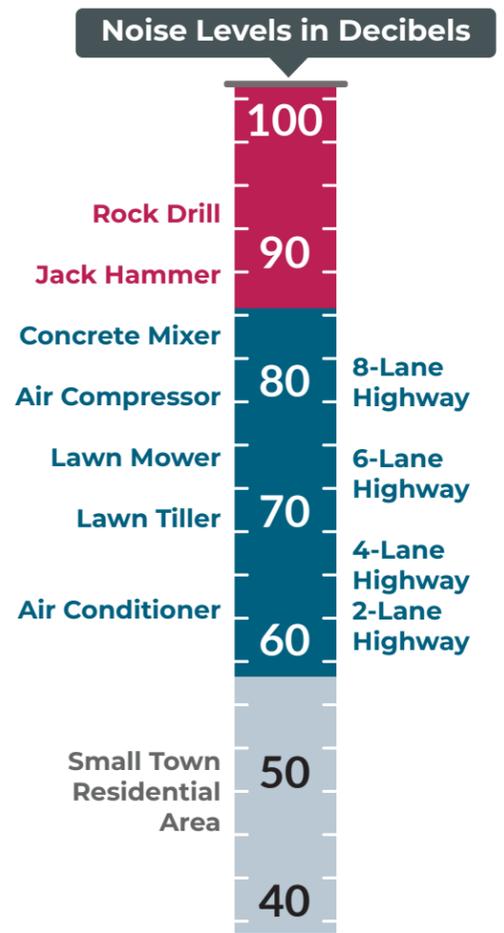


ENVIRONMENTAL RESOURCES TO BE EVALUATED

- ✓ Right-of-Way (ROW) and Access
- ✓ Section 4(f) of the Transportation Act (e.g. parks, recreation areas and historic properties)
- ✓ Section 6(f) of the Land Water Conservation Fund Act (e.g. parks, recreation areas)
- ✓ Utilities
- ✓ Water Resources and Water Quality
- ✓ Floodplain/Floodway
- ✓ Wetlands and Waters of the US
- ✓ Stormwater Drainage
- ✓ Endangered and Threatened Species, Designated Critical Habitat
- ✓ Demographics/Community Analysis/Civil Rights
- ✓ Historic Properties
- ✓ Hazardous Materials
- ✓ Noise and Air Quality

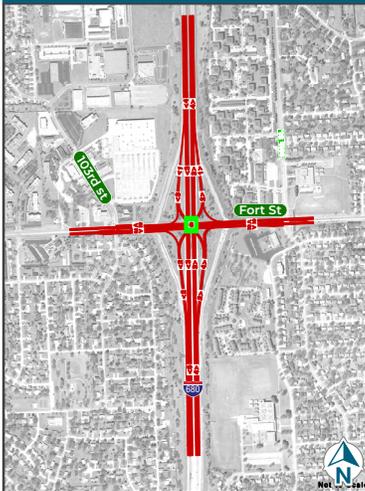
NOISE ANALYSIS

- 1 Federal Highway Administration (FHWA) requirement for Type 1 projects.
 - Projects that create
 - new highway segments
 - significant alterations to existing highways that may increase traffic noise levels.
- 2 Conducted in accordance with FHWA and NDOT policies.
- 3 Modeled noise levels using current and future traffic volumes to determine noise impacts.
- 4 Receiver is impacted if noise level is 66 decibels (dBA) or higher, or 15 dBA or higher than existing.
- 5 Evaluate the need for noise abatement for impacted areas.



FORT STREET ALTERNATIVES CONSIDERED

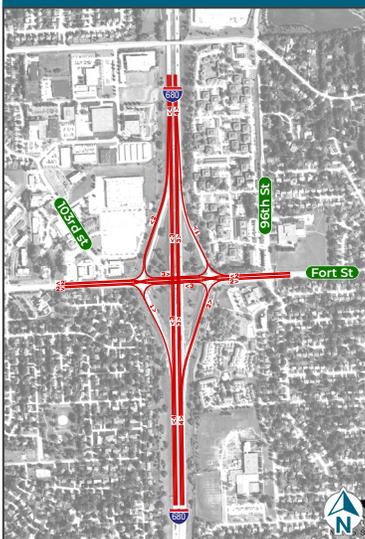
Single Point Urban Interchange (SPUI)



