

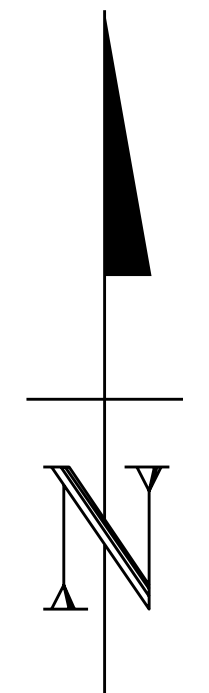
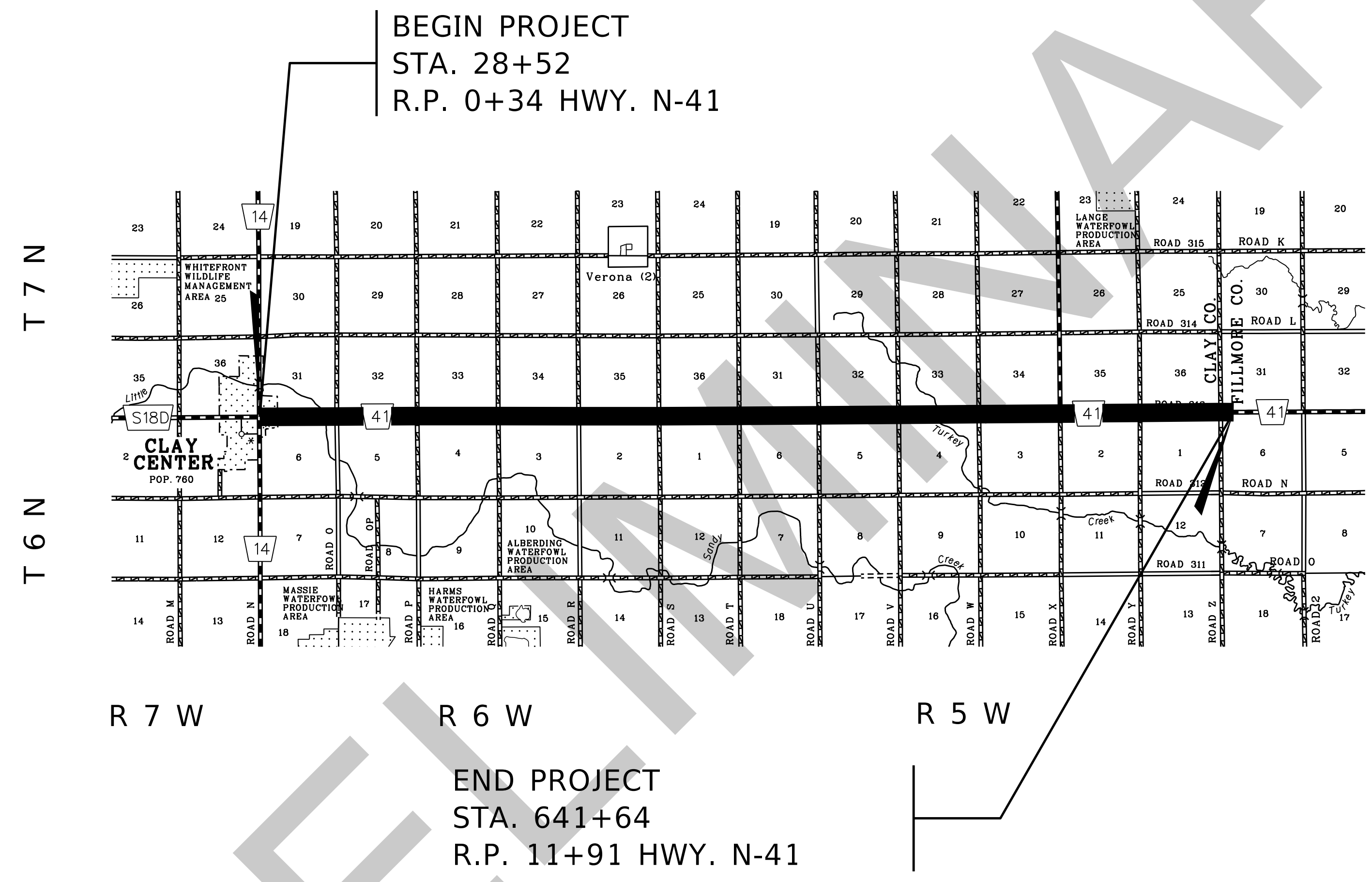
STATE OF NEBRASKA
DEPARTMENT OF TRANSPORTATION
PLANS FOR CONSTRUCTION
CLAY CENTER EAST
CLAY COUNTY

THE WORK ON THIS PROJECT CONSISTS OF GROUPS 1 - GRADING, 3 - CONCRETE PAVEMENT, 4 - CULVERTS, 5 - SEEDING, 6 - BRIDGE, 7 - GUARDRAIL, 8 - MISCELLANEOUS, 9 - BITUMINOUS & 10 - GENERAL	
▲ GROUPS <u>1, 3, 4, 5, 6, 7, 8, 9 & 10</u> ARE INCLUDED IN THE LETTING OF <u>August 29, 2024</u>	
▲ GROUPS _____ ARE INCLUDED IN THE LETTING OF _____	
■ GROUPS _____ ARE INCLUDED IN THE LETTING OF _____	

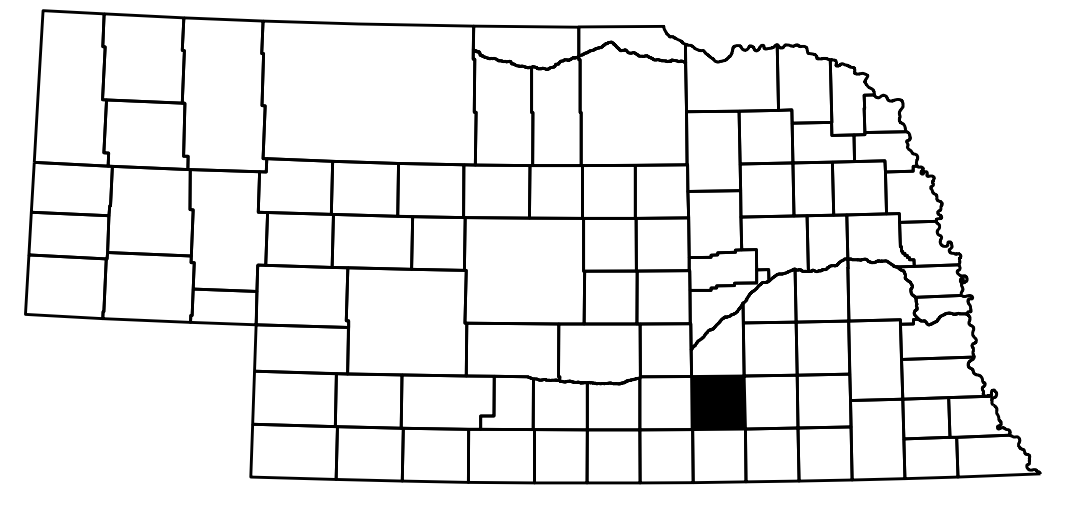
A1
Project Number STP-41-5(108)
C.N. 42829
▲ C.N.
▲ C.N.
■ C.N.

THE 2017 EDITION OF THE NEBRASKA STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS APPLY TO THIS PROJECT.

DESIGN DESIGNATION	
3R RURAL TRAFFIC	
YEAR: <u>2024</u>	<u>2034</u>
ADT: <u>495</u>	<u>495</u>
DHV: <u>-</u>	<u>-</u>
T= <u>11</u> %	D= <u>-</u> %



Project Raw Materials (Tons)	00000
Post Consumer Recycle Content in Project Raw Materials (Tons)	00000
Post Consumer Recycle Content	00000
Estimated Value of Post Consumer Content Recycled	00000



COMPUTER: BG0419M674
DATE: 21-MAR-2024 12:48
FILE: 428290_NDOT_Title_and_Index.dgn

REFERENCE POST NO. 0+34 TO REFERENCE POST NO. 11+91
TOTAL NET LENGTH OF PROJECT: 61,412 FEET 11.63 MILES



Mick Syslo, am the Coordinating Professional on the Clay Center East project.

COMPUTER: BG0419M674

DATE: 21-MAR-2024 12:35

FILE: 428290_NDOT_Title_and_Index.dgn

SHEET NO.

A1	TITLE PAGE
A2	INDEX OF SHEETS
B1 - B2	TYPICAL CROSS SECTIONS
C1	SUMMARY OF QUANTITIES
E1 - E11	AERIAL PHOTO SHEETS
F1 - F15	HORIZONTAL ALIGNMENT & ORIENTATION
G1 - G2	GENERAL INFORMATION
H1 - H6	PHASING PLANS
J1 - J8	EROSION & SEDIMENT CONTROL
L1 - L11	PLAN SHEETS
M1	TRAFFIC CONTROL PLAN--CULVERT @ STA. 112+99 PHASE 1
M2	TRAFFIC CONTROL PLAN--CULVERT @ STA. 112+99 PHASE 2
M3	TRAFFIC CONTROL PLAN--CULVERT @ STA. 429+01 PHASE 1
M4	TRAFFIC CONTROL PLAN--CULVERT @ STA. 429+01 PHASE 2
M5	TYPICAL TRAFFIC CONTROL PLAN--LANE CLOSURE AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD)
M6 - M7	TEMPORARY TRAFFIC SIGNAL PLAN--SHORT TERM CLOSURE
M8 - M9	TRAFFIC CONTROL PLAN--MOBILE OPERATIONS TWO-LANE AND MULTI-LANE
M10	TYPICAL TRAFFIC CONTROL PLAN--ONE LANE, TWO WAY OPERATION WITH BARRIERS
M11	TYPICAL SIGNAL CONTROL PLAN--TEMPORARY TRAFFIC SIGNAL DETAILS
M12	TYPICAL TRAFFIC CONTROL PLAN--INERTIAL BARRIER SYSTEM
Q1	EARTHWORK DATA SHEETS

STANDARD PLANS

307-R3	(2 SHEETS) MAILBOX TURNOUT
410-R4	(2 SHEETS) FLARED END SECTIONS FOR CULVERT PIPES
411-R2	(4 SHEETS) BEDDING AND BACKFILL REQUIREMENTS FOR CONCRETE PIPE
501-R7	(3 SHEETS) EROSION CONTROL

PRELIMINARY

A2

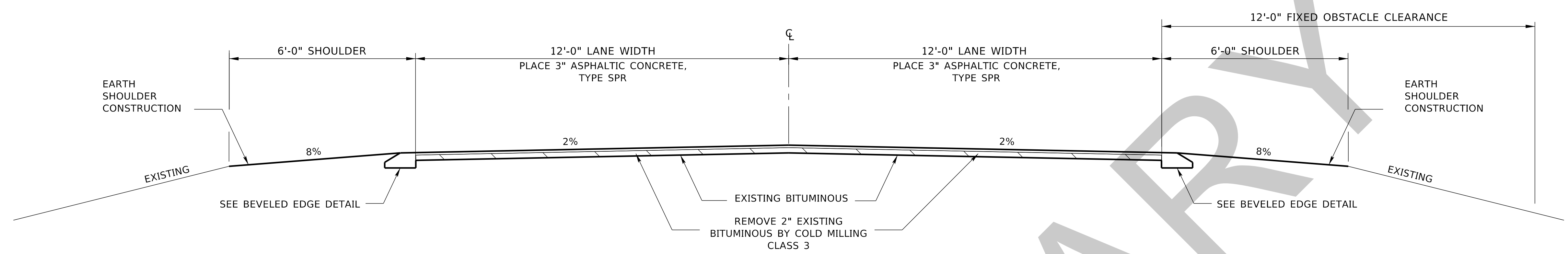
Project Number
41-5(108)

C.N. 42829

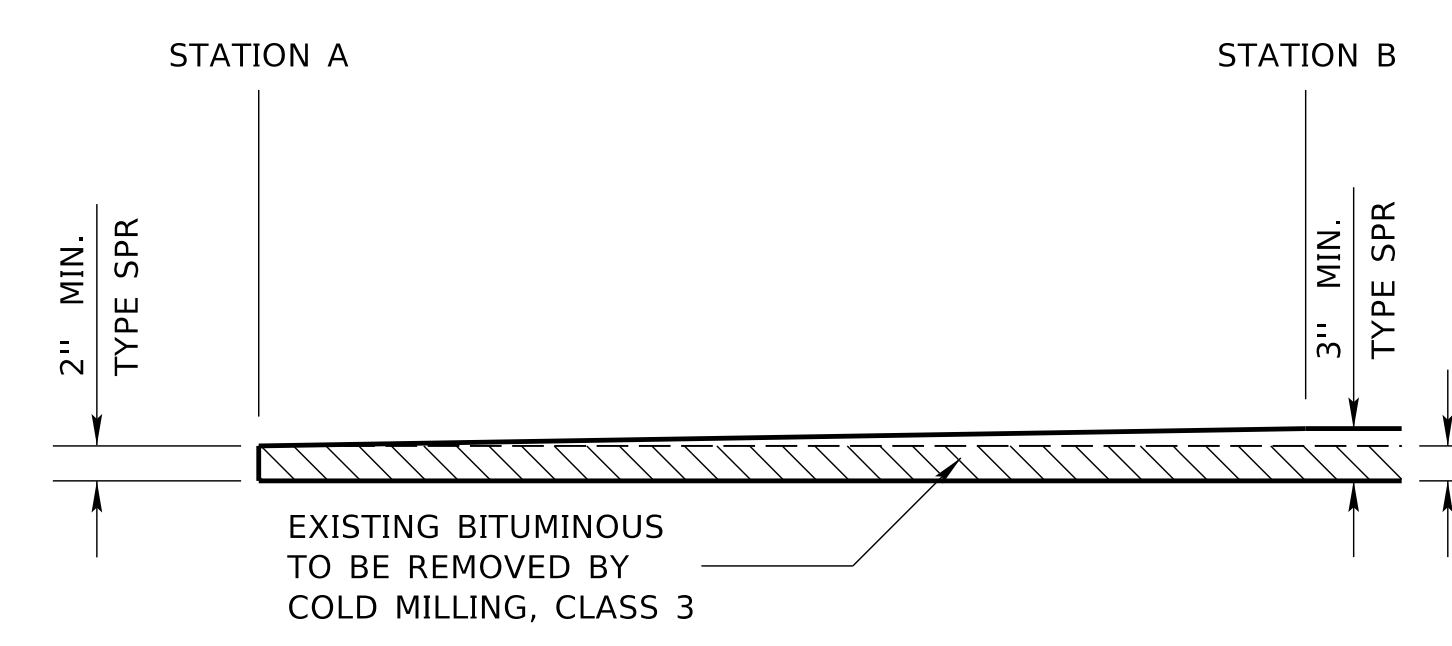
INDEX OF SHEETS



Roadway
Design
Division

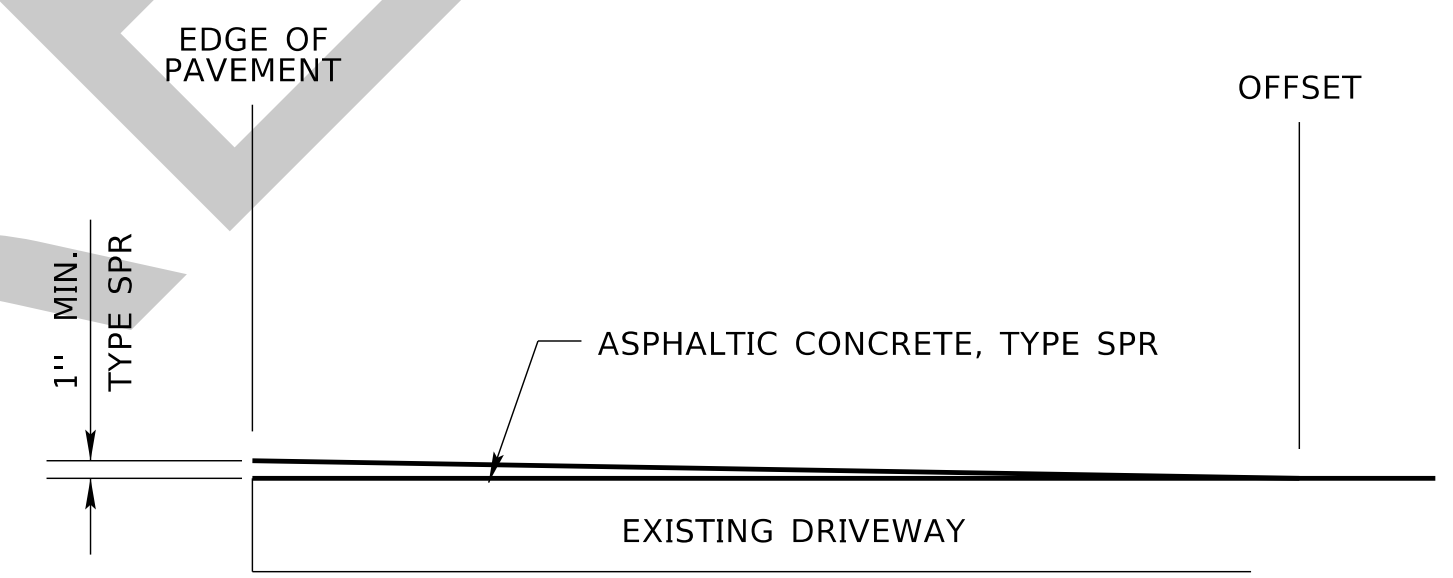
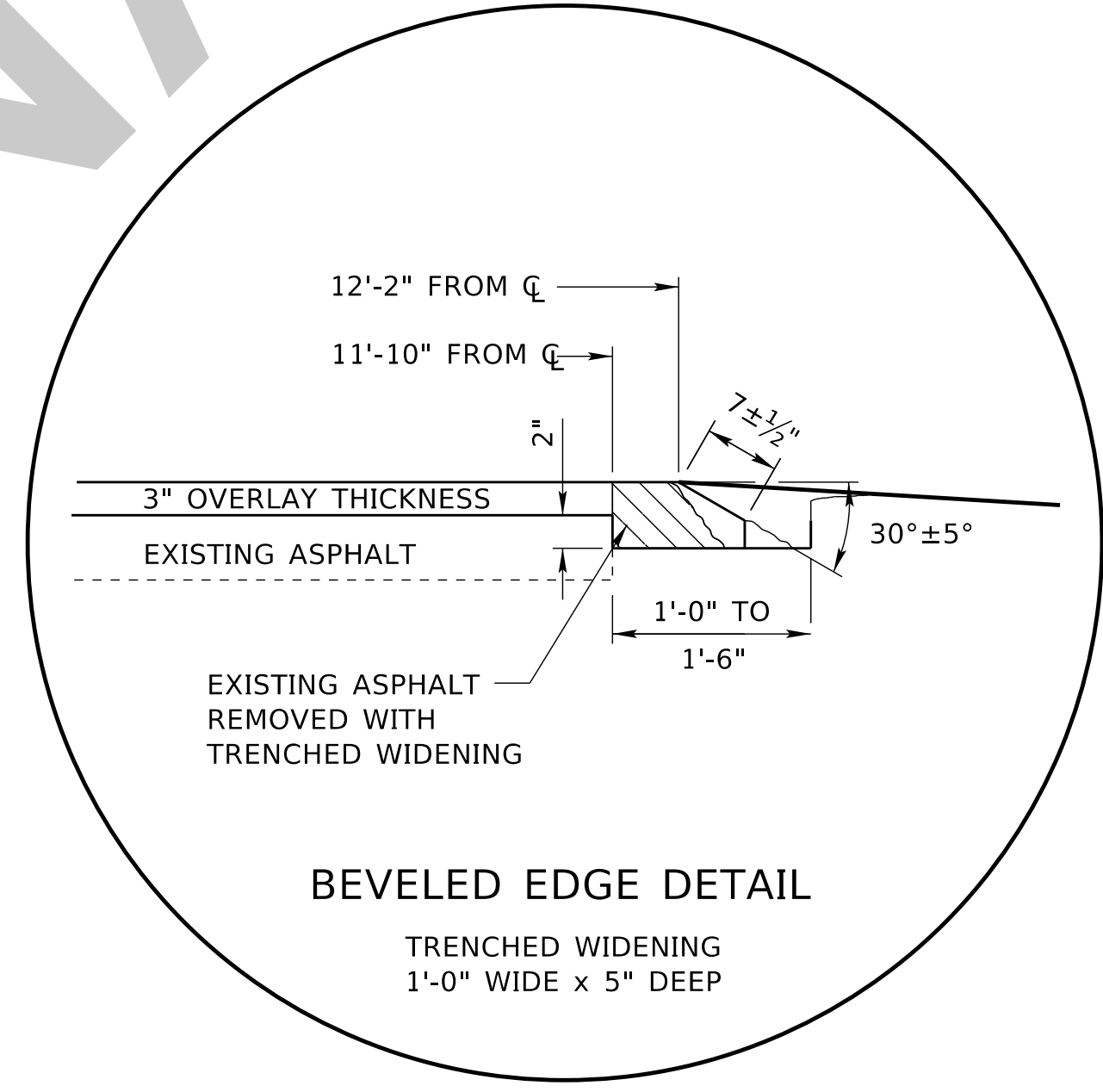


HWY. N-41
STATION TO STATION
28+02 - 642+14
(EXCEPT AS SHOWN ELSEWHERE)



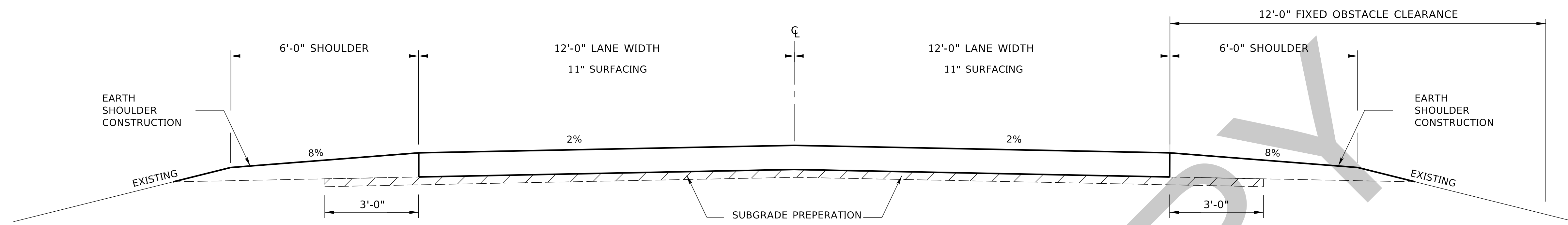
INLAY DETAIL

STATION A	STATION B
28+02 642+14	28+52 641+64



DRIVEWAY FEATHER DETAIL

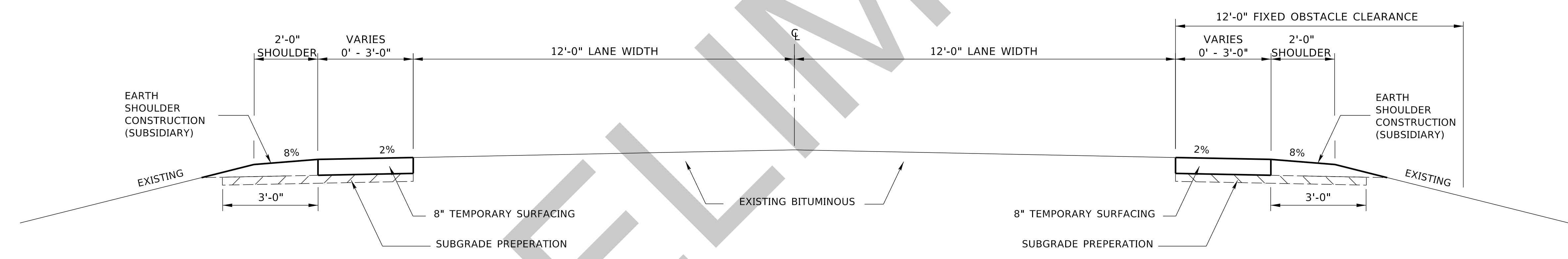
STATION	SIDE	OFFSET
136+30	RT	5'
139+91	RT	5'
155+01	LT	5'



SURFACING AT CULVERT REPLACEMENTS

STATION TO STATION

67+29	-	67+41
83+00	-	83+18
103+46	-	103+56
112+94	-	113+04
122+29	-	122+40
320+53	-	320+63
357+50	-	357+64
362+31	-	362+42
383+35	-	383+46
396+47	-	396+59
428+96	-	429+06
463+33	-	463+43
630+02	-	630+23



TEMPORARY SURFACING AT CULVERT REPLACEMENTS

STATION TO STATION

65+39	-	69+32
81+09	-	85+09
101+54	-	105+48
111+03	-	114+95
120+39	-	124+32
318+62	-	322+54
355+59	-	364+33
381+45	-	385+38
394+58	-	398+50
427+05	-	430+97
461+42	-	465+34
628+12	-	632+14

STATION TO STATION

65+39	-	69+32
81+10	-	85+10
101+55	-	105+47
111+03	-	114+95
120+38	-	124+31
318+62	-	322+54
355+59	-	364+33
381+44	-	385+37
394+57	-	398+49
427+05	-	430+97
461+42	-	465+34
628+11	-	632+14

TYPICAL CROSS SECTIONS

PRELIMINARY

C1

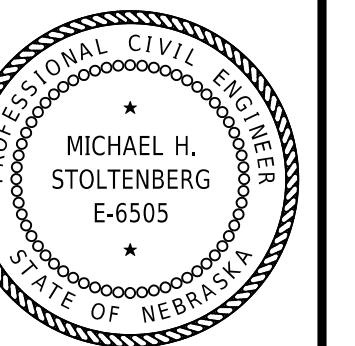
Project Number
41-5(108)

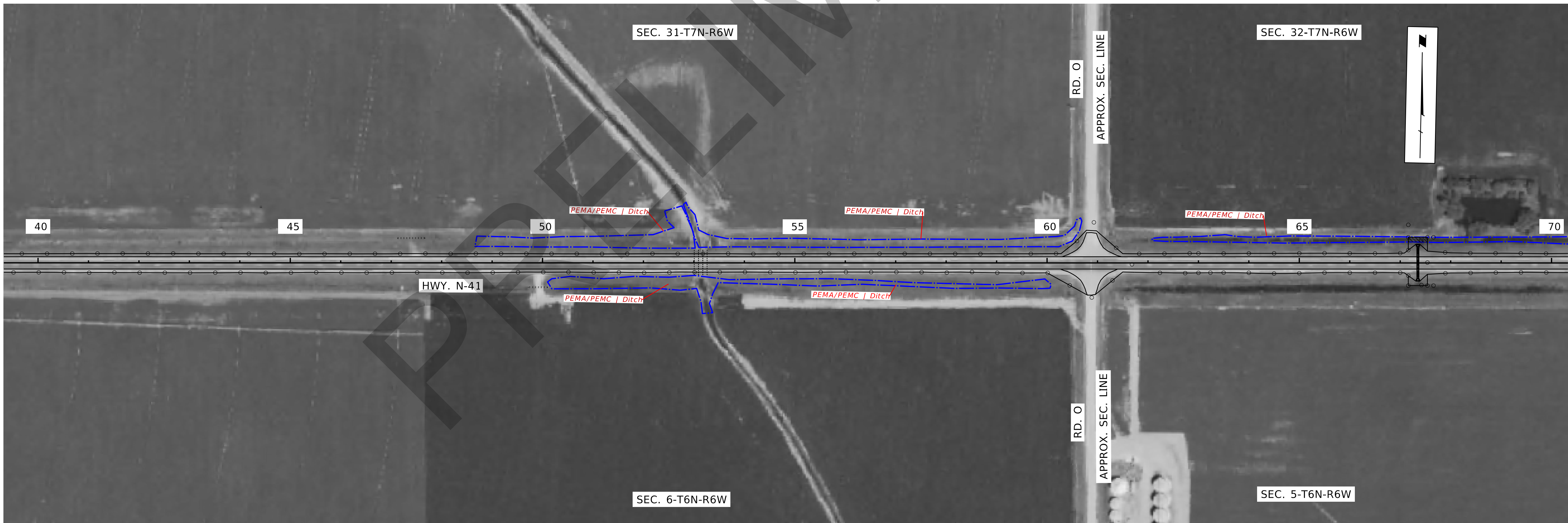
C.N. 42829

SUMMARY OF QUANTITIES

NEBRASKA
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DEPARTMENT OF TRANSPORTATION

Roadway
Design
Division





E1

Project Number
41-5(108)

C.N. 42829

DATE: 2018
FLIGHT: NAIP

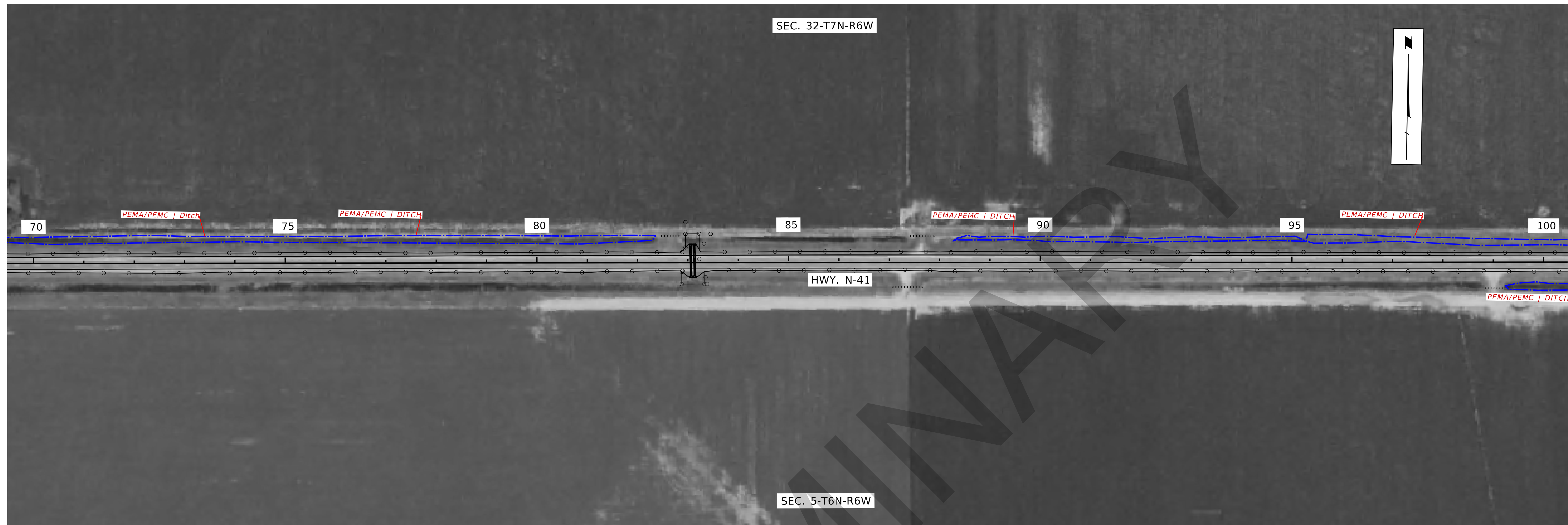
- LEGEND**
- LIMITS OF CONSTRUCTION
 - ▨ WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
 - ▨ TEMPORARY IMPACTED WETLANDS
 - SEN - SENSITIVE AREA - DO NOT ENTER -
 - TRAIL - SENSITIVE TRAIL

ENVIRONMENTAL

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Roadway Design Division

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E2

Project Number
41-5(108)

C.N. 42829

DATE: 2018
FLIGHT: NAIP

LEGEND

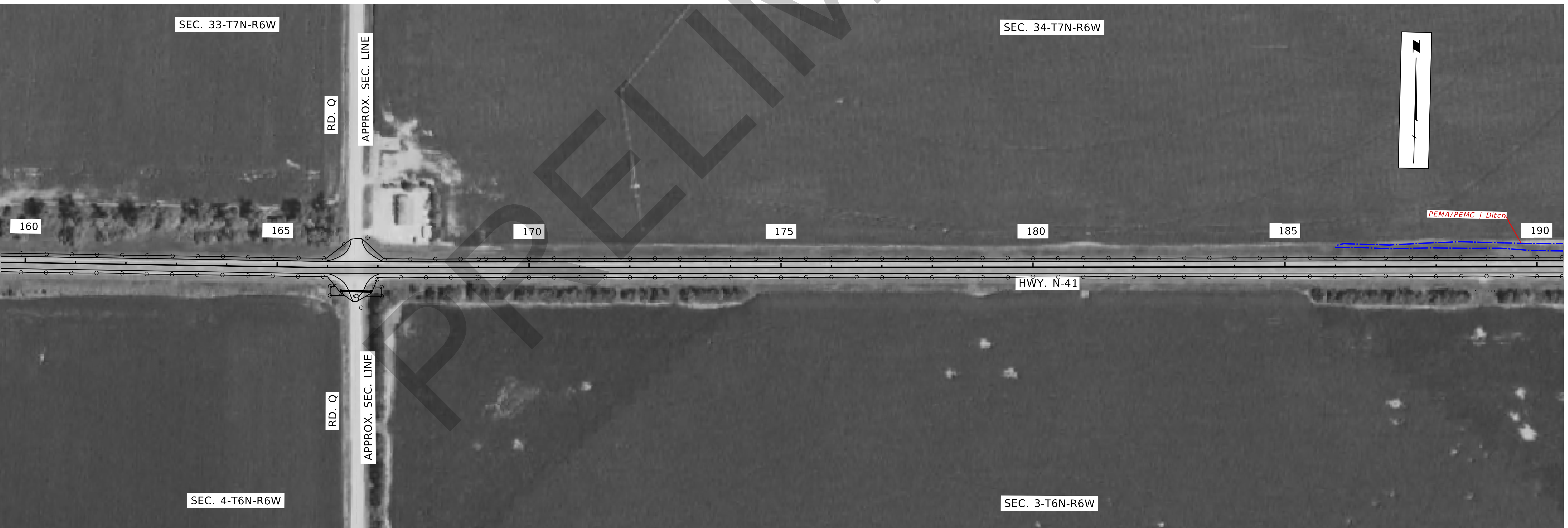
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	WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
	TEMPORARY IMPACTED WETLANDS
	- SEN - SENSITIVE AREA - DO NOT ENTER -
	- TRAIL - SENSITIVE TRAIL

ENVIRONMENTAL

NEBRASKA
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Roadway Design Division

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E3

Project Number
41-5(108)

C.N. 42829

DATE: 2018
FLIGHT: NAIP

- LEGEND**
- LIMITS OF CONSTRUCTION
 - WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
 - TEMPORARY IMPACTED WETLANDS
 - SEN - SENSITIVE AREA - DO NOT ENTER -
 - TRAIL - SENSITIVE TRAIL

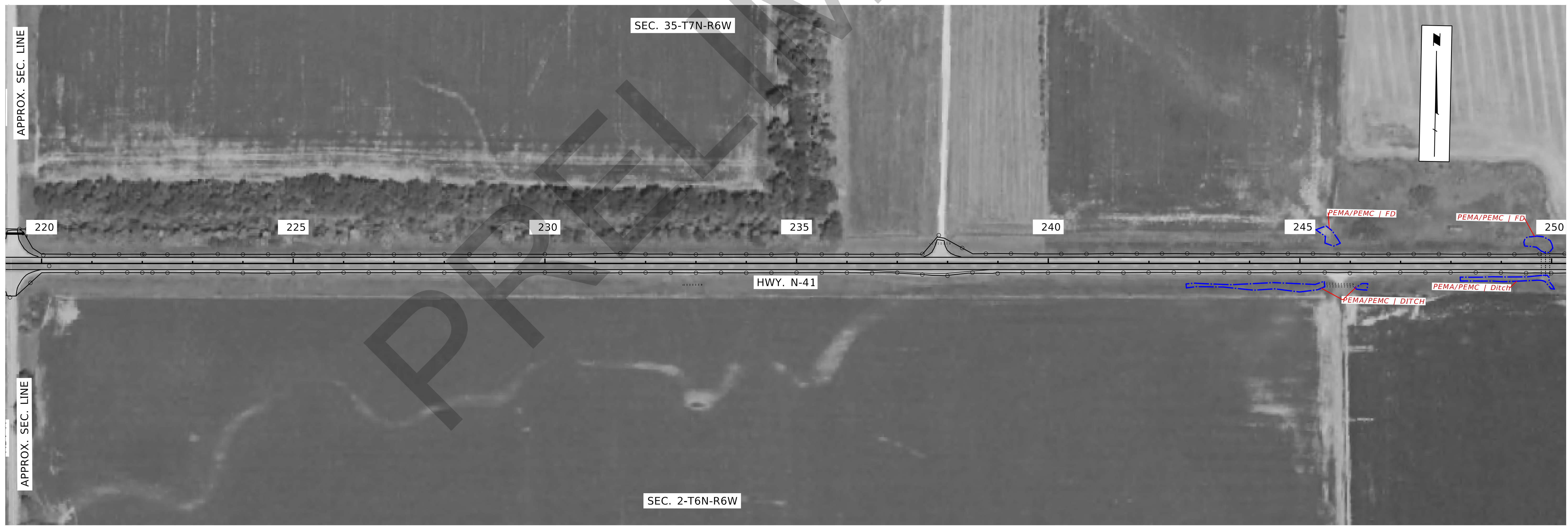
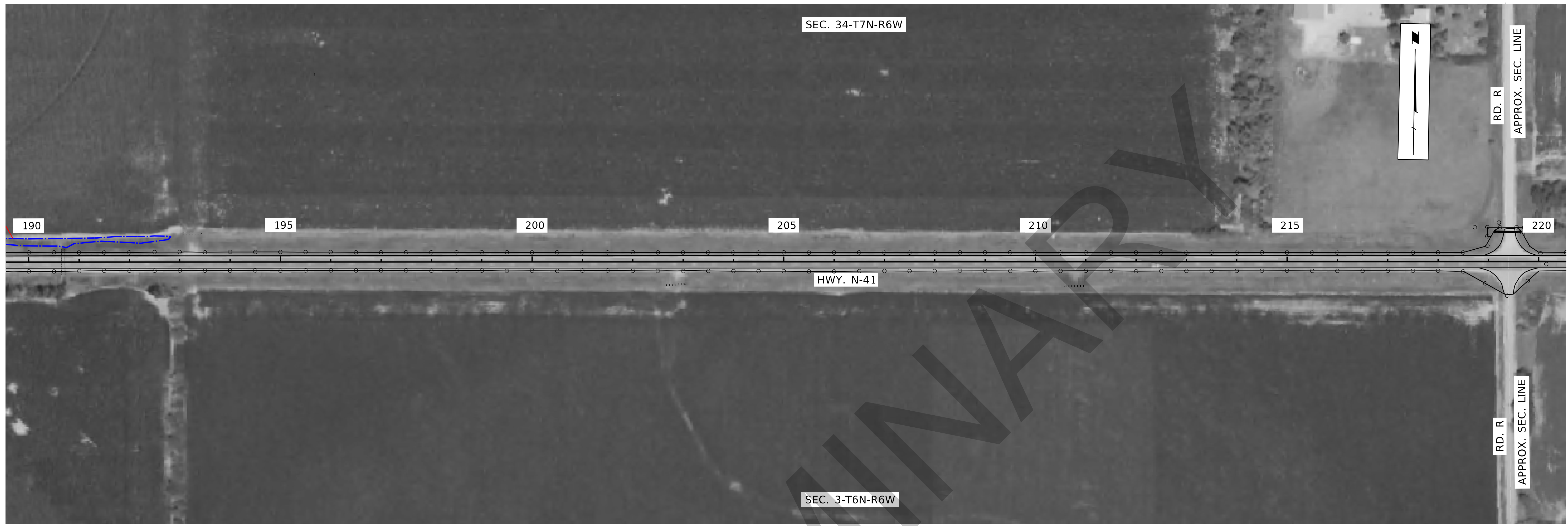
ENVIRONMENTAL

NEBRASKA
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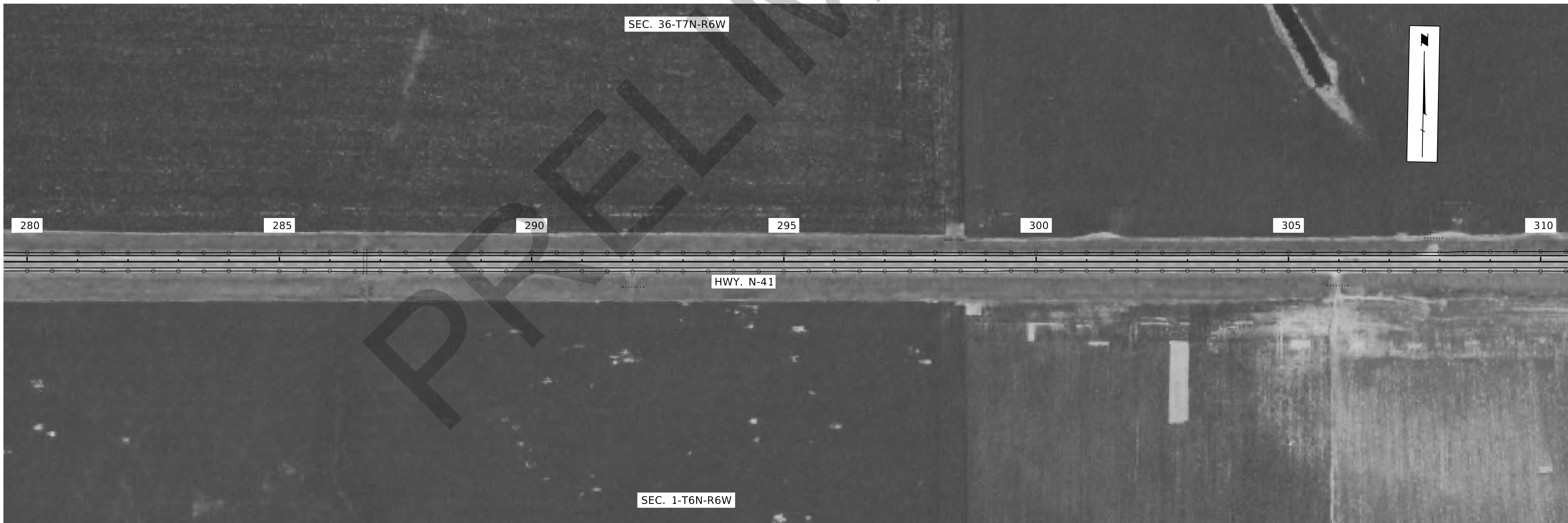
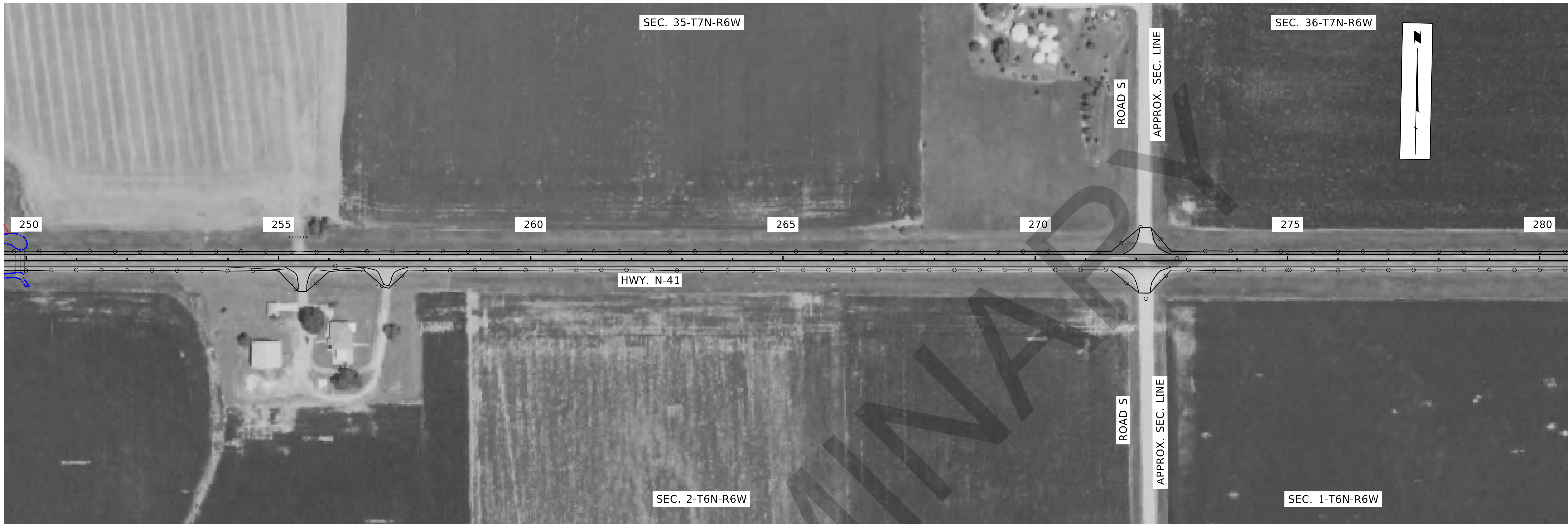
Roadway Design Division

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Project Number	41-5(108)
C.N.	42829
DATE:	2018
FLIGHT:	NAIP
LEGEND	
	LIMITS OF CONSTRUCTION
	WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
	TEMPORARY IMPACTED WETLANDS
	- SEN - SENSITIVE AREA - DO NOT ENTER -
	- TRAIL - SENSITIVE TRAIL
ENVIRONMENTAL	
 NEBRASKA Good Life. Great Journey. DEPARTMENT OF TRANSPORTATION	
Roadway Design Division	



E5

Project Number
41-5(108)

C.N. 42829

DATE: 2018
FLIGHT: NAIP

LEGEND

- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
- TEMPORARY IMPACTED WETLANDS
- SEN - SENSITIVE AREA - DO NOT ENTER -
- TRAIL - SENSITIVE TRAIL