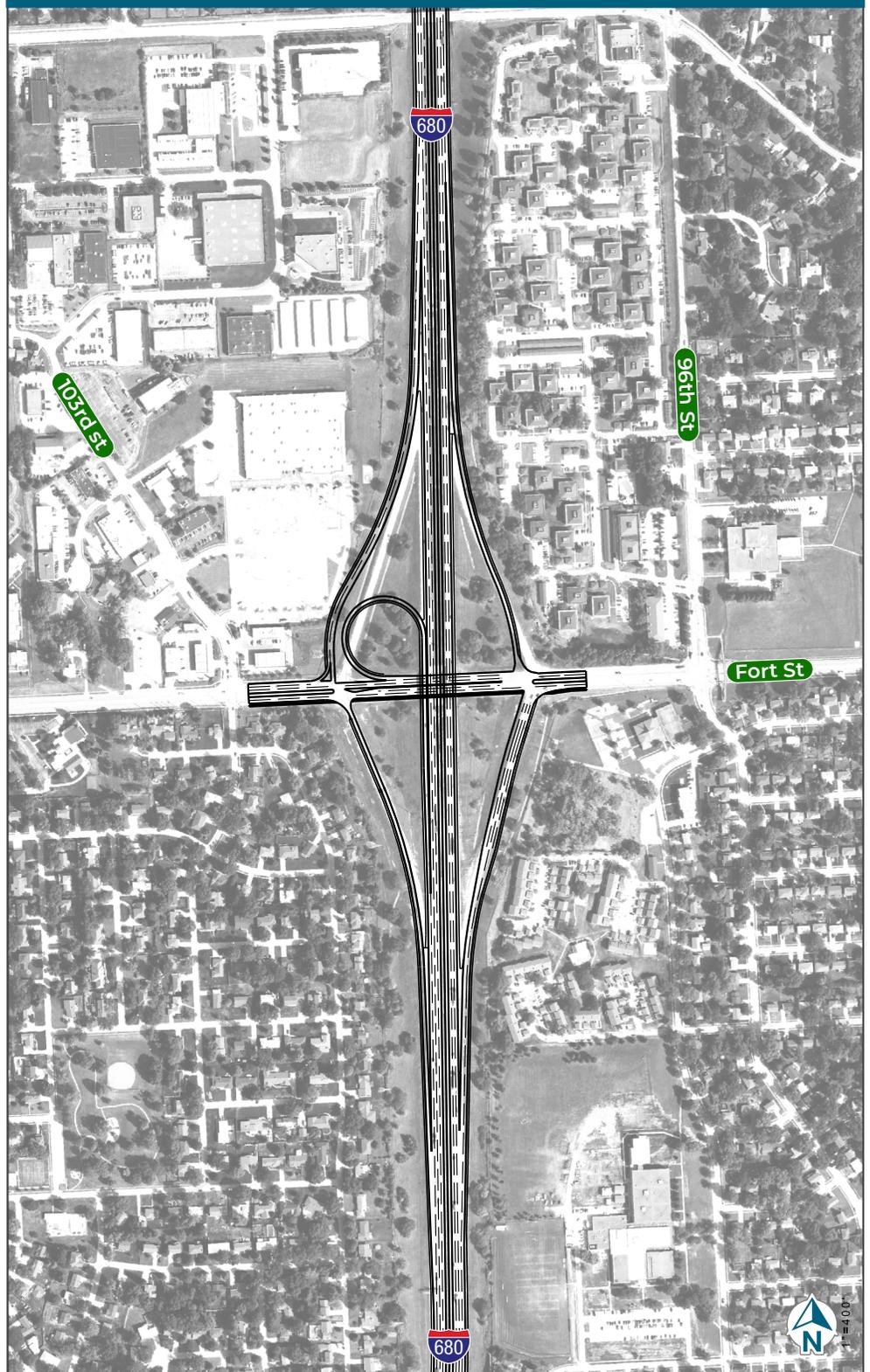
Half Partial Cloverleaf



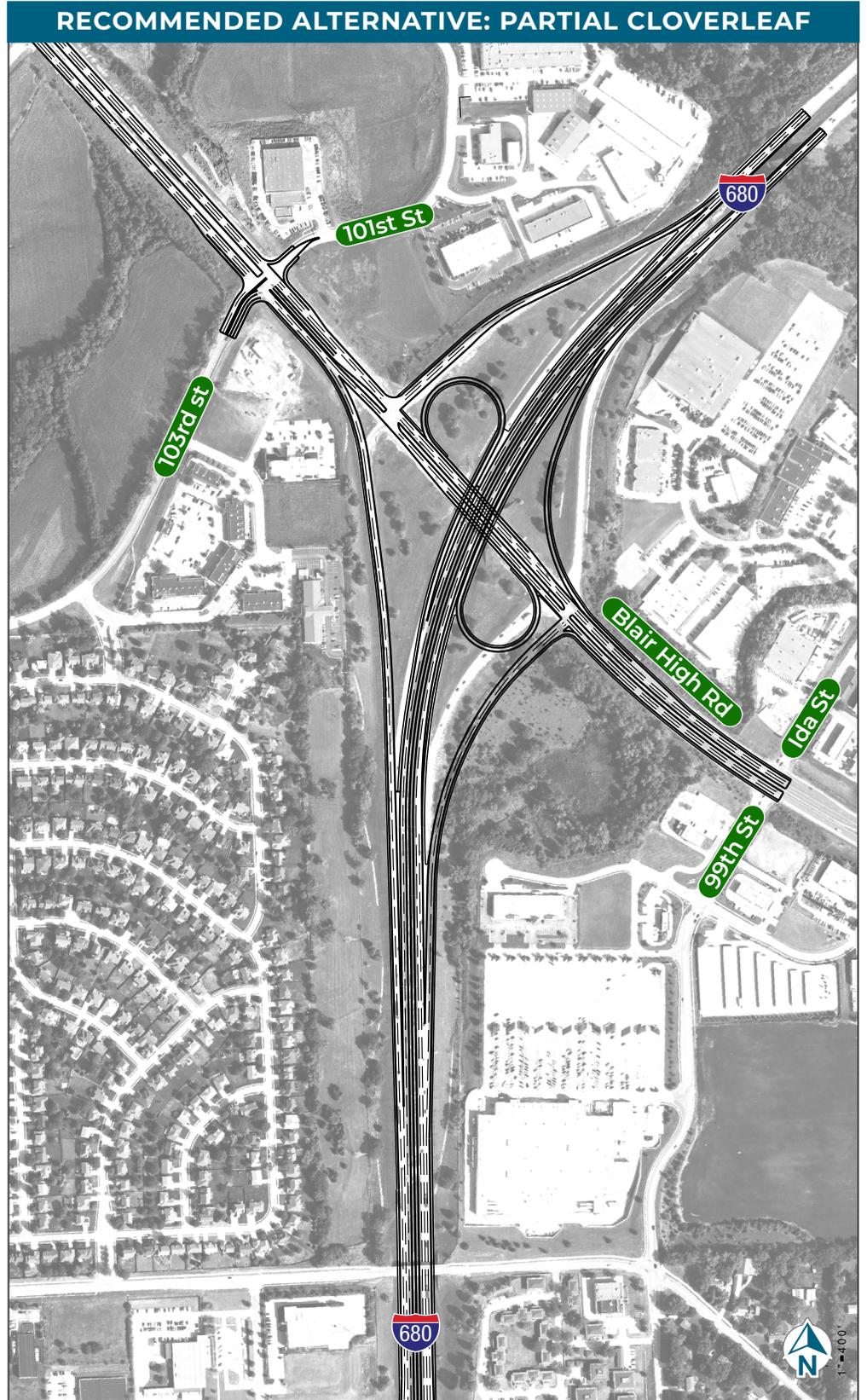
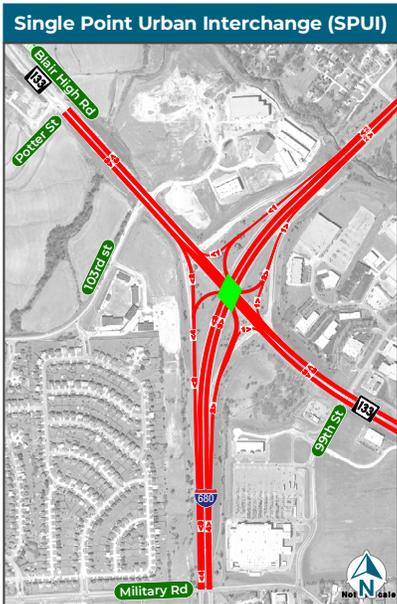
Diverging Diamond Interchange (DDI)



RECOMMENDED ALTERNATIVE: HALF PARTIAL CLOVERLEAF



BLAIR HIGH ROAD ALTERNATIVES CONSIDERED



CONSTRUCTION & SCHEDULE



TRAFFIC ACCOMMODATIONS

- The project would be constructed under traffic with lane closures controlled by appropriate traffic control devices and practices.
- Ramps maintained to the extent possible.
- Temporary surfacing may be required to accommodate phased construction.
- Access to community resources, area schools, police, and fire services would be coordinated throughout construction.
- No long-term highway detours are anticipated. Temporary ramp or local street closures may occur, primarily overnight, with traffic redirected to adjacent interchanges or local streets.

SCHEDULE & COST

This project schedule is based on several assumptions and is subject to change based on the availability of funds, agency approvals, etc.



Estimated Cost: \$66.8 Million*

*Funding comes from federal, state and local sources.

The proposed project would require the acquisition of additional property rights, which could include new right-of-way (ROW), control of access (CA), permanent easements (PE), and/or temporary easements (TE). Access to adjacent properties would be maintained during construction but may be limited at times due to phasing requirements.

THANK YOU!

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I-680, FORT ST. AND BLAIR HIGH RD. (N-133) IMPROVEMENTS

PROVIDE YOUR COMMENTS



Comments will be accepted through April 6, 2026 and can be submitted on the project website at ndot.info/22860 or to:

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402.318.1324

Tom Goodbarn
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402.595.2534

GET MORE INFORMATION



ndot.info/22860

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried out by NDOT pursuant to either 23 USC 326 and a Second Renewed Memorandum of Understanding dated September 12, 2024 executed by FHWA and NDOT or pursuant to 23 USC 327 and a Memorandum of Understanding dated February 23, 2026 and executed by FHWA and NDOT, as applicable.