ENVIRONMENTAL

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Roadway Design Division

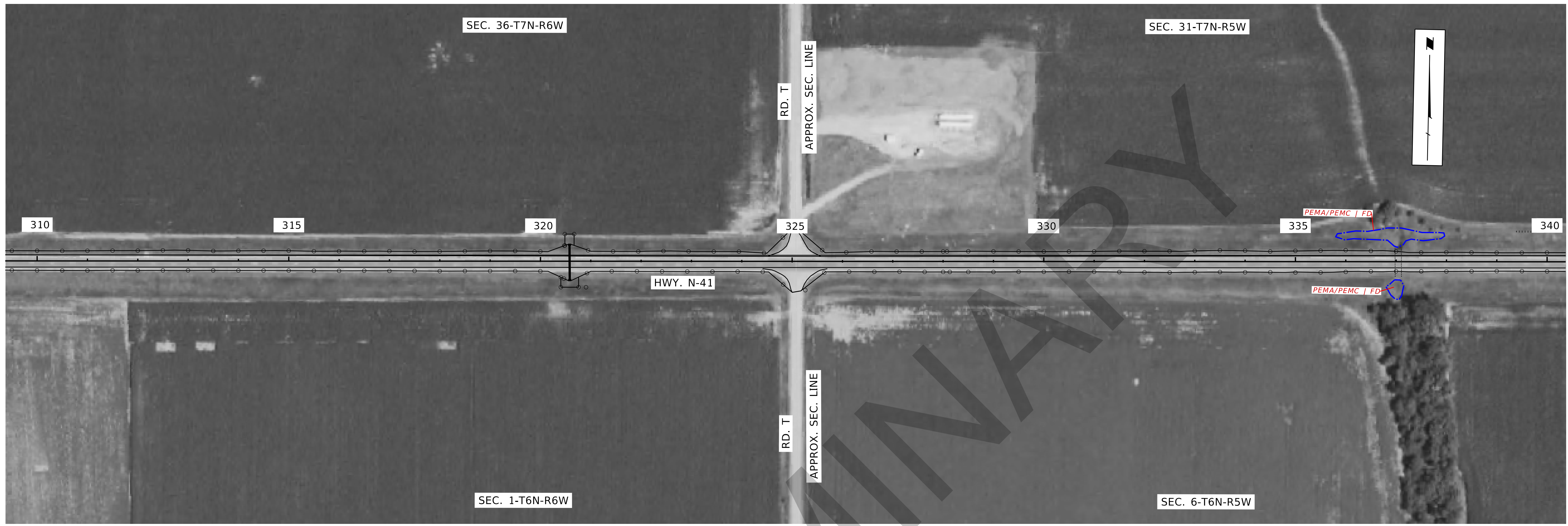
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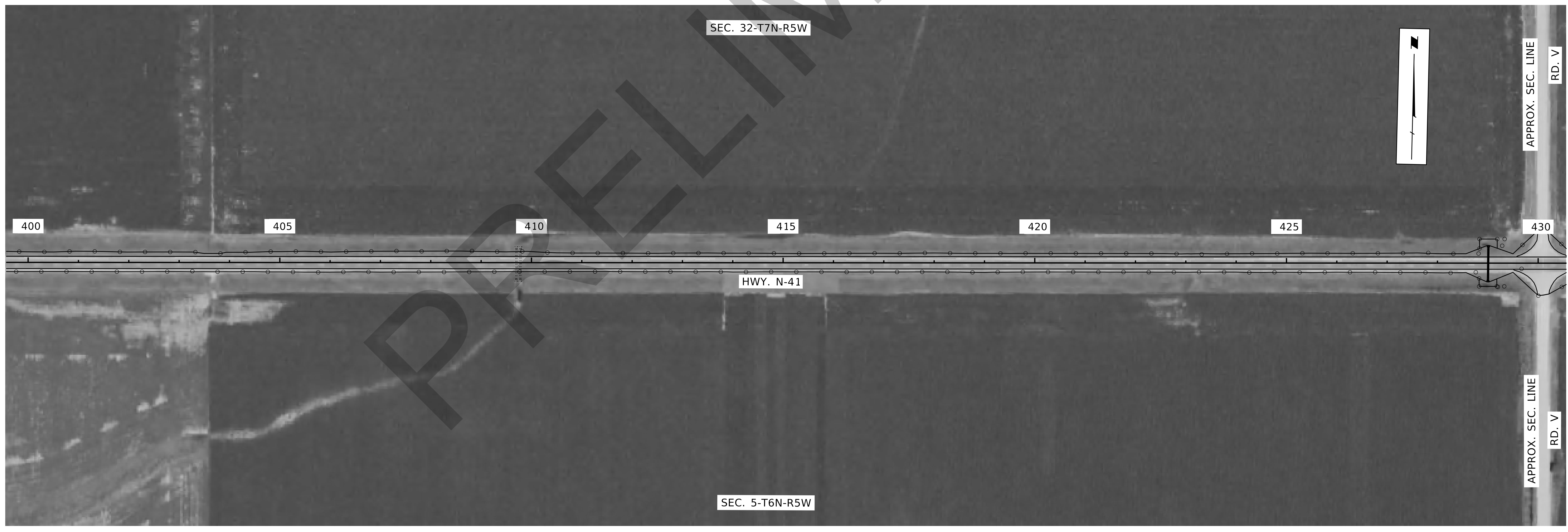
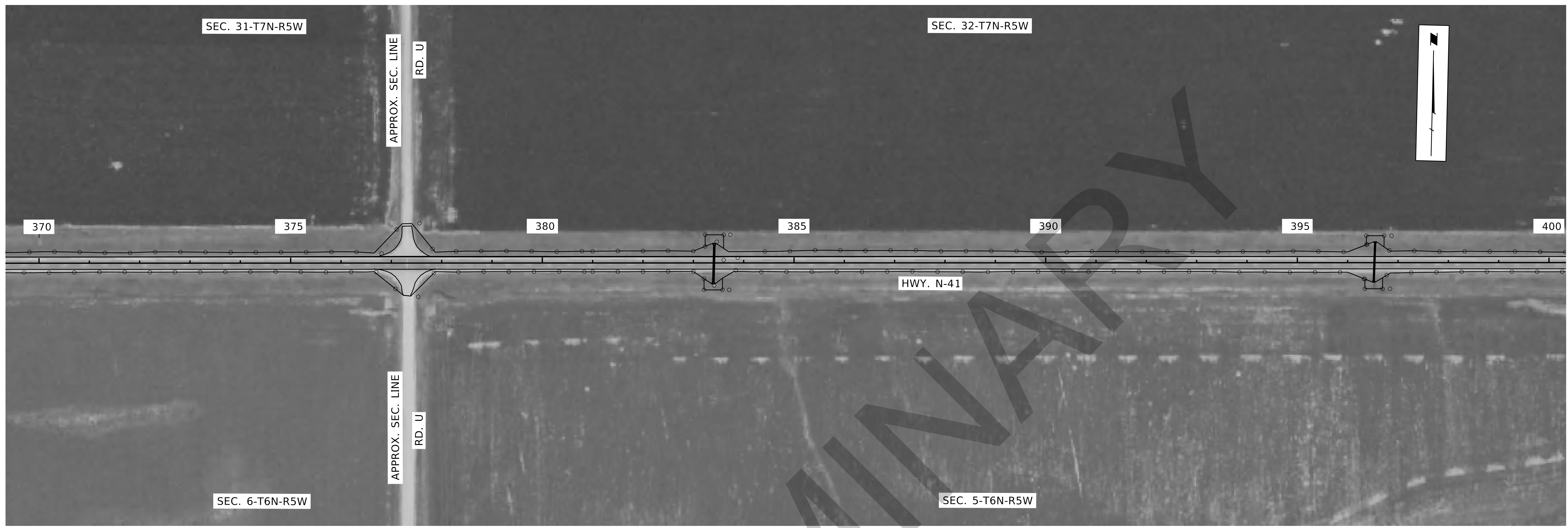
- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
- TEMPORARY IMPACTED WETLANDS
- SEN - SENSITIVE AREA - DO NOT ENTER - TRAIL - SENSITIVE TRAIL



ENVIRONMENTAL



Roadway Design Division



E7	
Project Number	41-5(108)
C.N.	42829
DATE:	2018
FLIGHT:	NAIP
LEGEND	
	LIMITS OF CONSTRUCTION
	WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
	TEMPORARY IMPACTED WETLANDS
	SENSITIVE AREA - DO NOT ENTER
	SENSITIVE TRAIL

ENVIRONMENTAL

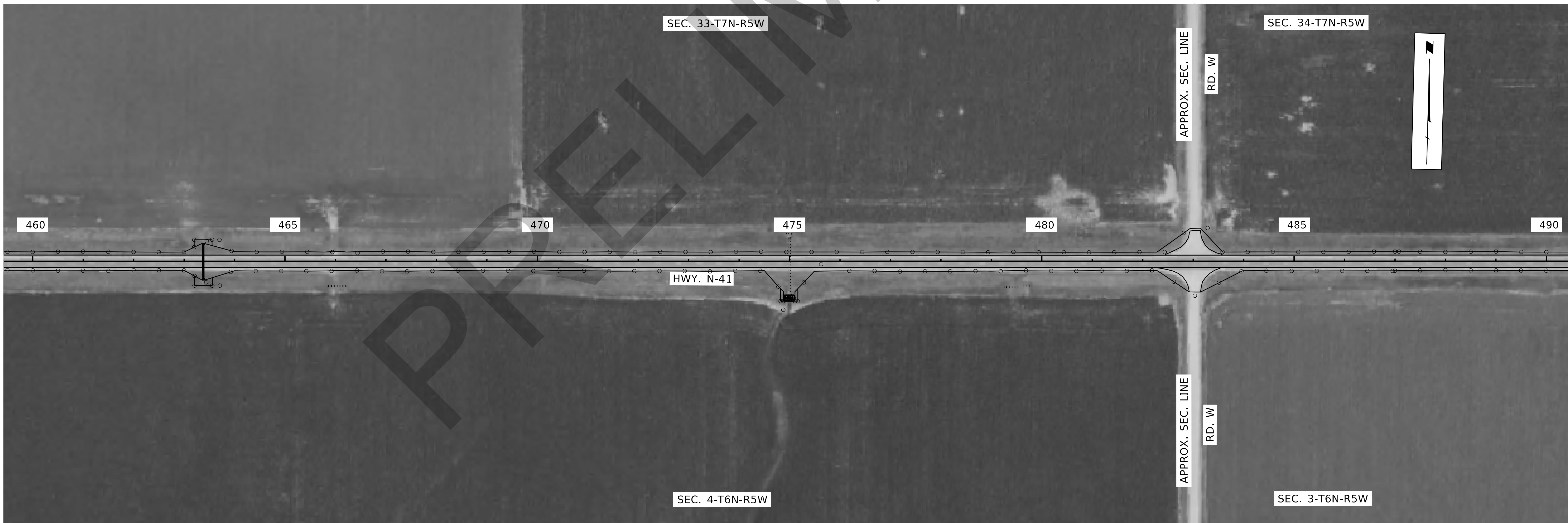
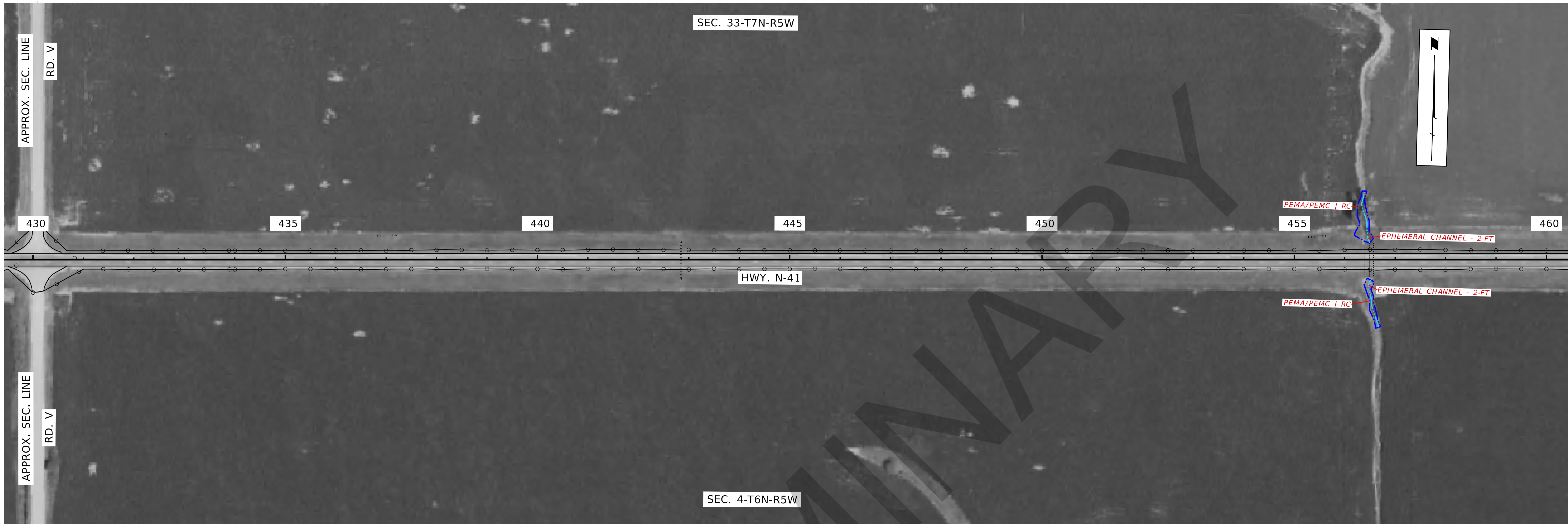
NEBRASKA
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Roadway Design Division

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DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Environmental.dgn



E8

Project Number
41-5(108)

C.N. 42829

DATE: 2018
FLIGHT: NAIP

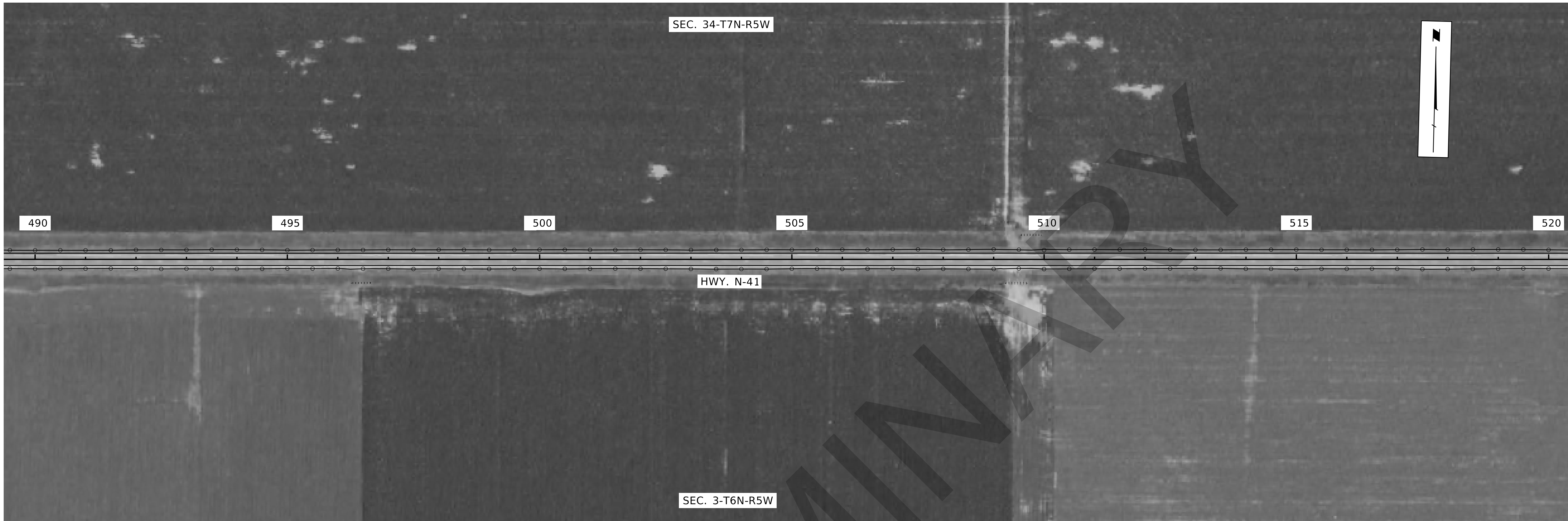
LEGEND

- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
- TEMPORARY IMPACTED WETLANDS
- SEN - SENSITIVE AREA - DO NOT ENTER - TRAIL - SENSITIVE TRAIL

ENVIRONMENTAL

NEBRASKA
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DEPARTMENT OF TRANSPORTATION

Roadway Design
Division



E9

Project Number
41-5(108)

C.N. 42829

DATE: 2018
FLIGHT: NAIP

- LEGEND**
- LIMITS OF CONSTRUCTION
 - ▨ WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
 - ▨ TEMPORARY IMPACTED WETLANDS
 - SEN - SENSITIVE AREA - DO NOT ENTER -
 - TRAIL - SENSITIVE TRAIL

ENVIRONMENTAL

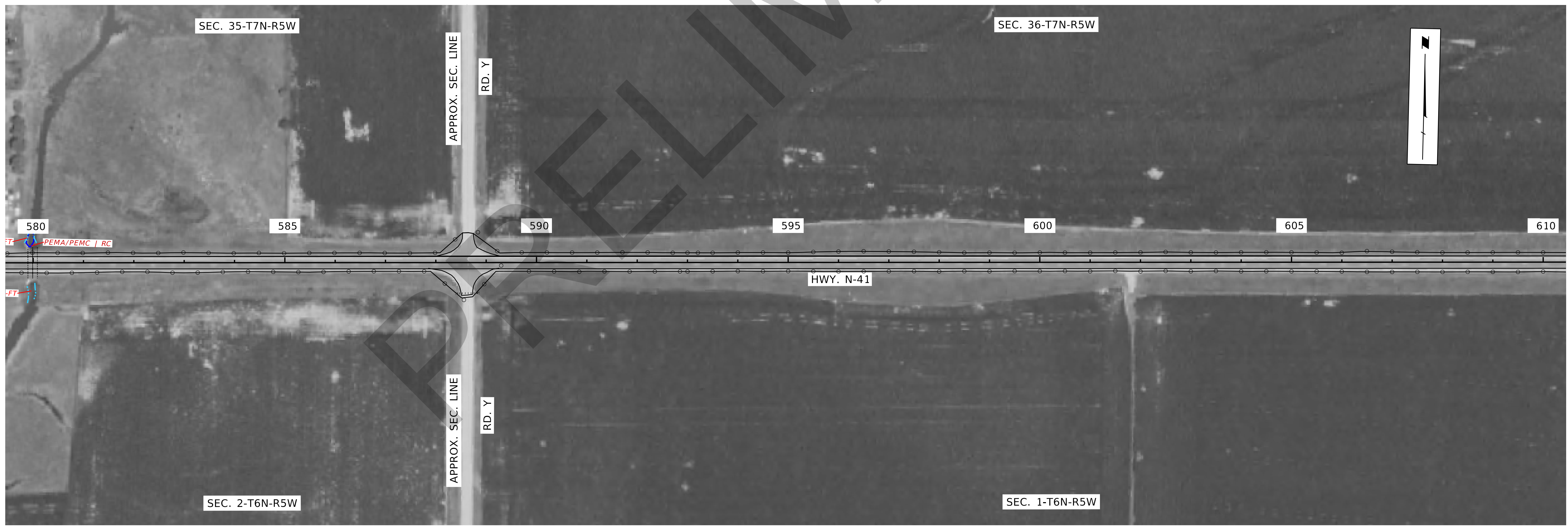
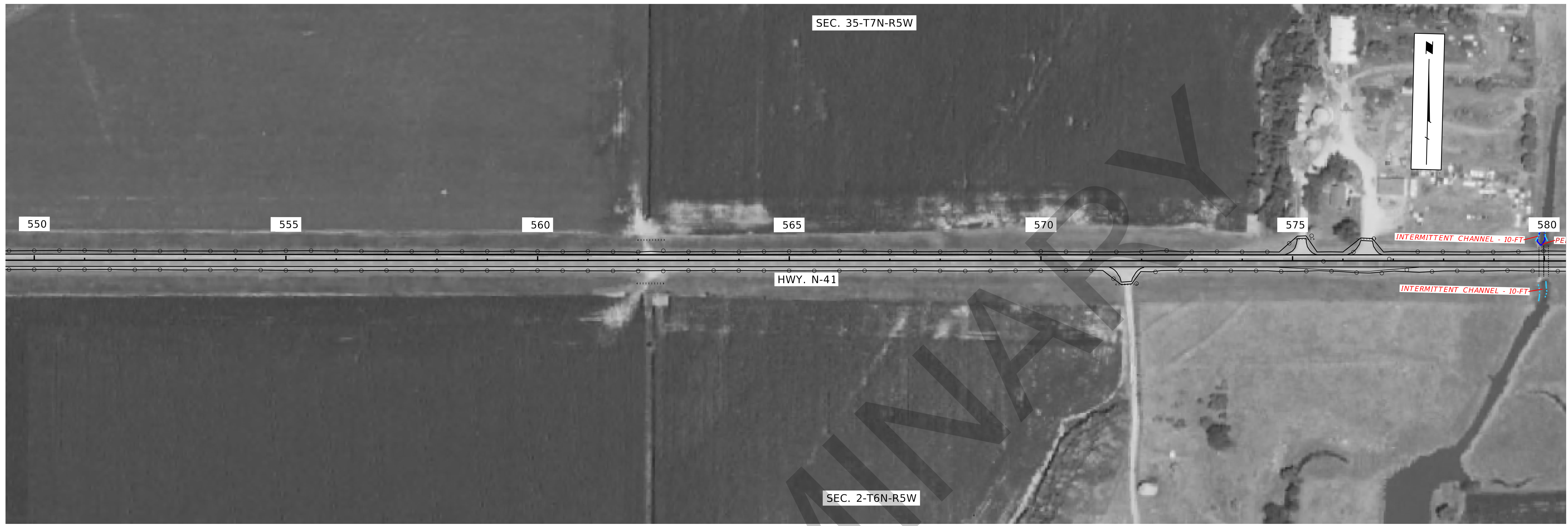
NEBRASKA
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DEPARTMENT OF TRANSPORTATION

Roadway Design
Division

COMPUTER: BG0419M674

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FILE: 42829 Sheets Environmental.dgn



E10

Project Number
41-5(108)

C.N. 42829

DATE: 2018
FLIGHT: NAIP

LEGEND

- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
- TEMPORARY IMPACTED WETLANDS
- SEN - SENSITIVE AREA - DO NOT ENTER -
- TRAIL - SENSITIVE TRAIL

ENVIRONMENTAL

NEBRASKA
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Roadway Design Division

COMPUTER: BG0419M674

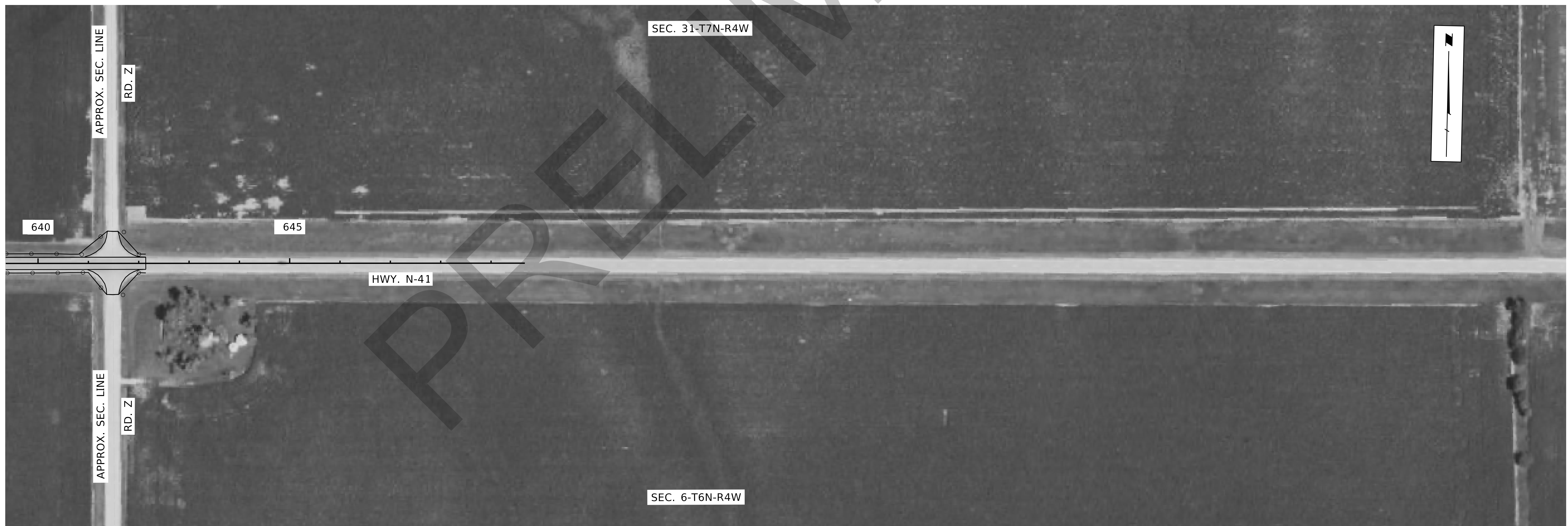
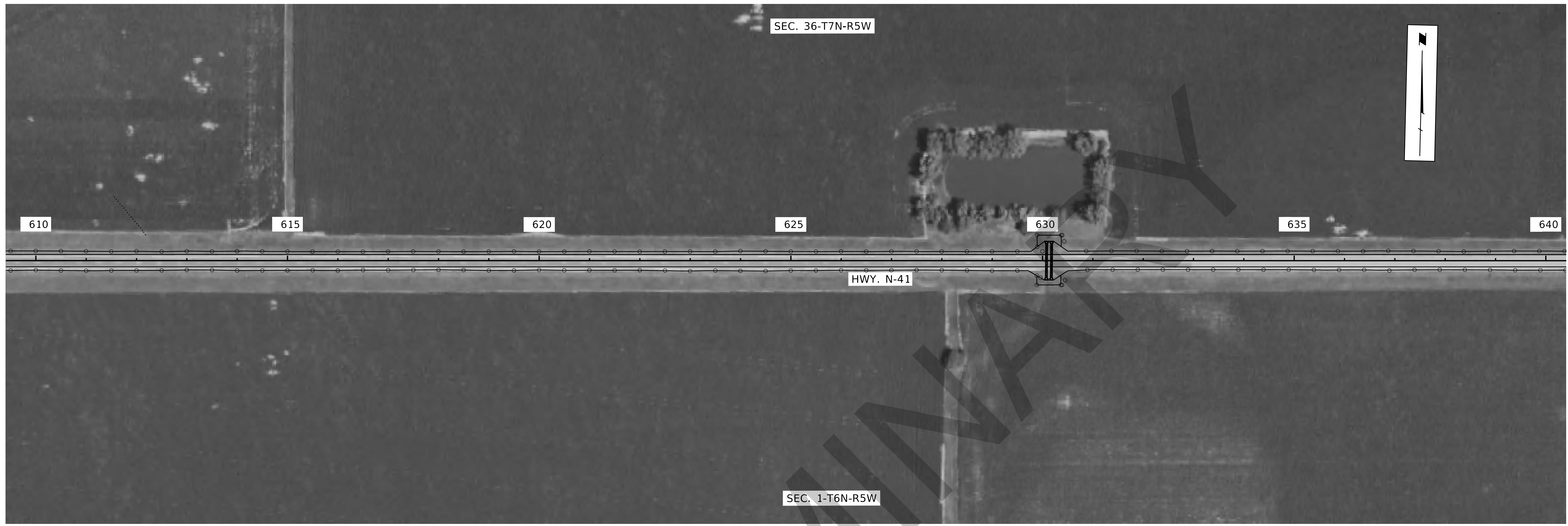
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FILE: 42829 Sheets Environmental.dgn



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Project Number	41-5(108)
C.N.	42829
DATE:	2018
FLIGHT:	NAIP
LEGEND	
	LIMITS OF CONSTRUCTION
	WETLANDS - DO NOT DISTURB - IMPACTED WETLANDS
	TEMPORARY IMPACTED WETLANDS
	- SEN - SENSITIVE AREA - DO NOT ENTER -
	- TRAIL - SENSITIVE TRAIL
ENVIRONMENTAL	
Roadway Design Division	

COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn



PRELIMINARY



F1

Project Number
41-5(108)

C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION



Roadway
Design
Division

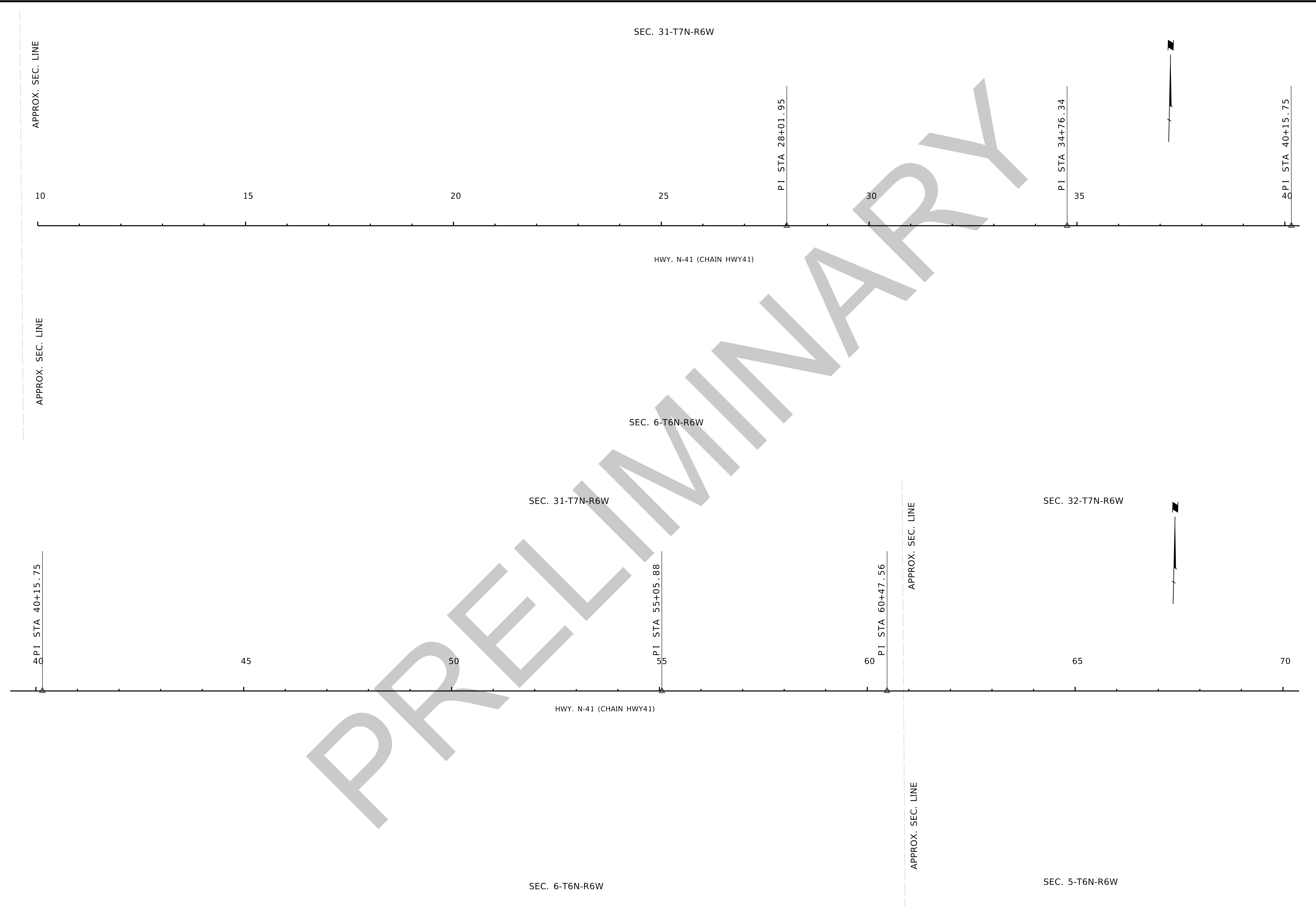
ALIGNMENT INFORMATION		
ALIGNMENT	CHAIN	PROFILE
HWY. N-41	HWY41	-

NOTE: ALIGNMENT HAS BEEN CREATED FROM ASBUILT INFORMATION AND MODIFIED TO REPRESENT THE FIELD SURVEY DATA COLLECTED.

COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn



F2

Project Number
41-5(108)

C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

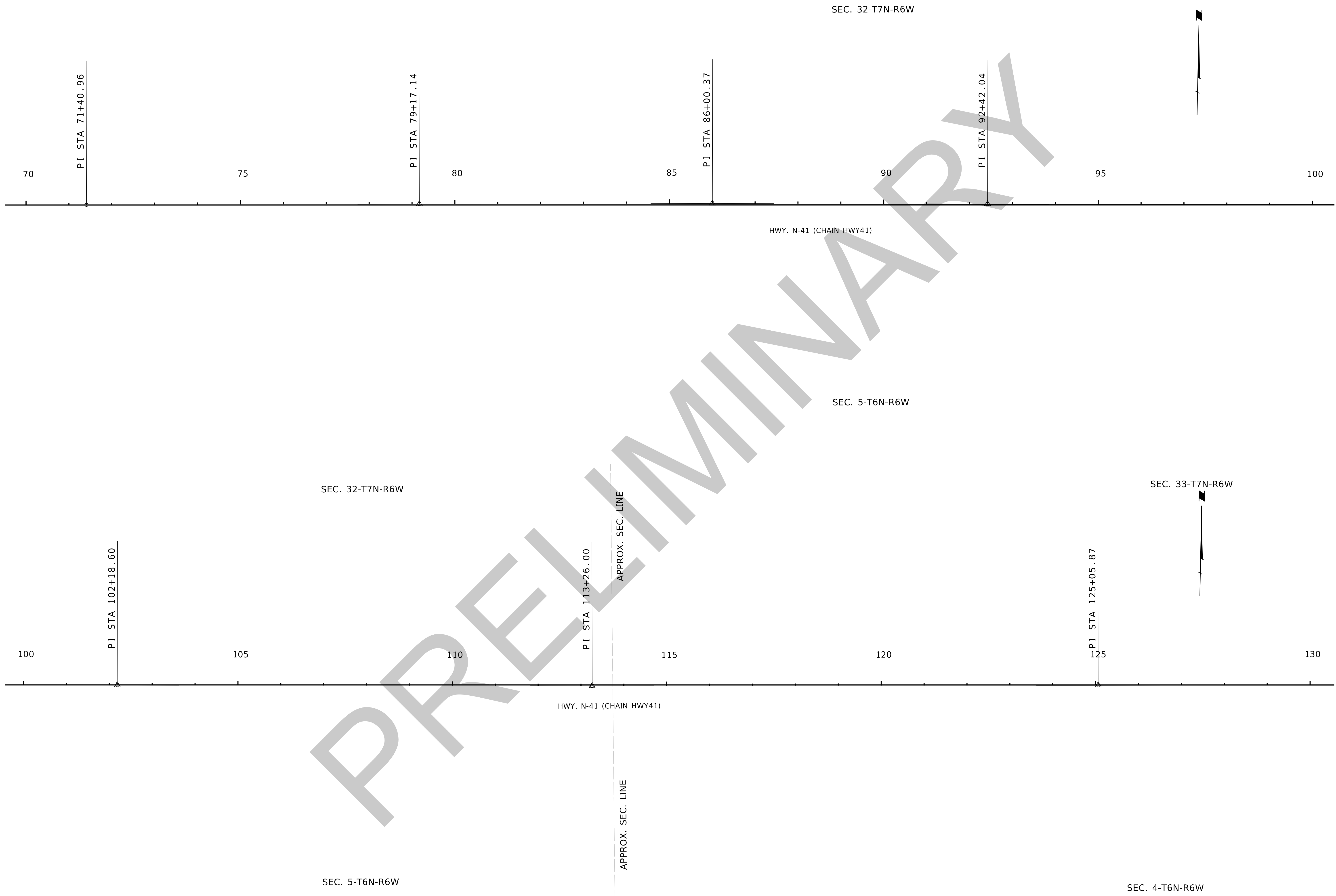


Roadway
Design
Division

COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn



F3

Project Number
41-5(108)

C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

NEBRASKA
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DEPARTMENT OF TRANSPORTATION

Roadway
Design
Division

COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn

SEC. 33-T7N-R6W

SEC. 4-T6N-R6W

SEC. 34-T7N-R6W

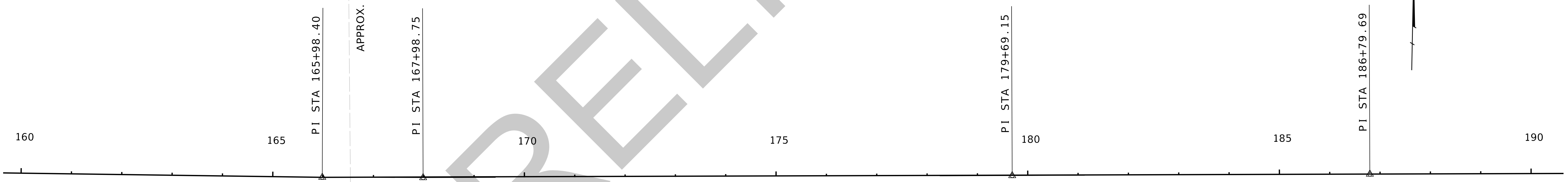
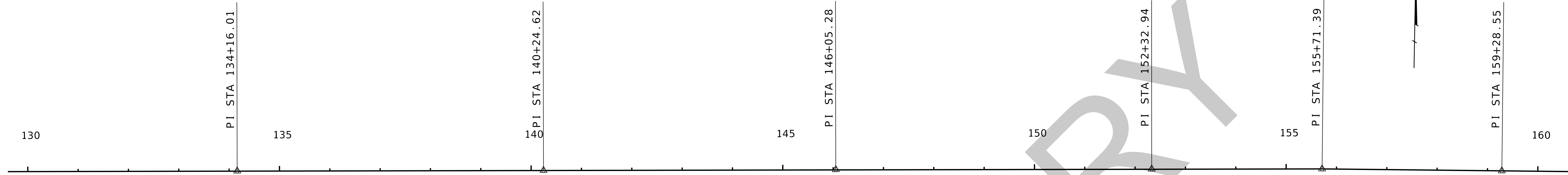
SEC. 33-T7N-R6W

SEC. 4-T6N-R6W

SEC. 3-T6N-R6W

HWY. N-41 (CHAIN HWY41)

HWY. N-41 (CHAIN HWY41)



APPROX. SEC. LINE

APPROX. SEC. LINE

F4

Project Number
41-5(108)

C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

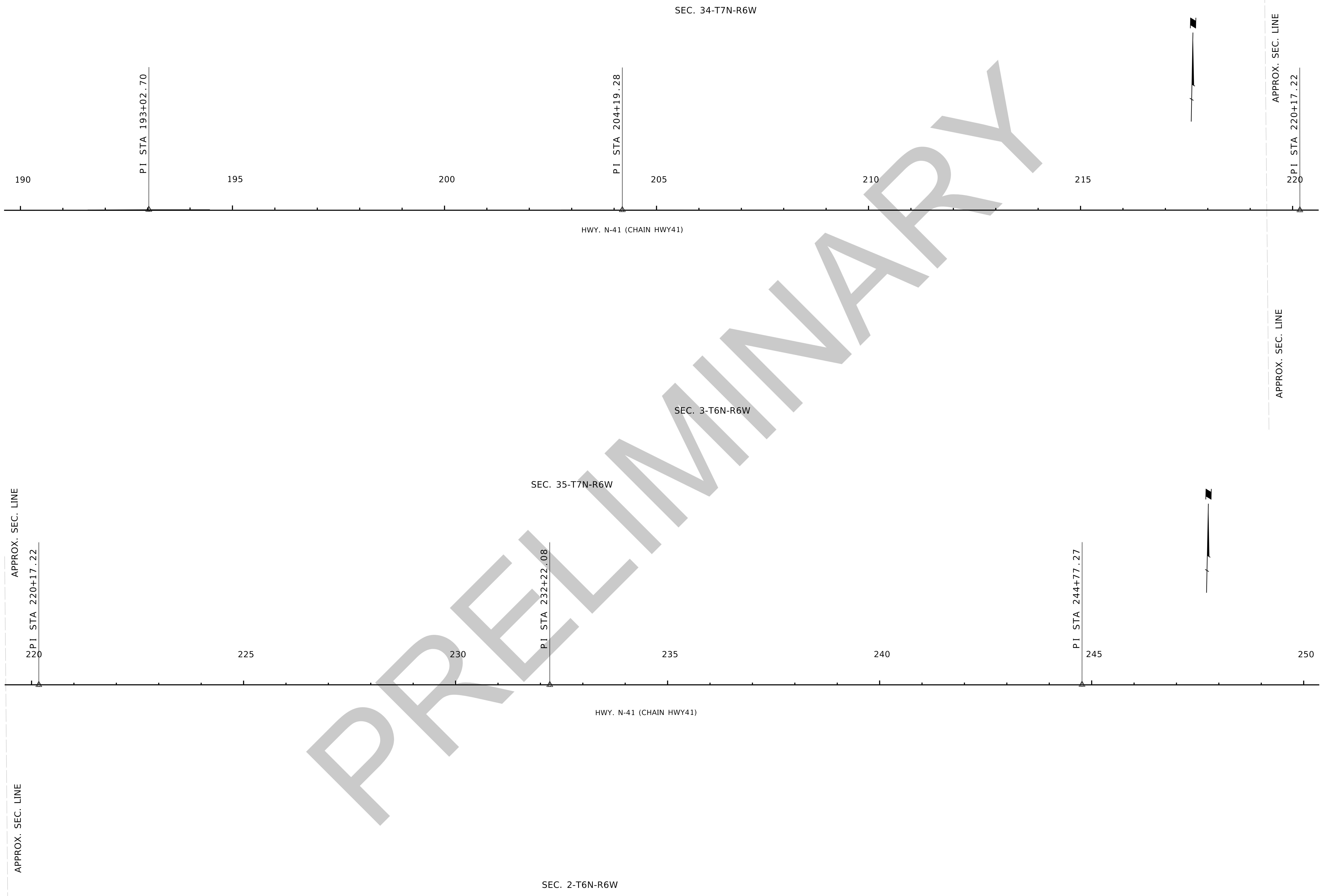
NEBRASKA
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Roadway
Design
Division

COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn



F5
 Project Number
 41-5(108)
 C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

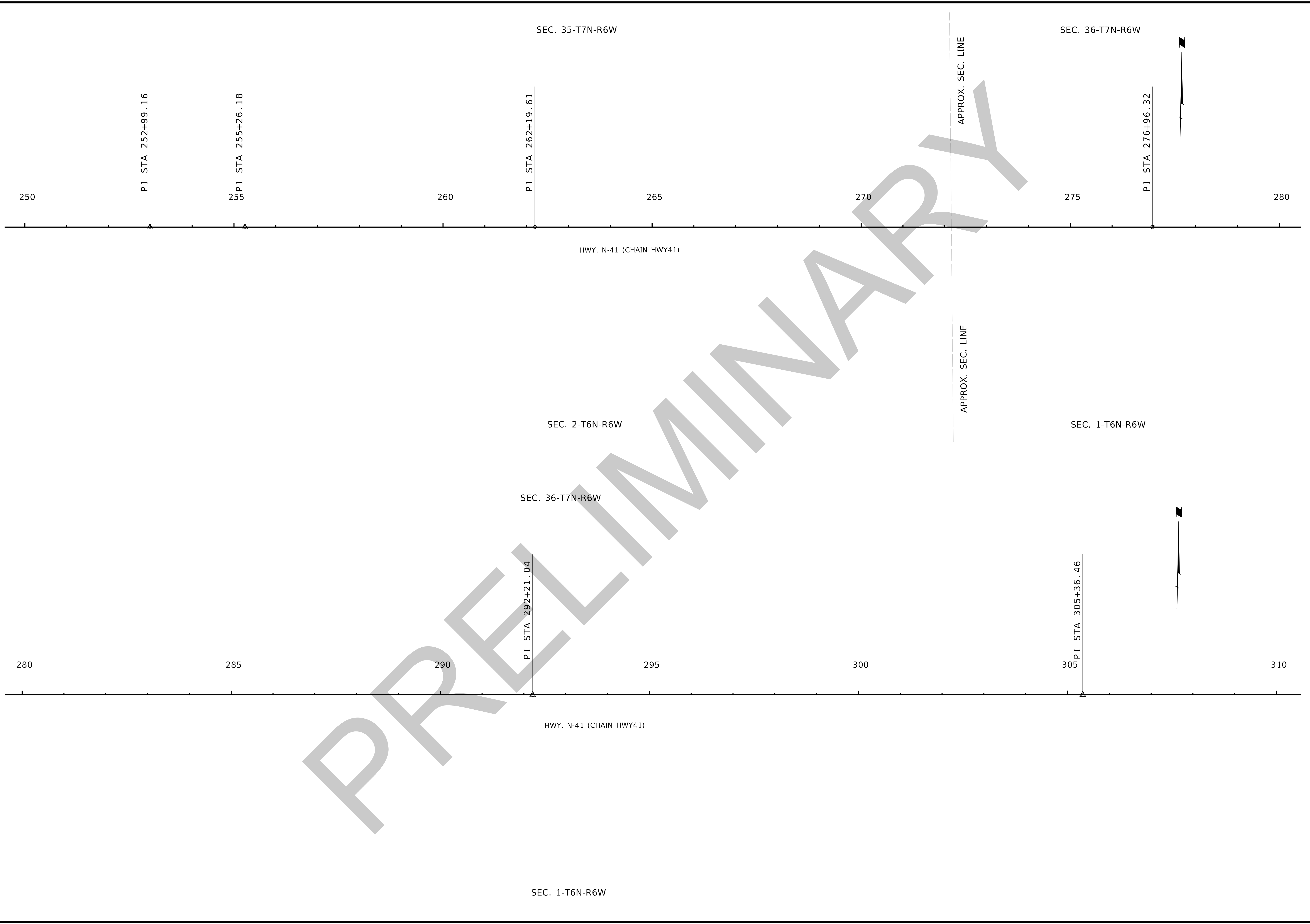


Roadway
 Design
 Division

COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn



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Project Number 41-5(108)
C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

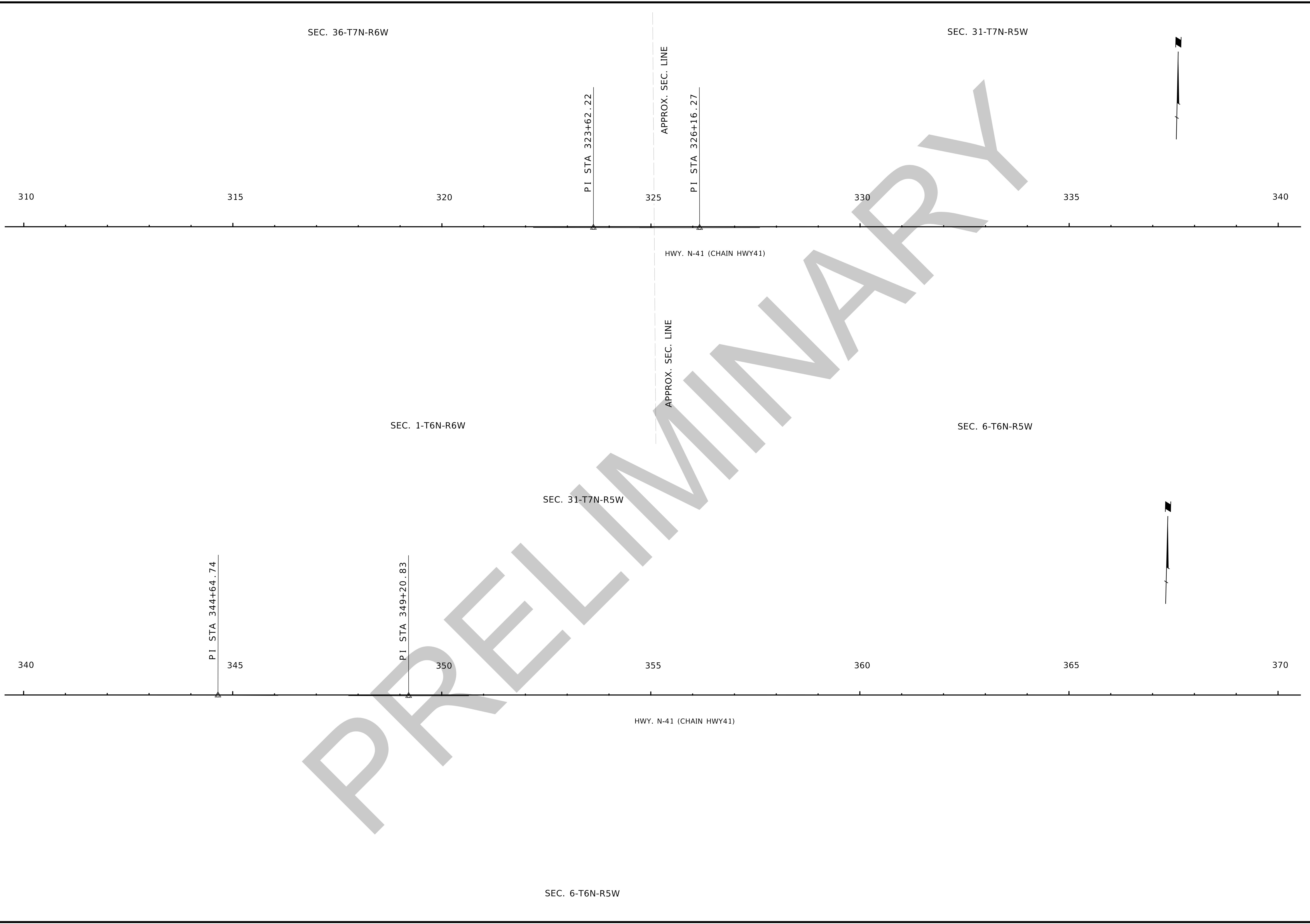


Roadway
Design
Division

COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn



F7
 Project Number
 41-5(108)
 C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION



Roadway
 Design
 Division

COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn

SEC. 31-T7N-R5W

SEC. 32-T7N-R5W

SEC. 6-T6N-R5W

SEC. 5-T6N-R5W

SEC. 32-T7N-R5W

SEC. 5-T6N-R5W

HWY. N-41 (CHAIN HWY41)

HWY. N-41 (CHAIN HWY41)

370 375 380 385 390 395 400

400 405 410 415 420 425 430

APPROX. SEC. LINE
PI STA 377+60.47

PI STA 404+79.05

PI STA 410+25.49

PI STA 388+15.45

PI STA 421+51.04

APPROX. SEC. LINE

APPROX. SEC. LINE

F8

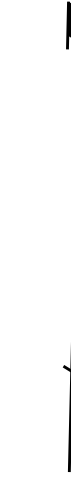
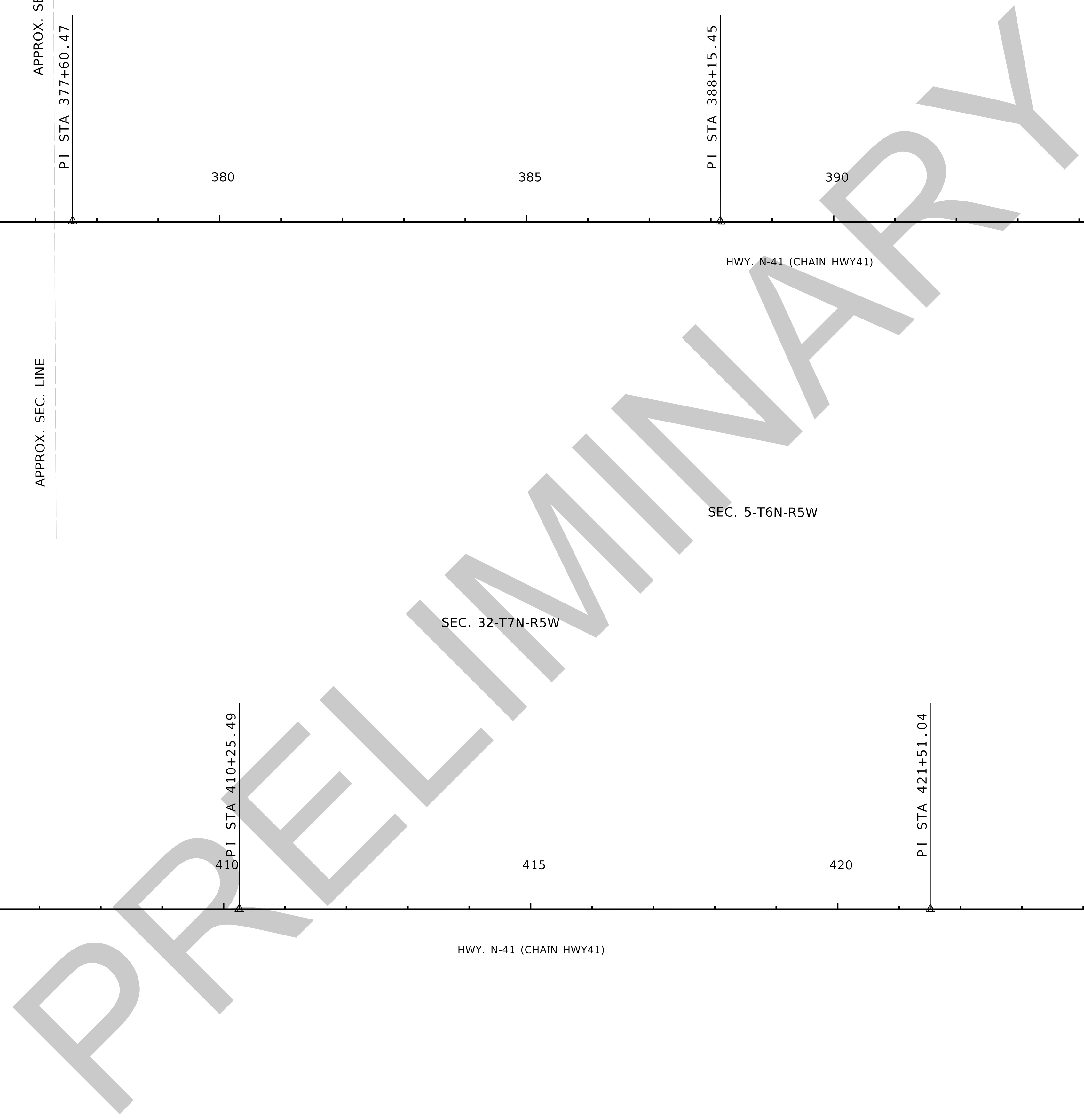
Project Number
41-5(108)

C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

NEBRASKA
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DEPARTMENT OF TRANSPORTATION

Roadway
Design
Division



SEC. 33-T7N-R5W

SEC. 4-T6N-R5W

SEC. 33-T7N-R5W

SEC. 34-T7N-R5W

SEC. 4-T6N-R5W

SEC. 3-T6N-R5W

HWY. N-41 (CHAIN HWY41)

HWY. N-41 (CHAIN HWY41)

APPROX. SEC. LINE

APPROX. SEC. LINE

APPROX. SEC. LINE

APPROX. SEC. LINE

PI STA 430+51.41

PI STA 444+83.53

PI STA 447+43.88

PI STA 456+63.83

PI STA 467+73.10

PI STA 480+19.82

PI STA 487+06.39

430

435

440

445

450

455

460

460

465

470

475

480

485

490

F9

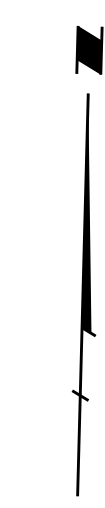
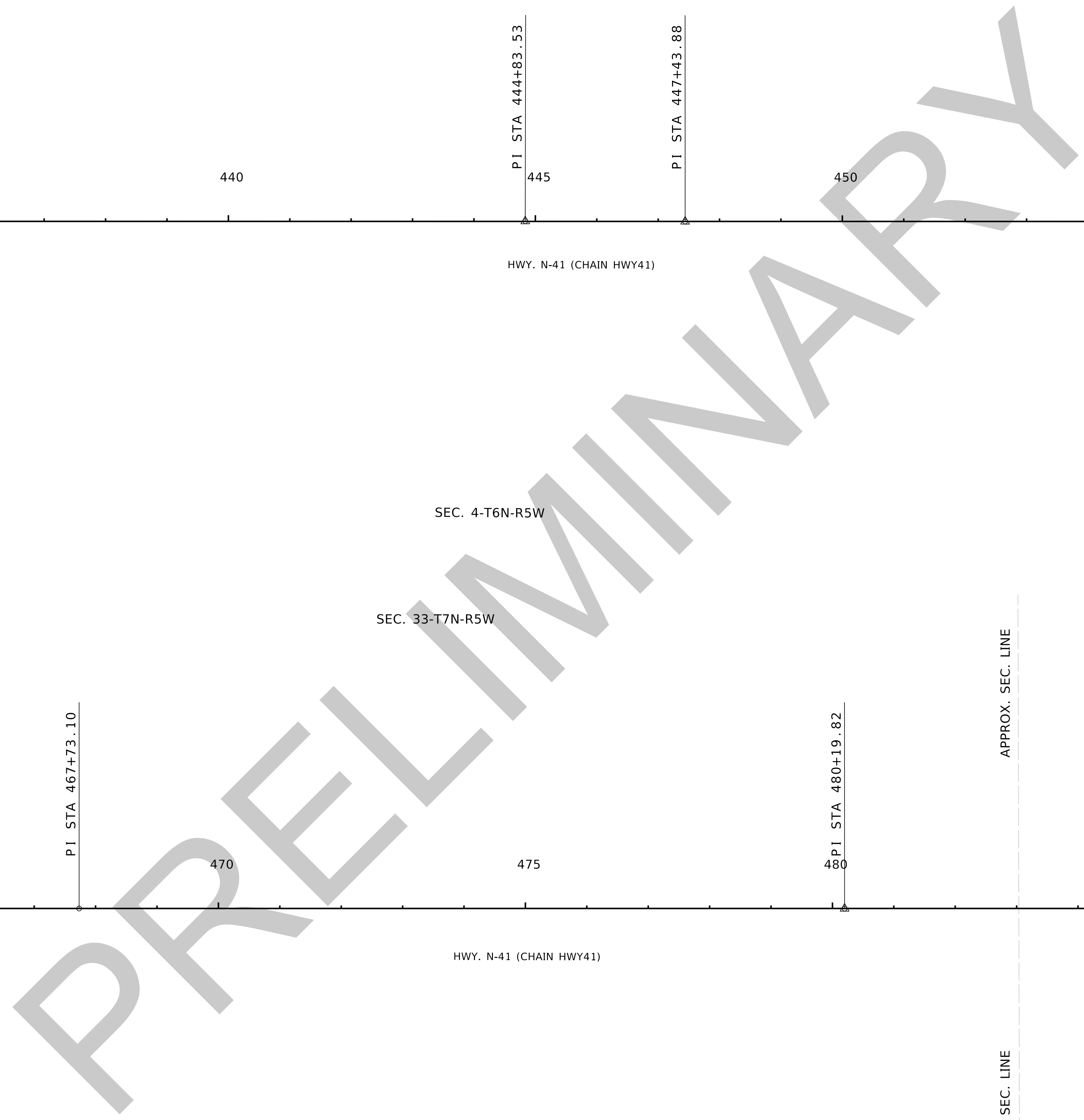
Project Number
41-5(108)

C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

NEBRASKA
Good Life. Great Journey.
DEPARTMENT OF TRANSPORTATION

Roadway
Design
Division



COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn

SEC. 34-T7N-R5W

SEC. 3-T6N-R5W

SEC. 34-T7N-R5W

SEC. 35-T7N-R5W

SEC. 3-T6N-R5W

SEC. 2-T6N-R5W

HWY. N-41 (CHAIN HWY41)

HWY. N-41 (CHAIN HWY41)

APPROX. SEC. LINE

APPROX. SEC. LINE

PI STA 496+69.36

PI STA 500+41.71

PI STA 510+02.45

PI STA 519+41.08

PI STA 534+60.05

PI STA 544+87.58

PI STA 548+55.46

490

495

500

505

510

515

520

520

525

530

535

540

545

550

F10

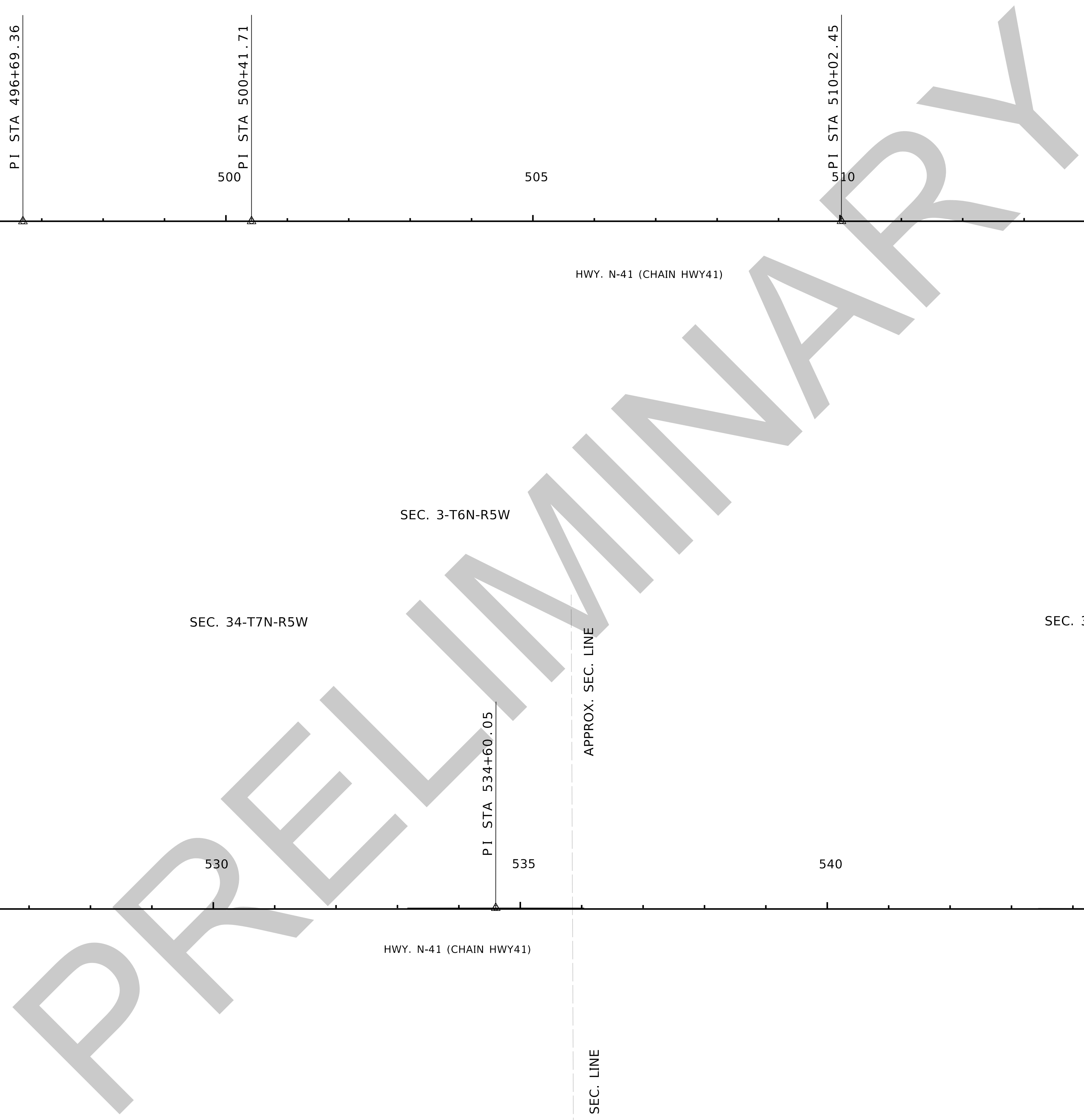
Project Number
41-5(108)

C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

NEBRASKA
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Roadway
Design
Division



COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn

SEC. 35-T7N-R5W

SEC. 2-T6N-R5W

SEC. 36-T7N-R5W

SEC. 35-T7N-R5W

SEC. 2-T6N-R5W

SEC. 1-T6N-R5W

HWY. N-41 (CHAIN HWY41)

HWY. N-41 (CHAIN HWY41)

550 555 560 565 570 575 580

580 585 590 595 600 605 610

PI STA 561+65.45

PI STA 569+35.36

PI STA 574+45.03

APPROX. SEC. LINE

PI STA 589+19.11

APPROX. SEC. LINE

PI STA 603+50.33

F11

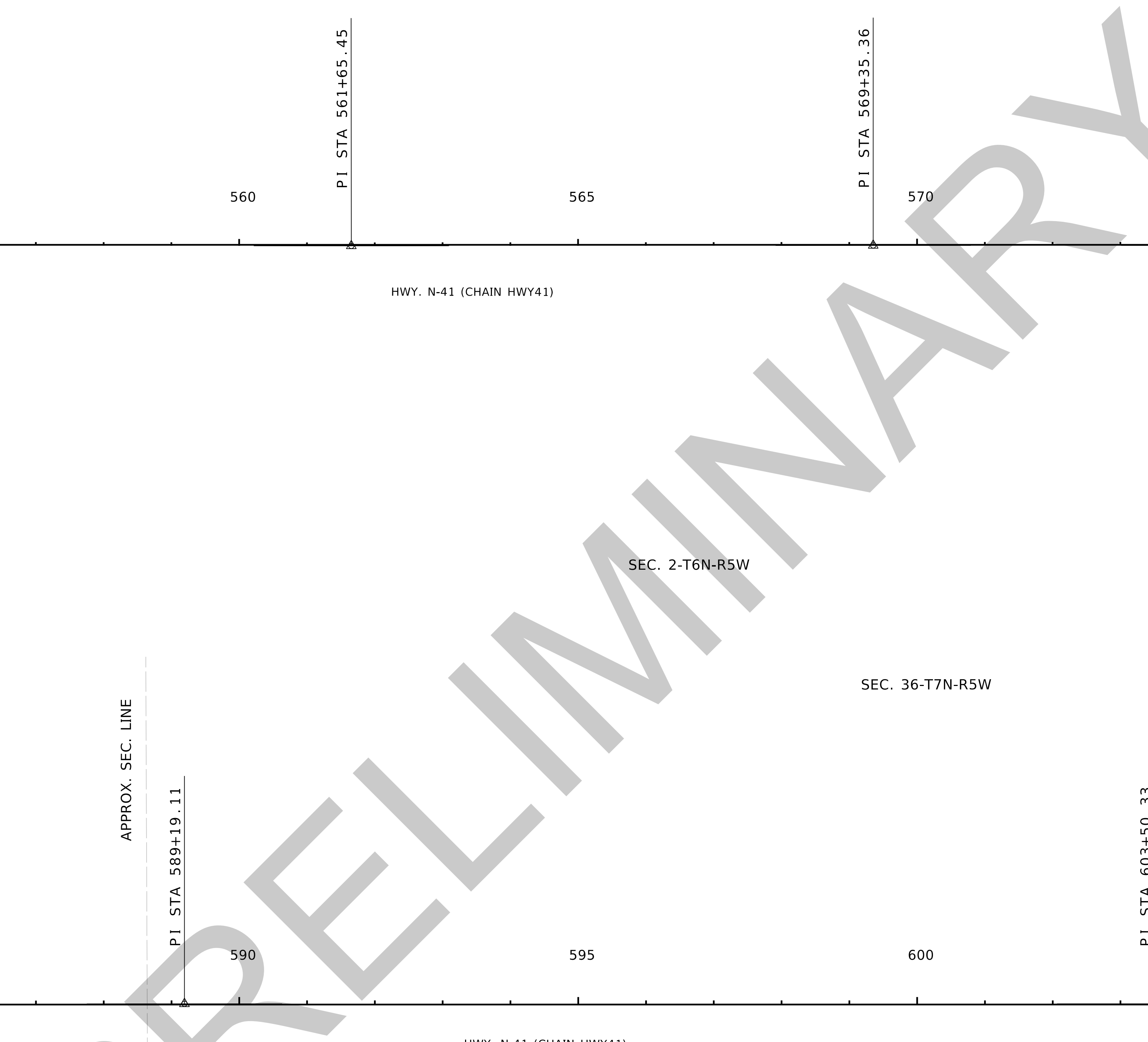
Project Number
41-5(108)

C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

NEBRASKA
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DEPARTMENT OF TRANSPORTATION

Roadway
Design
Division



COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:08

FILE: 42829 Sheets Horizontal Alignment.dgn

SEC. 36-T7N-R5W

610 615 620 625 630 635 640

HWY. N-41 (CHAIN HWY41)

PI STA 618+08.04

PI STA 628+68.48

PI STA 631+24.79

SEC. 1-T6N-R5W

SEC. 31-T7N-R4W

640 645

HWY. N-41 (CHAIN HWY41)

PI STA 642+99.55

PI STA 640+62.18

APPROX. SEC. LINE

APPROX. SEC. LINE

SEC. 6-T6N-R4W

F12

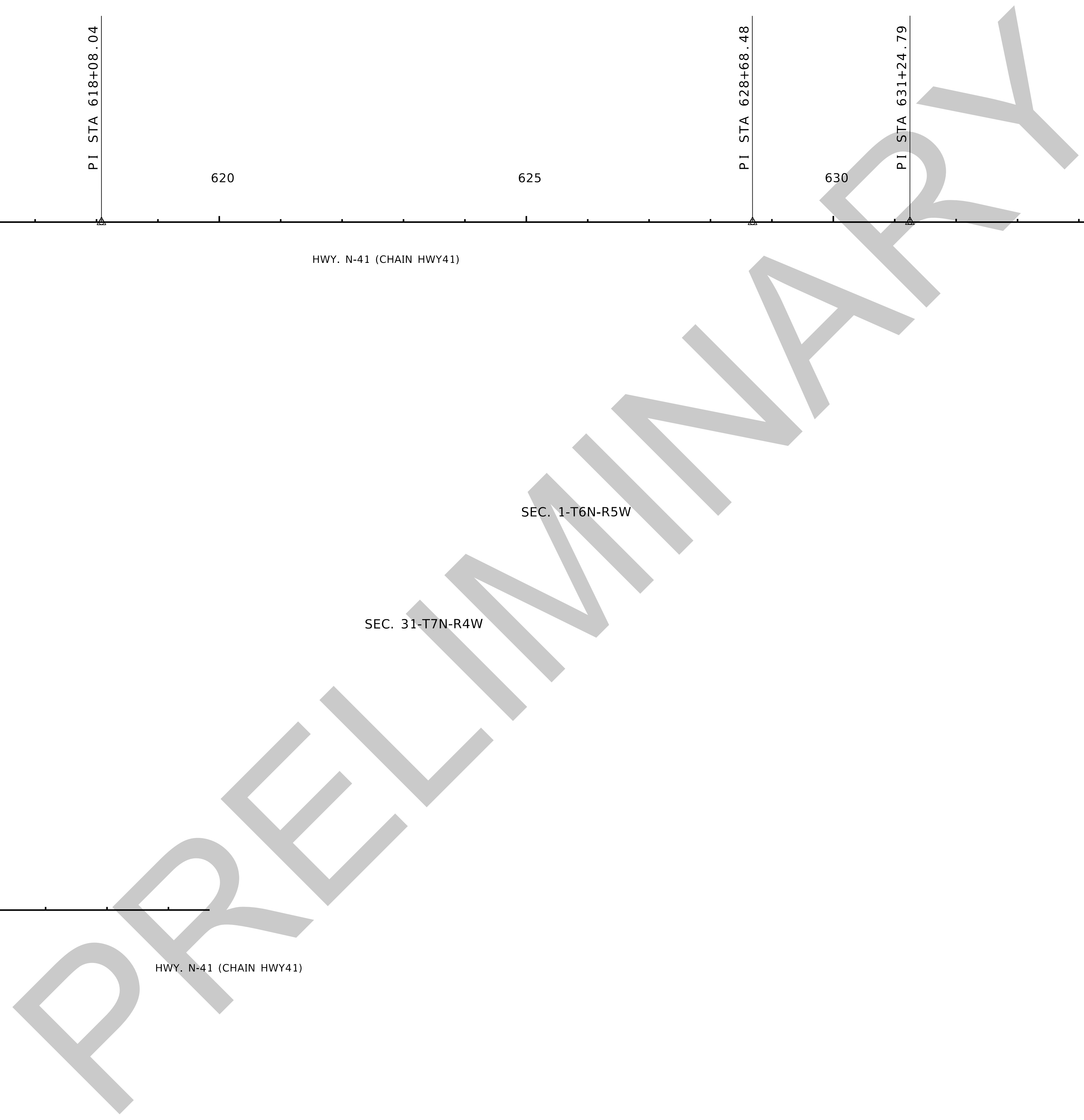
Project Number
41-5(108)

C.N. 42829

HORIZONTAL ALIGNMENT & ORIENTATION

NEBRASKA
Good Life. Great Journey.
DEPARTMENT OF TRANSPORTATION

Roadway
Design
Division



HWY. N-41 CONT'D

HPI	() 456+63.83	2227210.27	259028	Tangential Length:	1431.23		
Tangential Direction:	88.46°			Element: Linear			
Tangential Length:	919.95			HPI	() 603+50.33	2241891.94	259404.78
Element: Linear				HPI	() 618+08.04	2243349.23	259439.47
HPI	() 456+63.83	2227210.27	259028	Tangential Direction:	88.64°		
HPI	() 467+73.10	2228319.16	259057.04	Tangential Length:	1457.71		
Tangential Direction:	88.50°			Element: Linear			
Tangential Length:	1109.27			HPI	() 618+08.04	2243349.23	259439.47
Element: Linear				HPI	() 628+68.48	2244409.37	259464.92
HPI	() 467+73.10	2228319.16	259057.04	Tangential Direction:	88.62°		
HPI	() 480+19.82	2229565.46	259089.72	Tangential Length:	1060.44		
Tangential Direction:	88.50°			Element: Linear			
Tangential Length:	1246.73			HPI	() 628+68.48	2244409.37	259464.92
Element: Linear				HPI	() 631+24.79	2244665.59	259471.36
HPI	() 480+19.82	2229565.46	259089.72	Tangential Direction:	88.56°		
HPI	() 487+06.39	2230251.78	259108.18	Tangential Length:	256.31		
Tangential Direction:	88.46°			Element: Linear			
Tangential Length:	686.57			HPI	() 631+24.79	2244665.59	259471.36
Element: Linear				HPI	() 640+62.18	2245602.72	259493.51
HPI	() 487+06.39	2230251.78	259108.18	Tangential Direction:	88.65°		
HPI	() 496+69.36	2231214.42	259133.24	Tangential Length:	937.39		
Tangential Direction:	88.51°			Element: Linear			
Tangential Length:	962.97			HPI	() 640+62.18	2245602.72	259493.51
Element: Linear				HPI	() 642+99.55	2245840.02	259499.48
HPI	() 496+69.36	2231214.42	259133.24	Tangential Direction:	88.56°		
HPI	() 500+41.71	2231586.64	259143.22	Tangential Length:	237.37		
Tangential Direction:	88.46°			Element: Linear			
Tangential Length:	372.35			HPI	() 642+99.55	2245840.02	259499.48
Element: Linear				END	() 649+67.08	2246507.37	259514.91
HPI	() 500+41.71	2231586.64	259143.22	Tangential Direction:	88.68°		
HPI	() 510+02.45	2232547.03	259168.78	Tangential Length:	667.53		
Tangential Direction:	88.48°						
Tangential Length:	960.74						
Element: Linear							
HPI	() 510+02.45	2232547.03	259168.78				
HPI	() 519+41.08	2233485.35	259193.16				
Tangential Direction:	88.51°						
Tangential Length:	938.63						
Element: Linear							
HPI	() 519+41.08	2233485.35	259193.16				
HPI	() 534+60.05	2235003.81	259232.78				
Tangential Direction:	88.51°						
Tangential Length:	1518.97						
Element: Linear							
HPI	() 534+60.05	2235003.81	259232.78				
HPI	() 544+87.58	2236031.04	259257.44				
Tangential Direction:	88.62°						
Tangential Length:	1027.53						
Element: Linear							
HPI	() 544+87.58	2236031.04	259257.44				
HPI	() 548+55.46	2236398.82	259266.21				
Tangential Direction:	88.63°						
Tangential Length:	367.89						
Element: Linear							
HPI	() 548+55.46	2236398.82	259266.21				
HPI	() 561+65.45	2237708.44	259297.25				
Tangential Direction:	88.64°						
Tangential Length:	1309.98						
Element: Linear							
HPI	() 561+65.45	2237708.44	259297.25				
HPI	() 569+35.36	2238478.09	259317.23				
Tangential Direction:	88.51°						
Tangential Length:	769.91						
Element: Linear							
HPI	() 569+35.36	2238478.09	259317.23				
HPI	() 574+45.03	2238987.59	259330.09				
Tangential Direction:	88.55°						
Tangential Length:	509.67						
Element: Linear							
HPI	() 574+45.03	2238987.59	259330.09				
HPI	() 589+19.11	2240461.16	259368.85				
Tangential Direction:	88.49°						
Tangential Length:	1474.08						
Element: Linear							
HPI	() 589+19.11	2240461.16	259368.85				
HPI	() 603+50.33	2241891.94	259404.78				
Tangential Direction:	88.56°						

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Project Number

41-5(108)

C.N. 42829

DATUM INFORMATION

HORIZONTAL VERTICAL
NAD 83 (1995) NAVD 88

D.A.F. = 1.0002771

CONTROL POINT DATA

Control Point	X	Y	Z	Station	Offset	Object Used for Station
FMZA	2315987.6700	282494.2470	0.0000	Off Chain	Off Chain	STAINLESS STEEL ROD IN SLEEVE
GPS50	2183451.3450	258072.9670	1786.8220	18+94.25	18.8631	CONC MONU w/ALUM DISK
GPS60	2185008.2230	258162.6540	1764.5480	34+52.69	-38.9310	CONC MONU w/ALUM CAP
CP0-900	2187671.4420	258312.4230	1761.7840	61+18.58	-130.1912	CONC MONU w/ALUM NDOT DISK
CP1-901	2192946.2100	258382.4060	1762.0000	113+93.44	-95.4398	CONC MONU w/ALUM NDOT DISK
CP2-902	2198196.2790	258236.5790	1768.5230	166+40.43	140.7597	CONC MONU w/ALUM NDOT DISK
CP3-903	2203443.4470	258546.8210	1756.7320	218+91.90	-58.7659	CONC MONU w/ALUM NDOT DISK
CP4-904	2208799.0810	258449.3400	1753.4090	272+44.53	147.0037	CONC MONU w/ALUM NDOT DISK
CP5-905	2214103.9120	258774.5940	1747.2910	325+55.06	-70.5644	CONC MONU w/ALUM NDOT DISK
CP6-906	2219260.2450	258983.9990	1742.1530	377+15.31	-153.6531	CONC MONU w/ALUM NDOT DISK
CP7-907	2224580.7570	258841.5870	1739.7430	430+30.29	116.2952	CONC MONU w/ALUM NDOT DISK
CP8-908	2229813.4550	259210.0680	1729.9310	482+70.90	-113.6628	CONC MONU w/ALUM NDOT DISK
CP9-909	2235151.1180	258952.5960	1715.6350	536+00.62	283.5664	CONC MONU w/ALUM NDOT DISK
CP10-910	2240384.9390	259274.2720	1713.8870	588+40.44	92.3568	CONC MONU w/ALUM NDOT DISK
CP41-911	2245732.4790	259283.7240	1707.8710	641+86.90	212.6943	CONC MONU w/ALUM NDOT DISK

NOTE: CONTROL POINT TIE INFORMATION AVAILABLE UPON REQUEST.

PRELIMINARY

HORIZONTAL ALIGNMENT & ORIENTATION

NEBRASKA
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Roadway
Design
Division

NOTES

- The locations of all aerial and underground utility facilities may not be indicated in these plans. Underground utilities, whether indicated or not will be located and flagged by the Utilities at the request of the Contractor.

No excavation will be permitted in the area of underground utility facilities until all such facilities have been located and identified to the satisfaction of all parties. The excavation must be accomplished with extreme care in order to avoid any possibility of damage to the utility facility.

- Unless Otherwise Noted on the Plans, Determination of Design Discharge (Q) and Headwater (HW) by Standard Methods is not Applicable

- The Contractor will be required to furnish Borrow on this Project.

CULVERT PIPE LEGEND		
TYPE	DESCRIPTION	
1	RCSP	Reinforced Concrete Sewer Pipe
2	RCP	Reinforced Concrete Pipe
3	GCCMP	Galvanized (zinc) Coated Corrugated Metal Pipe
4	ACCOMP	Aluminum Coated Corrugated Metal Pipe
5	PCCMP	Polymer Coated Corrugated Metal Pipe
6	HDPE-CI	High Density Polyethylene (corrugated interior)
7	HDPE-SI	High Density Polyethylene (smooth interior)
8	PVC	Polyvinyl Chloride Pipe

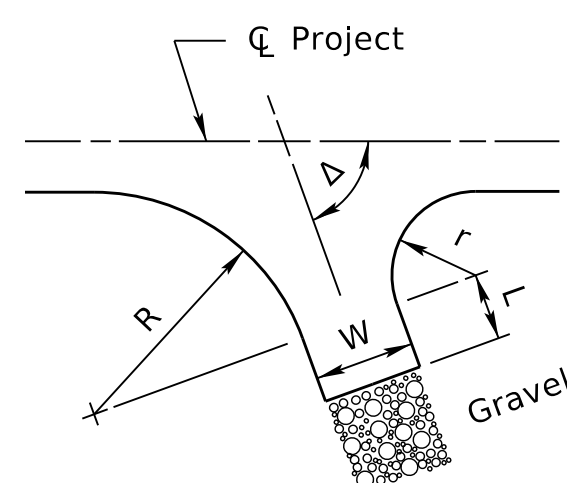
EARTHWORK QUANTITIES			
STATION TO STATION	EXCAVATION AVAILABLE (CU. YDS.)	EARTHWORK MEASURED IN EMBANKMENT (CU. YDS.)	
27+43 - 28+94	10	5	
236+90 - 238+41	-	3	
254+76 - 256+27	-	1	
575+52 - 577+02	-	1	
TOTAL	10	10	

BUILD RUMBLE STRIPS, SPECIAL PLAN C						
STATION TO STATION	CENTERLINE (STA.)		SHOULDER (STA.)		EDGELINE (STA.)	
	STA.	SIDE	STA.	SIDE	STA.	SIDE
28+02 - 642+14	614.12	-	-	-	-	-
TOTAL	614.12	-	-	-	-	-

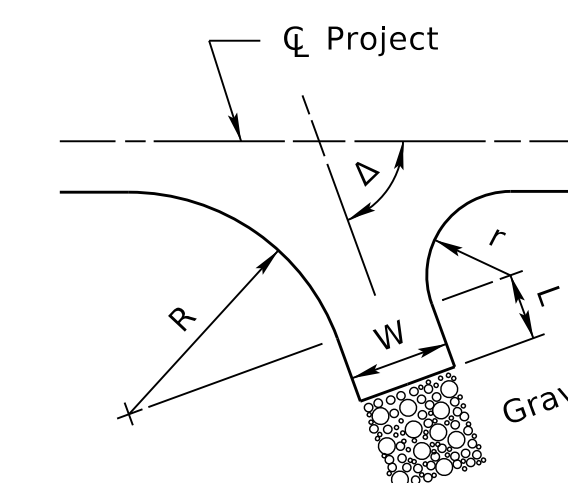
ASPHALTIC CONCRETE, TYPE SLX FOR MAILBOX TURNOUT, PLAN 307		
STATION	SIDE	SQ. YDS.
28+42	RT	135
237+86	RT	135
255+48	RT	94
576+47	RT	135

MAILBOX POSTS		
STATION	SIDE	EACH
28+42	RT	1
237+86	RT	1
255+48	RT	1
576+47	RT	1

2" ASPHALTIC CONCRETE, TYPE SLX ON MILLED SURFACE



6" ASPHALTIC CONCRETE, TYPE SLX ON PREPARED SUBGRADE



SUMMARY OF QUANTITIES AND LOCATION OF SURFACED INTERSECTIONS

STATION	SIDE	RETURN RADIUS		WIDTH W	LENGTH L	INTERSECTION ANGLE Δ	PLACEMENT SQ. YDS.	ASPHALTIC CONCRETE TONS	1" TAPERED TO 2" COLD MILLING CLASS 5 SQ. YDS.	GRAVEL CU. YDS.
		R	r							
		FEET	FEET							
60+88	RT	45	40	16	-	90	258	22	258	10
60+88	LT	45	40	20	-	90	257	21	257	10
113+76	LT	45	45	19	-	90	259	22	259	10
166+59	LT	45	35	21	-	90	240	20	240	10
219+41	RT	40	35	17	-	90	250	21	250	10
272+19	RT	35	30	22	-	90	248	21	248	10
272+17	LT	35	30	22	-	90	254	21	254	10
325+08	LT	35	35	13	-	90	233	20	233	10
325+09	RT	40	35	17	-	90	240	20	240	10
377+32	RT	40	35	15	-	90	250	21	250	10
377+32	LT	40	35	19	-	90	255	21	255	10
430+09	LT	35	35	15	-	90	241	20	241	10
430+09	RT	40	40	23	-	90	284	24	284	10
483+03	RT	40	40	22	-	90	264	22	264	10
483+05	LT	40	35	20	-	90	255	21	255	10
535+82	LT	45	40	26	-	90	350	29	350	-
588+62	RT	45	40	22	-	90	284	24	284	10
588+65	LT	35	35	22	-	90	225	19	225	10
641+49	RT	40	35	24	-	90	250	21	250	10
641+49	LT	40	40	21	-	90	259	22	259	10

SUMMARY OF QUANTITIES AND LOCATION OF SURFACED INTERSECTIONS

STATION	SIDE	RETURN RADIUS	RADIUS	WIDTH W	LENGTH L	INTERSECTION ANGLE Δ	PLACEMENT SQ. YDS.	ASPHALTIC CONCRETE TONS	PREPARATION SQ. YDS.	GRAVEL CU. YDS.
		R	r							
		FEET	FEET							
113+75	RT	35	30	19	-	90	212	18	212	10
166+58	RT	35	30	8	-	90	238	20	238	10
219+42	LT	35	30	20	-	90	251	21	251	10
535+85	RT	40	35	23	-	90	345	29	345	-

LEGEND

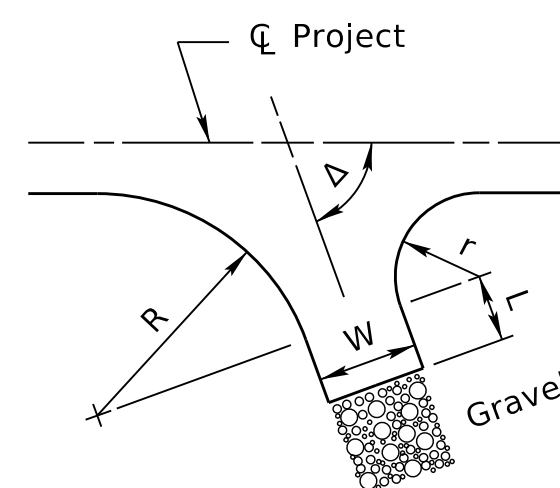
- G --- GAS LINE
- E --- ELECTRICAL SERVICE
- P --- POWER LINE
- OP --- OVERHEAD POWER LINE
- SAN --- SANITARY SEWER
- SS --- STORM SEWER
- T --- TELEPHONE LINE
- TFO --- FIBER OPTIC TELE. LINE
- OT --- OVERHEAD TELEPHONE LINE
- TV --- CABLE TV LINE
- OTV --- OVERHEAD CABLE TV LINE
- W --- WATER LINE
- o --- FENCE - CHAIN LINK
- x --- FENCE - R.O.W. OR WIRE
- □ --- FENCE - WOOD
- --- FLOWLINE
- --- CENTER LINE DRIVE
- ⊕ BENCH MARK
- ⊙ CENTER PIVOT
- ⊙ CONTROL POINT
- ▭ CULVERT
- ▭ DIKE
- ⊙ GAS METER
- ⊗ GAS VALVE
- ⊕ GRID TICK
- ▬ GUARDRAIL
- ⊙ GUARD POST
- GUY POLE
- GUY WIRE
- ☀ OR ☀ LIGHT POLE
- ⊕ MAILBOX
- ⊙ MANHOLE
- ▬ MARSH
- ⊕ OIL WELL
- ⊕ PHOTO CODE POINT
- ⊕ POWER BOX
- ⊕ POWER POLE
- ⊕ POWER PULL BOX
- ⊕ PROPANE TANK
- ⊕ R.O.W. MARKER
- ⊕ ADVANCED R.R. WARNING SIGN
- ⊕ RAILROAD WARNING
- ▬ RAILROAD TRACKS
- ▬ RETAINING WALL
- ⊕ SATELLITE DISH
- ⊕ SIGN
- ☀ TRAFFIC SIGNAL
- ☀ TRAFFIC SIGNAL/ST. LIGHT
- ⊕ TELEPHONE BOX
- ⊕ TELE. FIBER OPTICS BOX
- ⊕ TELEPHONE PULL BOX
- ⊕ TELEPHONE POLE
- ⊕ TELEVISION BOX
- TRAVELED WAY
- ☀ TREE - CONIFEROUS
- ☀ TREE - DECIDUOUS
- ☀ TREE STUMP
- ⊕ WATER (FIRE) HYDRANT
- ⊕ WATER VALVE
- ⊕ WATER METER
- ⊕ WELL
- ⊕ WINDMILL

GENERAL INFORMATION

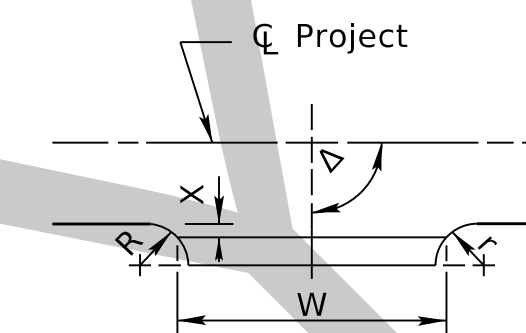


Roadway Design Division

2" ASPHALTIC CONCRETE,
TYPE SLX ON MILLED SURFACE



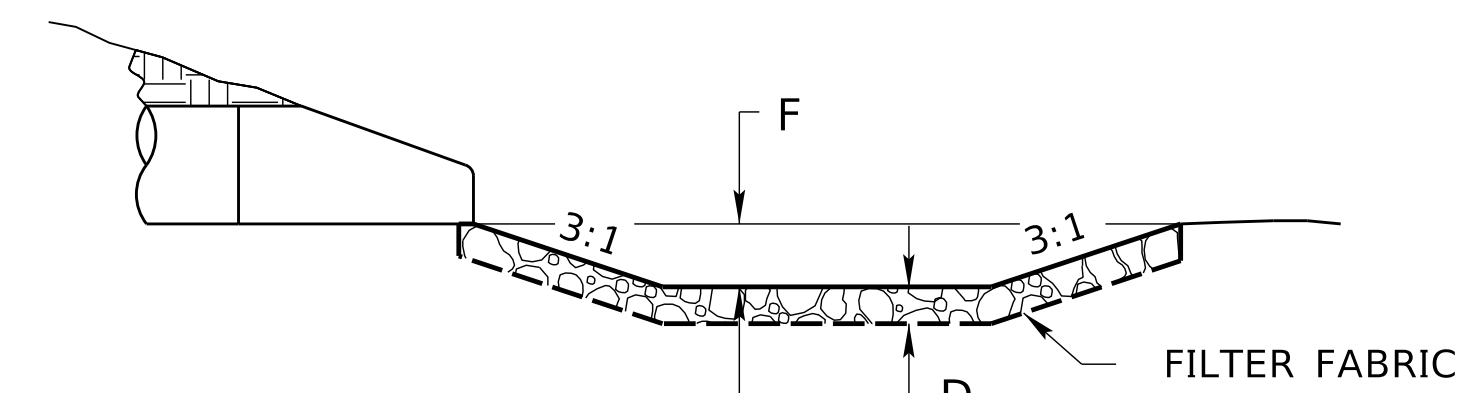
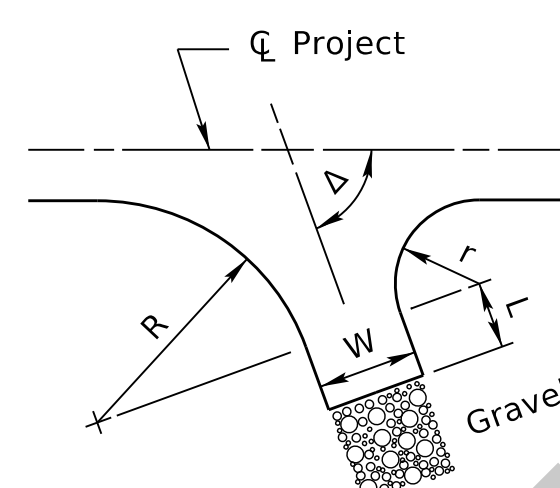
1" TO 0" ASPHALTIC CONCRETE,
TYPE SLX. SEE SHEET B.



SUMMARY OF QUANTITIES AND LOCATION OF SURFACED DRIVEWAYS										
STATION	SIDE	RETURN RADIUS		WIDTH W	LENGTH L	INTERSECTION ANGLE Δ	PLACEMENT SQ. YDS.	ASPHALTIC CONCRETE TONS	1" TAPERED TO 2" COLD MILLING CLASS 5 SQ. YDS.	GRAVEL CU. YDS.
		R	r							
		FEET	FEET	FEET	FEET					
28+42	LT	30	30	14	-	90	114	13	114	5
237+86	LT	30	25	14	-	90	117	13	117	5
255+48	RT	25	20	17	-	90	143	16	143	-
257+16	RT	25	25	10	-	90	124	14	124	5
571+71	RT	25	20	21	-	90	114	13	114	5
575+17	LT	25	25	17	-	90	108	12	108	5
576+47	LT	30	20	23	-	90	121	14	121	5

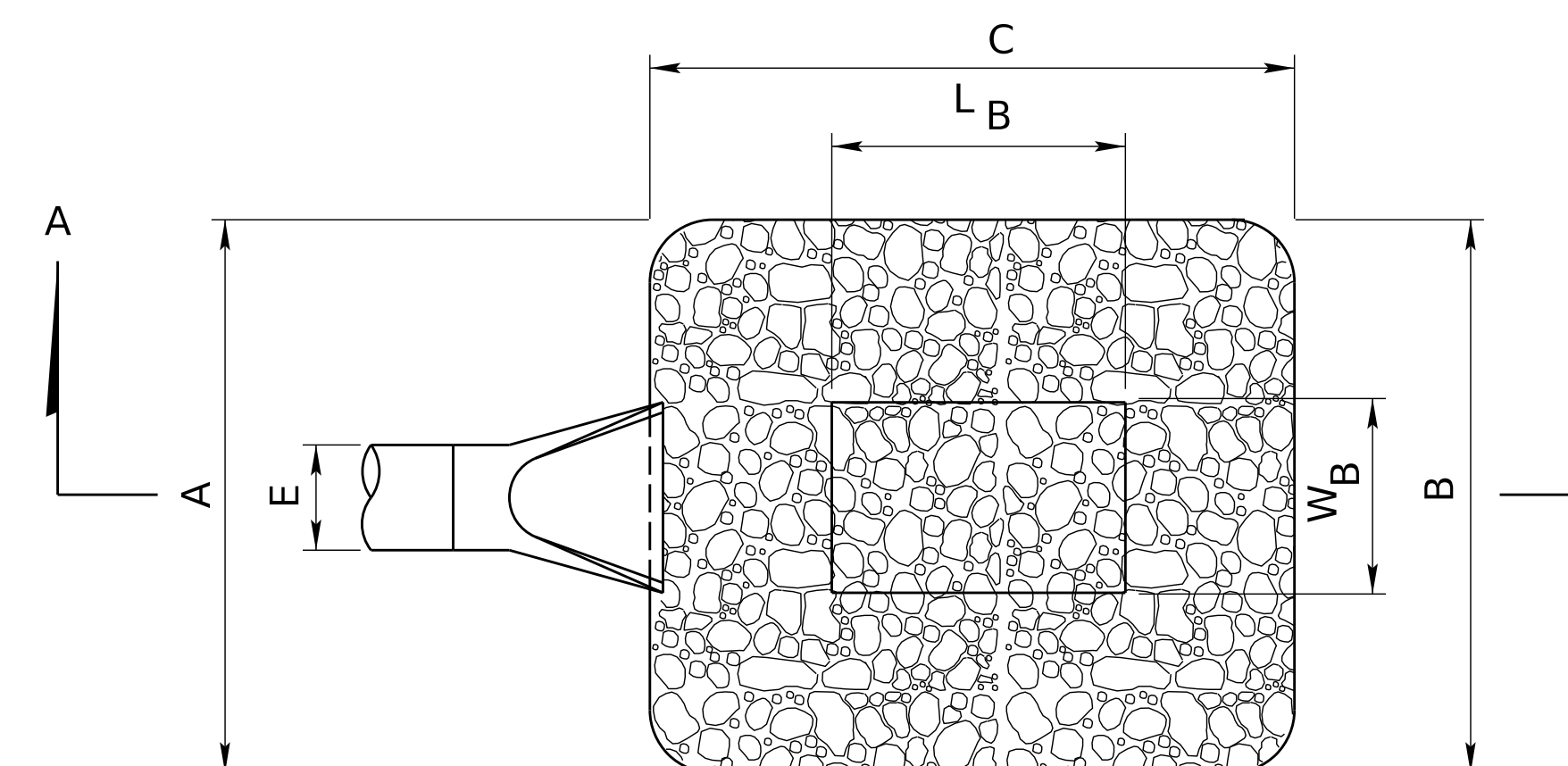
SUMMARY OF QUANTITIES AND LOCATION OF SURFACED DRIVEWAYS										
STATION	SIDE	RETURN RADIUS		WIDTH W	LENGTH X	INTERSECTION ANGLE Δ	PLACEMENT SQ. YDS.	ASPHALTIC CONCRETE		
		R	r					TONS	CU. YDS.	
		FEET	FEET	FEET	FEET					
136+30	RT	25	25	52	5	90	31	4		
139+91	RT	25	20	45	5	90	29	4		
155+01	LT	30	20	83	5	90	48	6		

6" ASPHALTIC CONCRETE,
TYPE SLX ON PREPARED SUBGRADE



SECTION A-A

SUMMARY OF QUANTITIES AND LOCATION OF SURFACED DRIVEWAYS										
STATION	SIDE	RETURN RADIUS		WIDTH W	LENGTH L	INTERSECTION ANGLE Δ	PLACEMENT SQ. YDS.	ASPHALTIC CONCRETE TONS	PREPARATION SQ. YDS.	GRAVEL CU. YDS.
		R	r							
		FEET	FEET	FEET	FEET					
34+35	LT	25	25	26	-	90	99	33	99	5



PLAN

DETAIL OF ROCK RIPRAP SCOUR HOLE

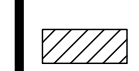
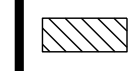

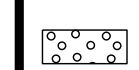
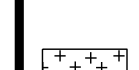

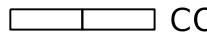
BUILD ROCK RIPRAP SCOUR HOLE											
STATION	A	B	C	D	E	F	W _B	L _B	TONS	RIPRAP TYPE	FILTER FABRIC SQ. YDS.
474+99	23	23	12	2.4	7.0	1.5	14	3	35	B	52

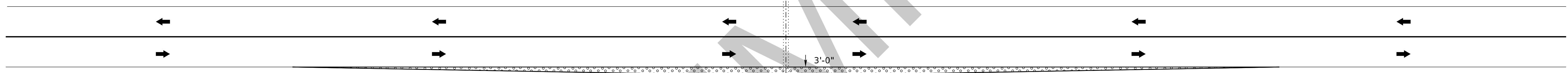
H1

Project Number
41-5(108)

C.N. 42829

LEGEND

-  REMOVE ASPHALT
-  REMOVE TEMPORARY SURFACING
-  BULD 11" SURFACING
-  BUILD TEMPORARY SURFACING
-  COMPLETED SURFACING
-  TRAFFIC FLOW
-  CONCRETE BARRIER



TYPICAL PHASING FOR CULVERT REPLACEMENT

PHASE 1

BUILD TEMPORARY SURFACING			
STATION	TO	STATION	SQ. YDS.
65+39	-	69+32	76
81+10	-	85+10	78
101+55	-	105+47	76
111+03	-	114+95	53
120+38	-	124+31	76
318+62	-	322+54	76
355+59	-	364+33	236
381+44	-	385+37	76
394+57	-	398+49	76
427+05	-	430+97	55
461+42	-	465+34	76
628+11	-	632+14	79

PHASING

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Roadway Design Division

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DATE: 21-MAR-2024 11:09

FILE: 42829 Sheets Phasing.dgn

COMPUTER: BG0419M674

DATE: 21-MAR-2024 11:09








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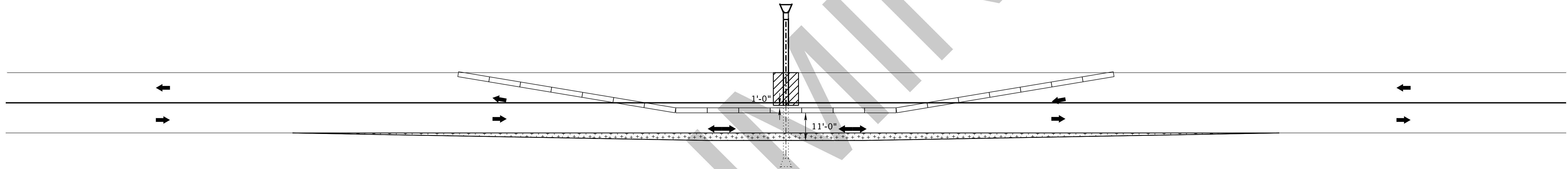
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Project Number
41-5(108)

C.N. 42829

LEGEND

-  REMOVE ASPHALT
-  REMOVE TEMPORARY SURFACING
-  BUILD 11" SURFACING
-  BUILD TEMPORARY SURFACING
-  COMPLETED SURFACING
-  TRAFFIC FLOW
-  CONCRETE BARRIER



TYPICAL PHASING FOR CULVERT REPLACEMENT
PHASE 2

PRELIMINARY

PHASING

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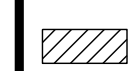
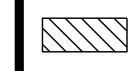

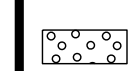
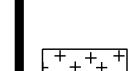

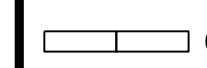
Roadway
Design
Division

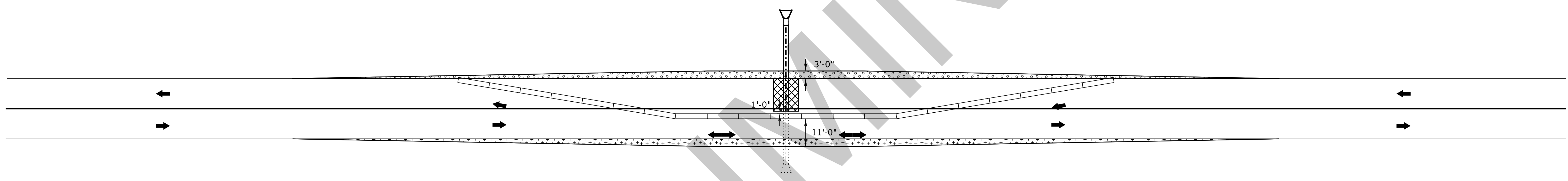
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Project Number
41-5(108)

C.N. 42829

LEGEND

-  REMOVE ASPHALT
-  REMOVE TEMPORARY SURFACING
-  BUILD 11" SURFACING
-  BUILD TEMPORARY SURFACING
-  COMPLETED SURFACING
-  TRAFFIC FLOW
-  CONCRETE BARRIER



TYPICAL PHASING FOR CULVERT REPLACEMENT
PHASE 3

BUILD TEMPORARY SURFACING			
STATION	TO	STATION	SQ. YDS.
65+39	-	69+32	76
81+09	-	85+09	78
101+54	-	105+48	76
111+03	-	114+95	50
120+39	-	124+32	76
318+62	-	322+54	76
355+59	-	364+33	236
381+45	-	385+38	76
394+58	-	398+50	76
427+05	-	430+97	58
461+42	-	465+34	76
628+12	-	632+14	79

PHASING

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Design
Division

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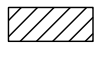
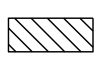


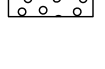
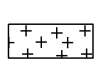

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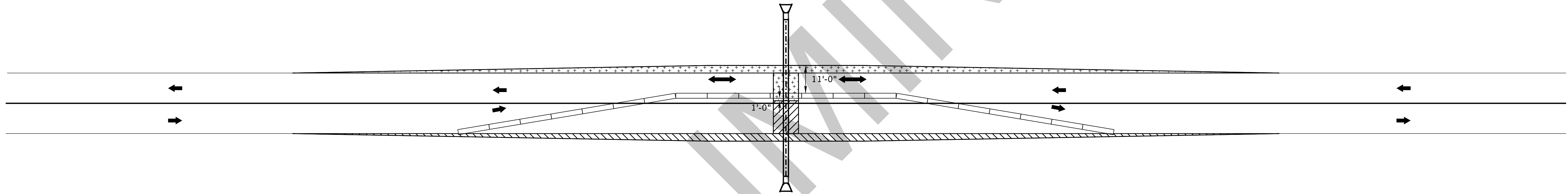
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Project Number
41-5(108)

C.N. 42829

LEGEND

-  REMOVE ASPHALT
-  REMOVE TEMPORARY SURFACING
-  BUILD 11" SURFACING
-  BUILD TEMPORARY SURFACING
-  COMPLETED SURFACING
-  TRAFFIC FLOW
-  CONCRETE BARRIER



TYPICAL PHASING FOR CULVERT REPLACEMENT
PHASE 4

PHASING

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






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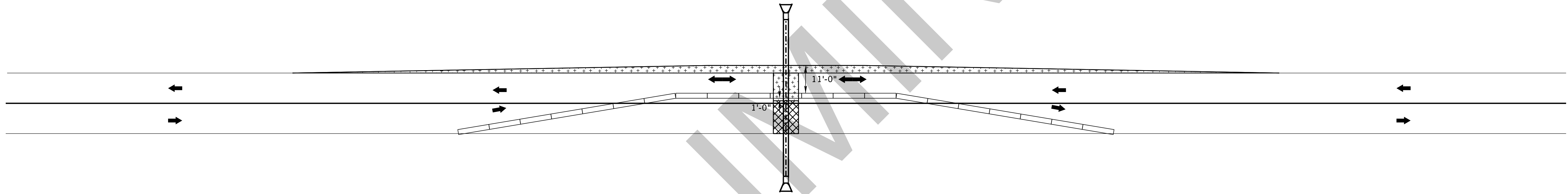
H5

Project Number
41-5(108)

C.N. 42829

LEGEND

-  REMOVE ASPHALT
-  REMOVE TEMPORARY SURFACING
-  BUILD 11" SURFACING
-  BUILD TEMPORARY SURFACING
-  COMPLETED SURFACING
-  TRAFFIC FLOW
-  CONCRETE BARRIER



TYPICAL PHASING FOR CULVERT REPLACEMENT
PHASE 5

PRELIMINARY

PHASING

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






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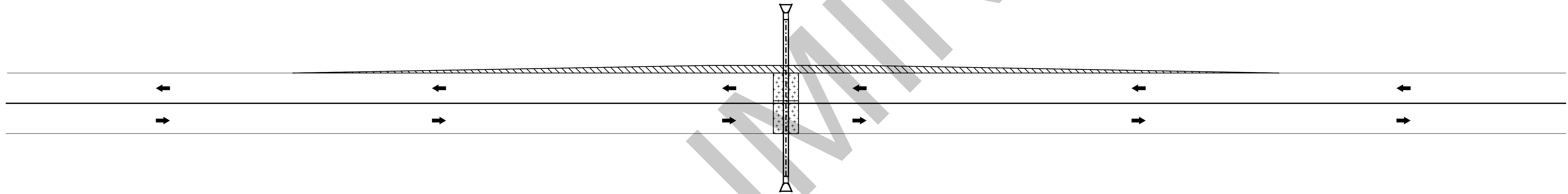
H6

Project Number
41-5(108)

C.N. 42829

LEGEND

-  REMOVE ASPHALT
-  REMOVE TEMPORARY SURFACING
-  BUILD 11" SURFACING
-  BUILD TEMPORARY SURFACING
-  COMPLETED SURFACING
-  TRAFFIC FLOW
-  CONCRETE BARRIER



TYPICAL PHASING FOR CULVERT REPLACEMENT
PHASE 6

PRELIMINARY

PHASING

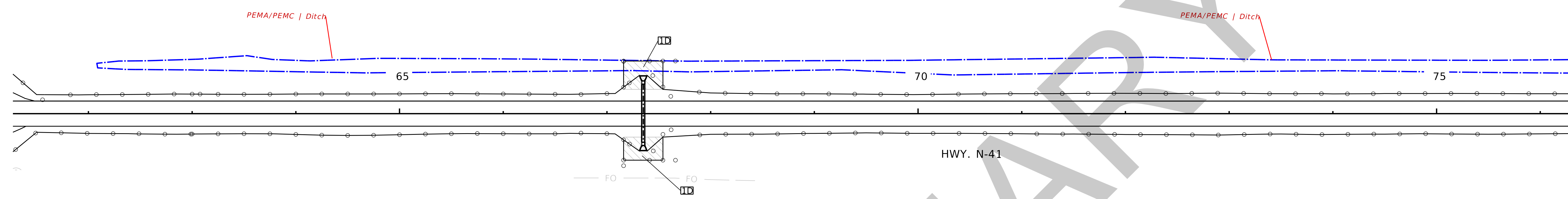
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LEGEND

- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB -
- EROSION CONTROL, CLASS 1D

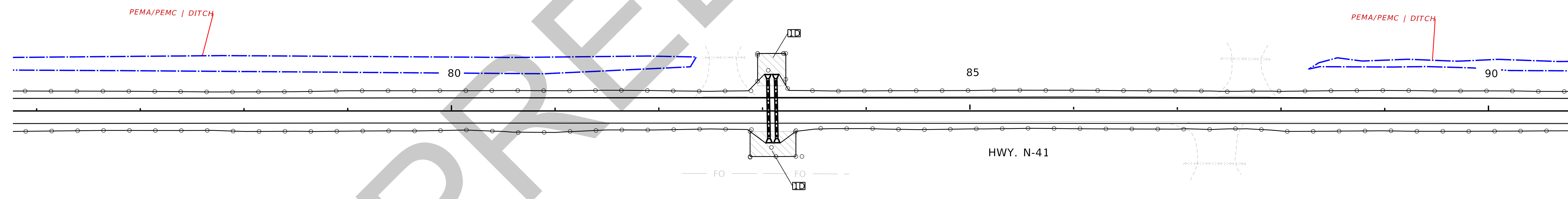
SEC. 32-T7N-R6W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION	TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.
67+16	-	67+54	Lt.	Culvert Outlet	38	115
67+16	-	67+54	Rt.	Culvert Inlet	38	94

SEC. 5-T6N-R6W

SEC. 32-T7N-R6W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION	TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.
82+88	-	83+32	Rt.	Culvert Outlet	44	119
82+95	-	83+23	Lt.	Culvert Inlet	27	95

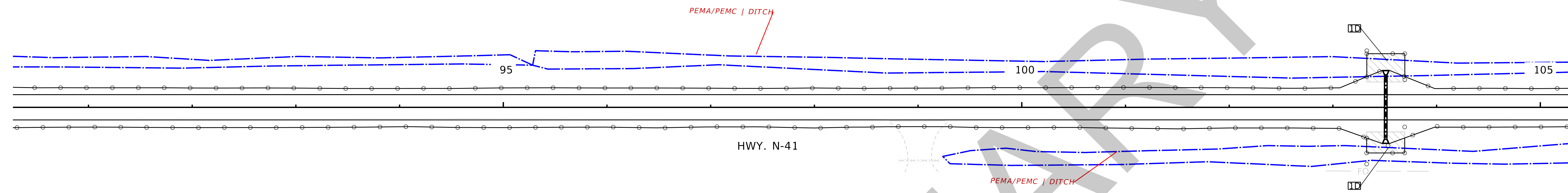
SEC. 5-T6N-R6W

EROSION & SEDIMENT CONTROL

LEGEND

- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB -
- EROSION CONTROL, CLASS 1D

SEC. 32-T7N-R6W

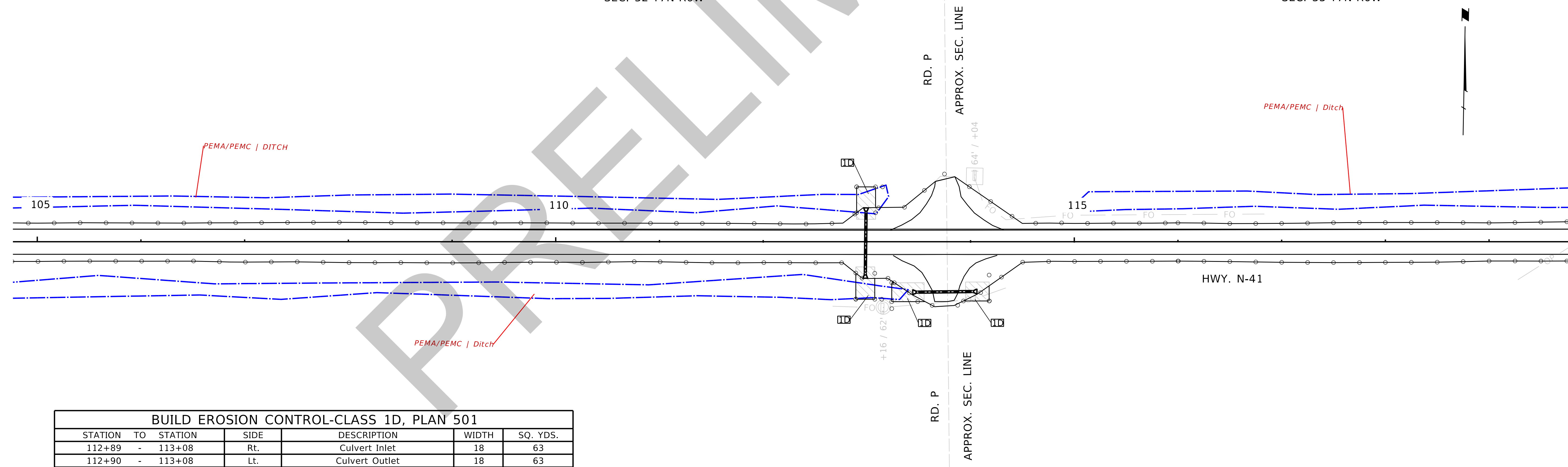


BUILD EROSION CONTROL-CLASS 1D, PLAN 501					
STATION	TO	STATION	SIDE	DESCRIPTION	SQ. YDS.
103+33	-	103+69	Lt.	Culvert Inlet	111
103+33	-	103+69	Rt.	Culvert Outlet	82

SEC. 5-T6N-R6W

SEC. 32-T7N-R6W

SEC. 33-T7N-R6W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION	TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.
112+89	-	113+08	Rt.	Culvert Inlet	18	63
112+90	-	113+08	Lt.	Culvert Outlet	18	63
113+24	-	113+55	Rt.	Culvert Outlet	31	63
113+95	-	114+18	Rt.	Culvert Inlet	23	46

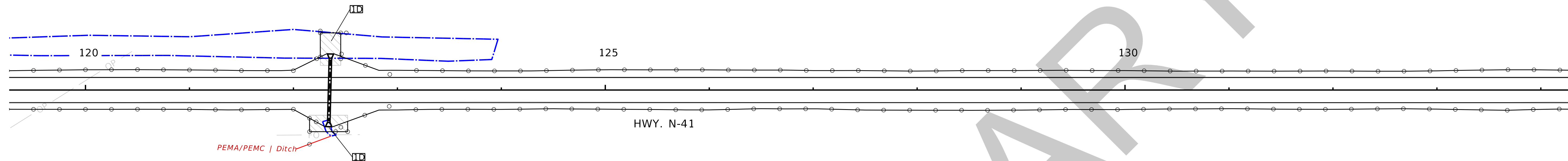
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SEC. 4-T6N-R6W

LEGEND

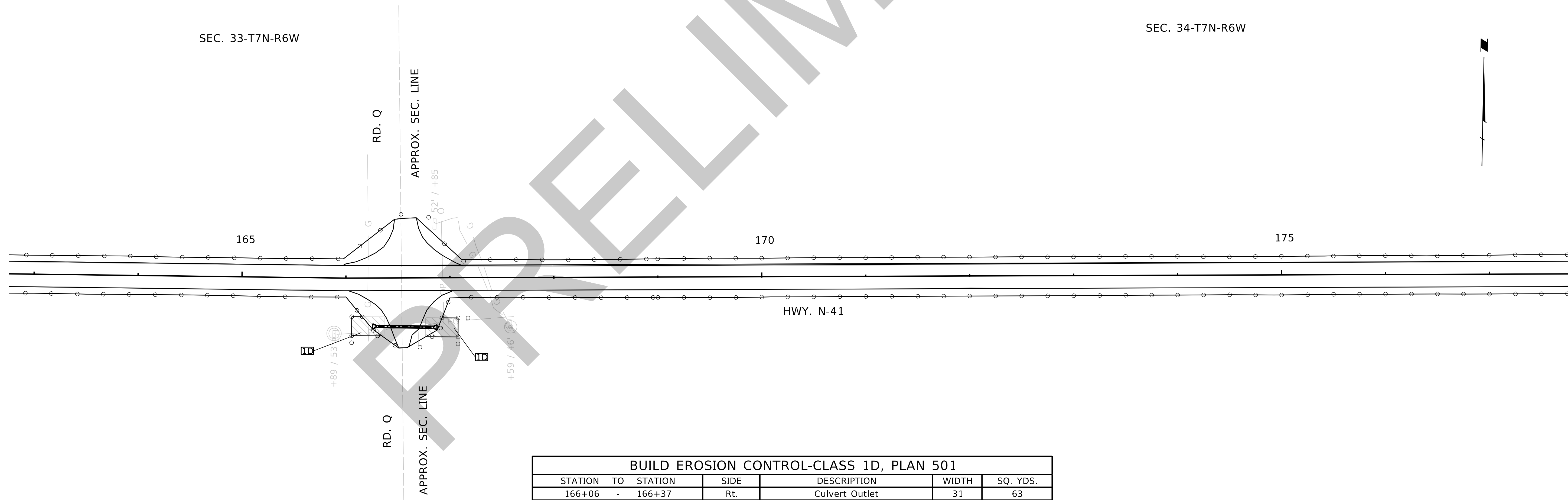
- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB -
- EROSION CONTROL, CLASS 1D

SEC. 33-T7N-R6W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.	
122+16	- 122+52	Rt.	Culvert Outlet	37	64	
122+26	- 122+46	Lt.	Culvert Inlet	20	68	

SEC. 4-T6N-R6W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.	
166+06	- 166+37	Rt.	Culvert Outlet	31	63	
166+77	- 167+08	Rt.	Culvert Inlet	31	63	

SEC. 33-T7N-R6W

SEC. 34-T7N-R6W

SEC. 4-T6N-R6W

SEC. 3-T6N-R6W

LEGEND

- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB -
- EROSION CONTROL, CLASS 1D

SEC. 34-T7N-R6W

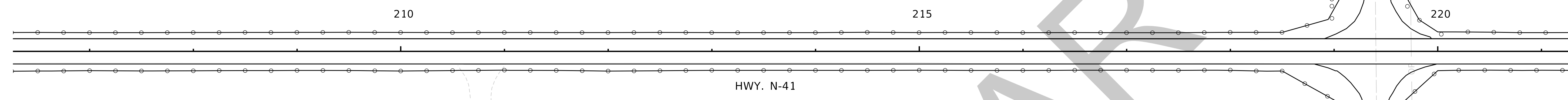
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SEC. 3-T6N-R6W

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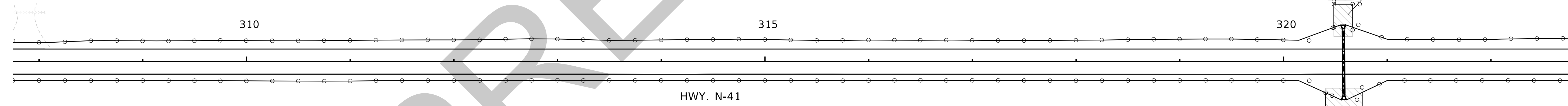
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SEC. 1-T6N-R6W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501

STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.
218+98	- 219+23	Lt.	Culvert Outlet	25	51
219+53	- 219+71	Lt.	Culvert Inlet	18	35



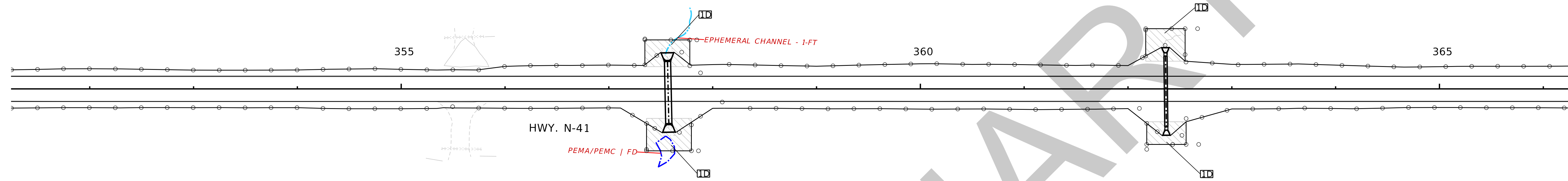
BUILD EROSION CONTROL-CLASS 1D, PLAN 501

STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.
320+41	- 320+76	Rt.	Culvert Inlet	35	95
320+48	- 320+67	Lt.	Culvert Outlet	18	63

LEGEND

- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB -
- EROSION CONTROL, CLASS 1D

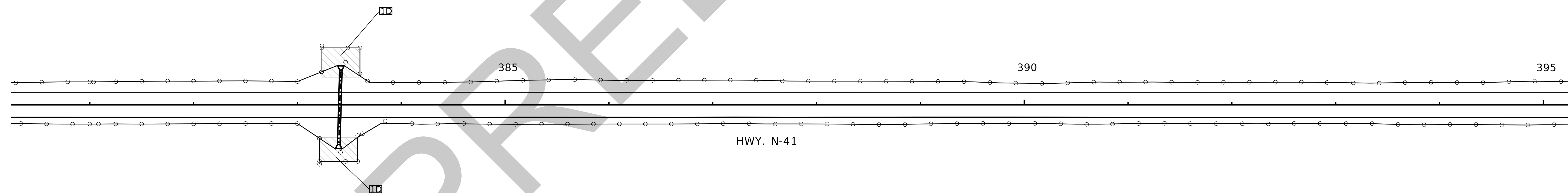
SEC. 31-T7N-R5W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501					
STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.
357+35	- 357+78	Lt.	Culvert Inlet	43	124
357+36	- 357+80	Rt.	Culvert Outlet	43	151
362+17	- 362+55	Lt.	Culvert Inlet	38	132
362+18	- 362+56	Rt.	Culvert Outlet	38	92

SEC. 6-T6N-R5W

SEC. 32-T7N-R5W



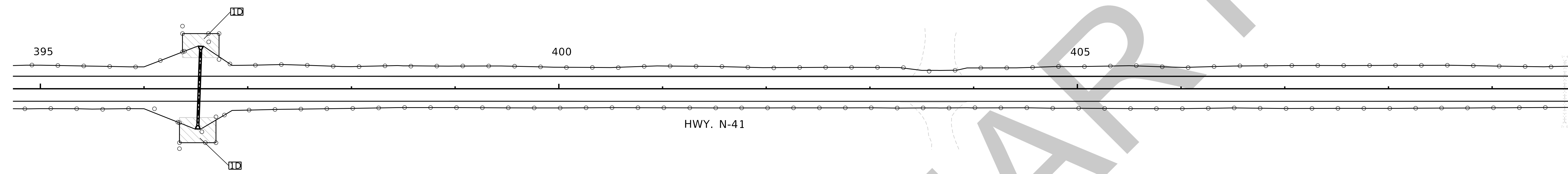
BUILD EROSION CONTROL-CLASS 1D, PLAN 501					
STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.
383+21	- 383+58	Rt.	Culvert Outlet	37	95
383+24	- 383+60	Lt.	Culvert Inlet	37	115

SEC. 5-T6N-R5W

LEGEND

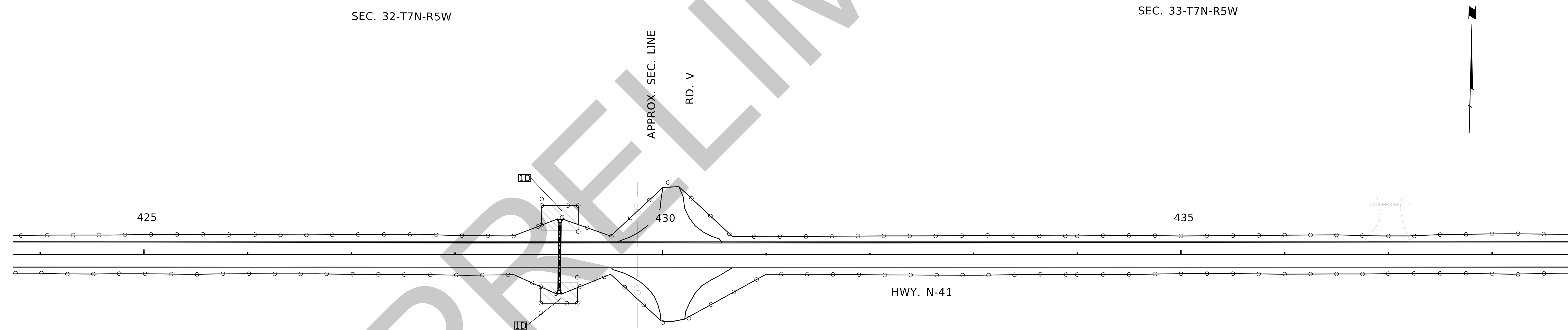
- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB -
- EROSION CONTROL, CLASS 1D

SEC. 32-T7N-R5W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.	
396+34	- 396+69	Rt.	Culvert Inlet	35	95	
396+37	- 396+72	Lt.	Culvert Outlet	35	91	

SEC. 5-T6N-R5W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.	
428+83	- 429+18	Rt.	Culvert Outlet	35	79	
428+84	- 429+19	Lt.	Culvert Inlet	35	95	

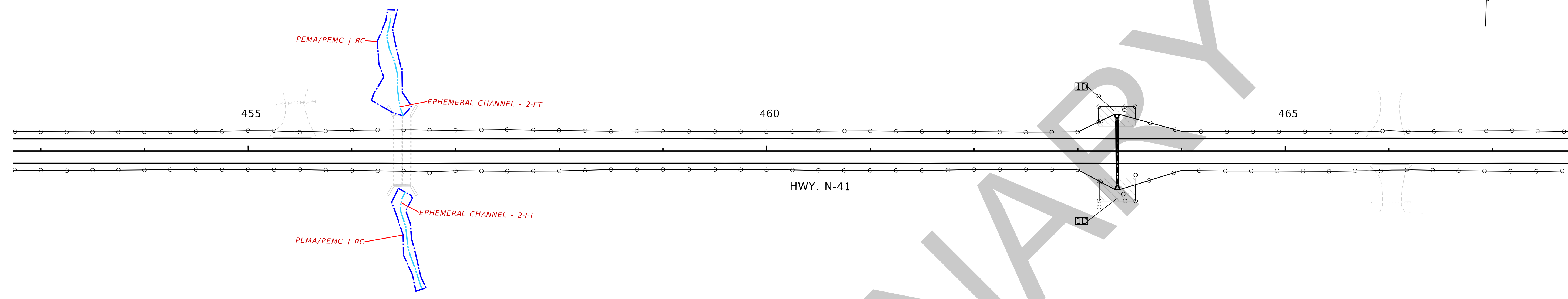
SEC. 5-T6N-R5W

SEC. 4-T6N-R5W

LEGEND

- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB -
- EROSION CONTROL, CLASS 1D

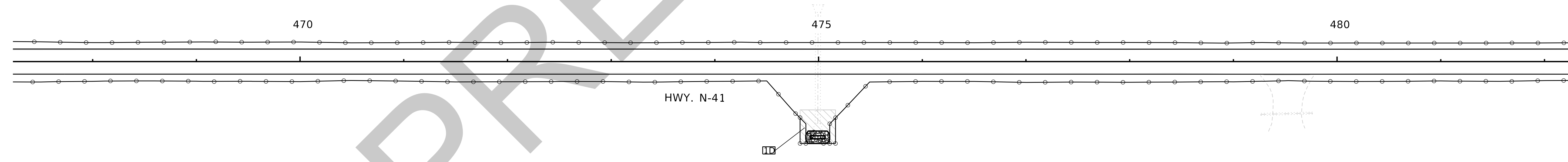
SEC. 33-T7N-R5W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION	TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.
463+20	-	463+55	Lt.	Culvert Inlet	35	73
463+21	-	463+56	Rt.	Culvert Outlet	35	87

SEC. 4-T6N-R5W

SEC. 33-T7N-R5W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION	TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.
474+75	-	475+24	Rt.	Culvert Outlet	34	94

SEC. 4-T6N-R5W

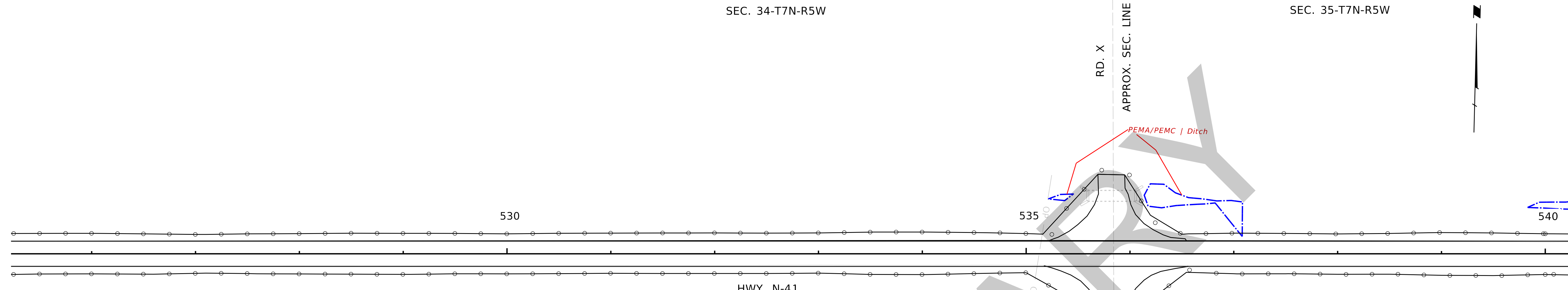
EROSION & SEDIMENT CONTROL

LEGEND

- LIMITS OF CONSTRUCTION
- WETLANDS - DO NOT DISTURB -
- EROSION CONTROL, CLASS 1D

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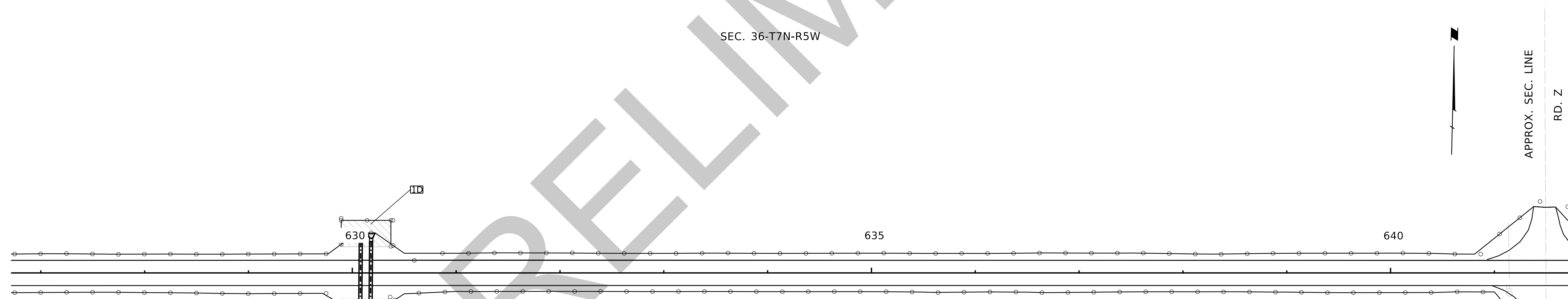
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BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.	
535+41	- 535+67	Rt.	Culvert Outlet	25	87	
536+01	- 536+21	Rt.	Culvert Inlet	20	66	

SEC. 3-T6N-R5W

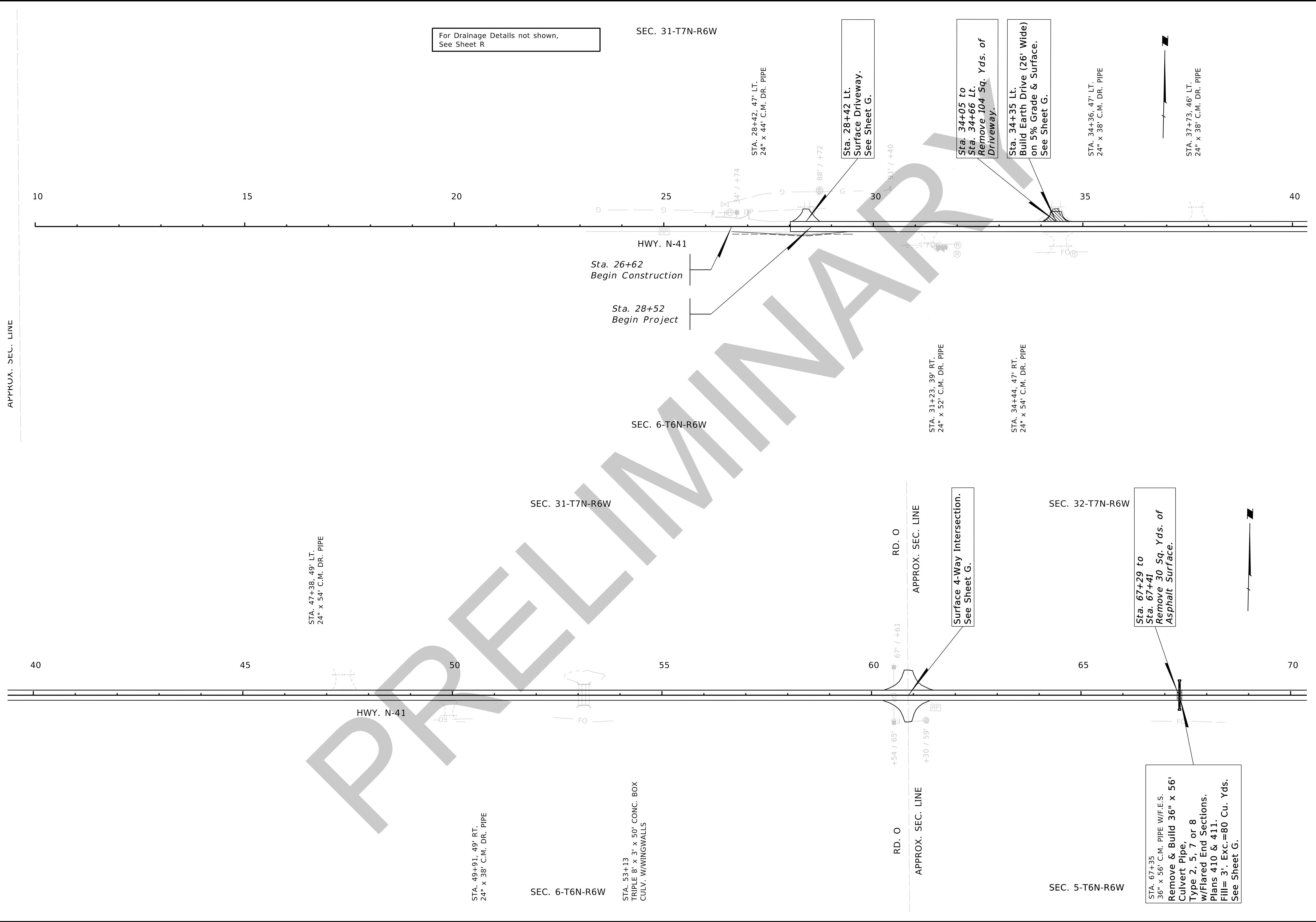
SEC. 2-T6N-R5W



BUILD EROSION CONTROL-CLASS 1D, PLAN 501						
STATION TO	STATION	SIDE	DESCRIPTION	WIDTH	SQ. YDS.	
629+89	- 630+36	Rt.	Culvert Outlet	48	122	
629+89	- 630+37	Lt.	Culvert Inlet	48	136	

SEC. 1-T6N-R5W

EROSION & SEDIMENT CONTROL



L1

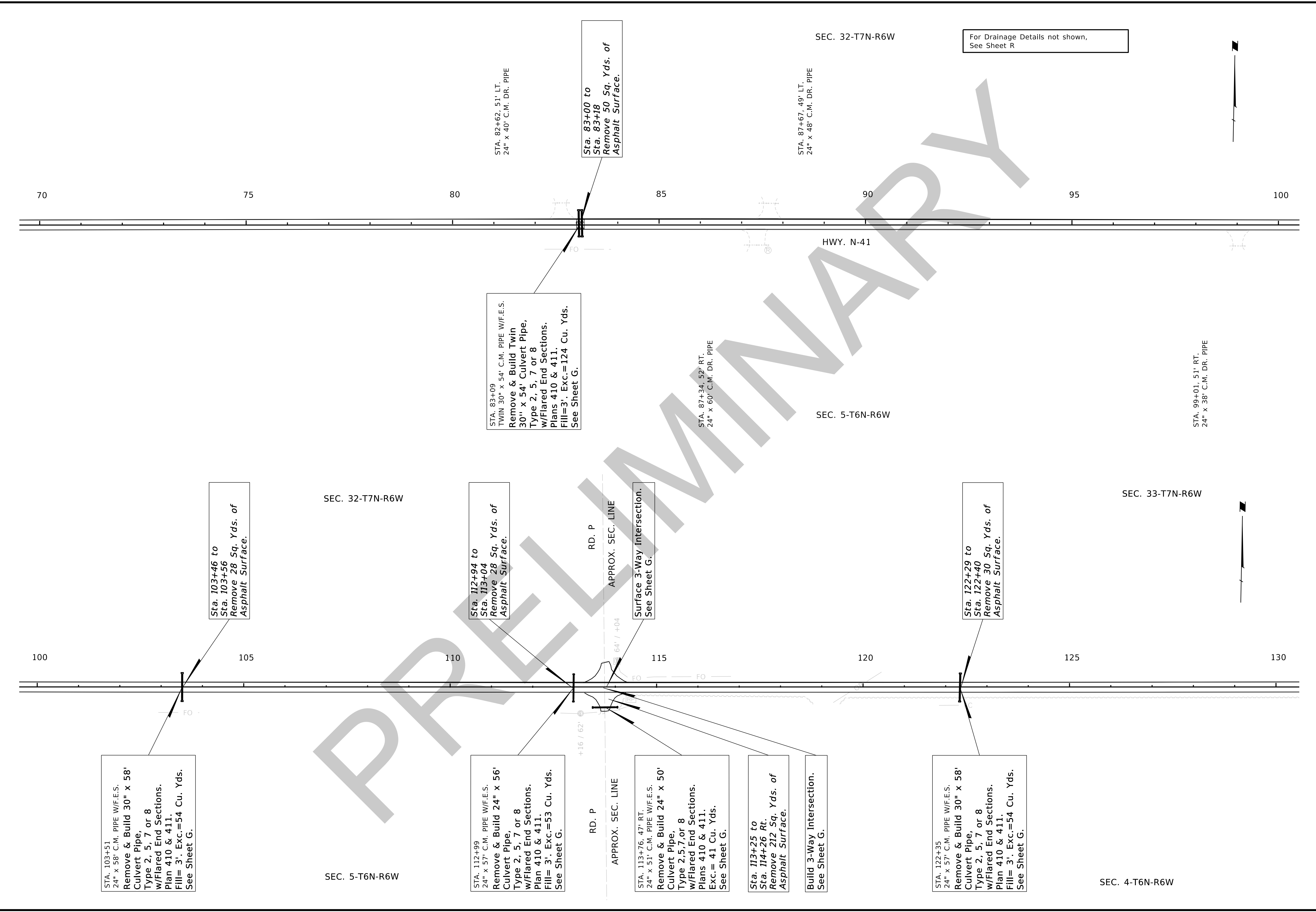
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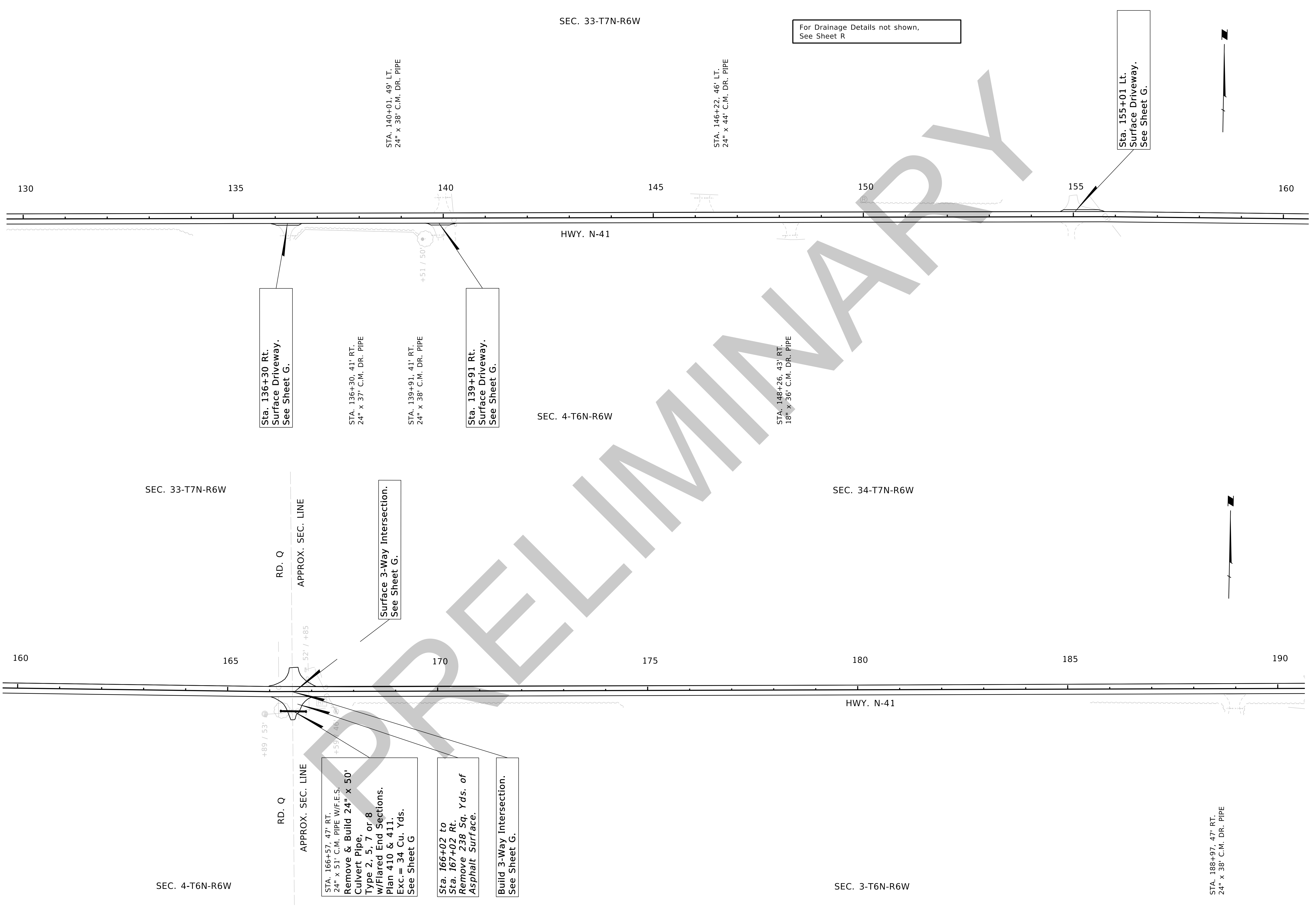
C.N. 42829

PLAN - PLAN

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Roadway Design
Division





For Drainage Details not shown, See Sheet R

Sta. 136+30 Rt. Surface Driveway. See Sheet G.

STA. 136+30, 41' RT. 24" x 37' C.M. DR. PIPE

STA. 139+91, 41' RT. 24" x 38" C.M. DR. PIPE

Sta. 139+91 Rt. Surface Driveway. See Sheet G.

Surface 3-Way Intersection. See Sheet G.

RD. Q APPROX. SEC. LINE

RD. Q APPROX. SEC. LINE

STA. 166+57, 47' RT. 24" x 51' C.M. PIPE W/F.E.S. Remove & Build 24" x 50' Culvert Pipe, Type 2, 5, 7 or 8 w/Flared End Sections. Plan 410 & 411. Exc. = 34 Cu. Yds. See Sheet G

Sta. 166+02 to Sta. 167+02 Rt. Remove 238 Sq. Yds. of Asphalt Surface.

Build 3-Way Intersection. See Sheet G.

STA. 146+22, 46' LT. 24" x 44' C.M. DR. PIPE

STA. 148+26, 43' RT. 18" x 36' C.M. DR. PIPE

Sta. 155+01 Lt. Surface Driveway. See Sheet G.

STA. 188+97, 47' RT. 24" x 38' C.M. DR. PIPE

L3
Project Number 41-5(108)
C.N. 42829

PLAN - PLAN

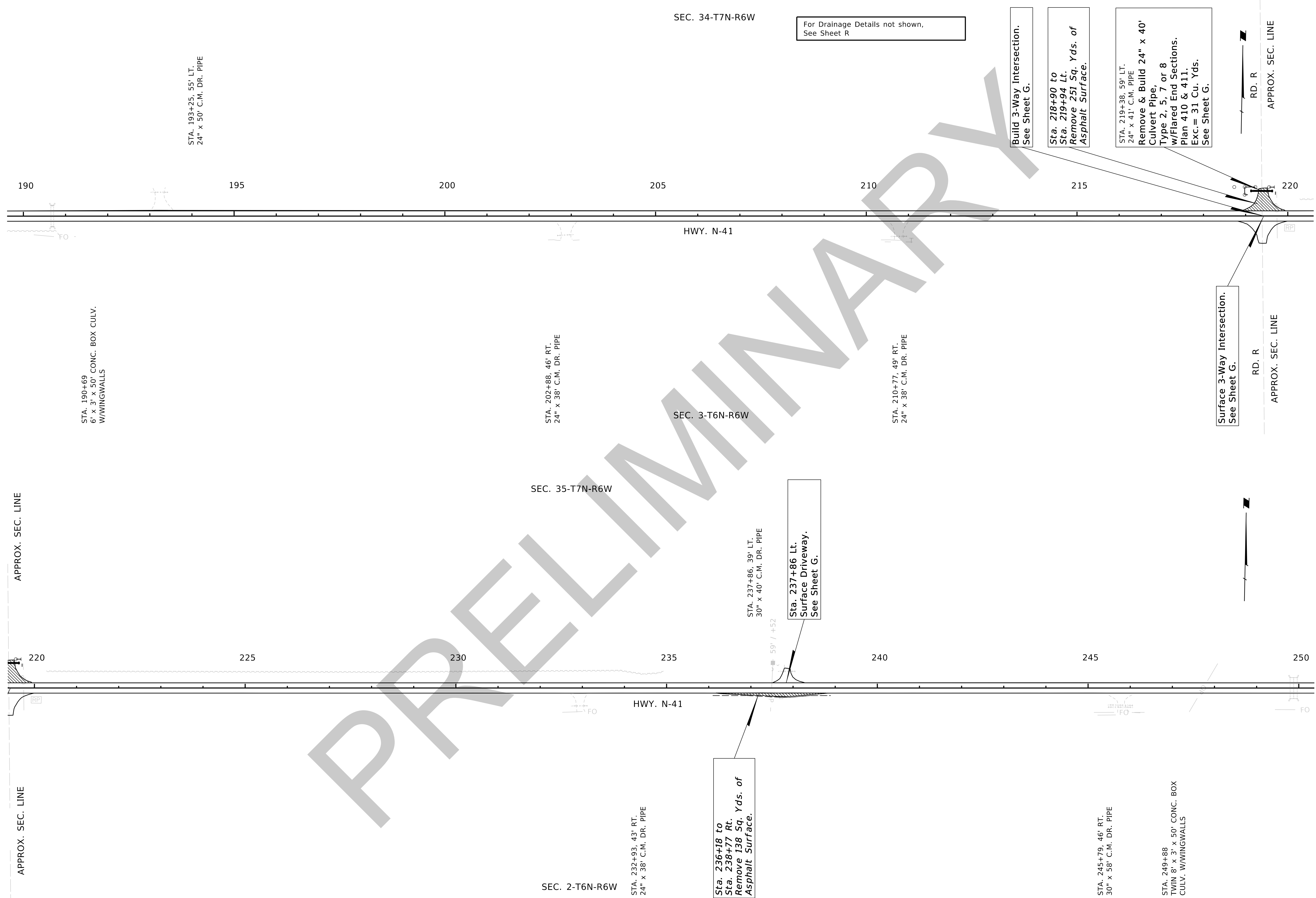


Roadway Design Division

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For Drainage Details not shown,
See Sheet R

Build 3-Way Intersection.
See Sheet G.

Sta. 218+90 to
Sta. 219+94 Lt.
Remove 251 Sq. Yds. of
Asphalt Surface.

STA. 219+38, 59' LT.
24" x 41' C.M. PIPE
Remove & Build 24" x 40'
Culvert Pipe,
Type 2, 5, 7 or 8
w/Flared End Sections.
Plan 410 & 411.
Exc. = 31 Cu. Yds.
See Sheet G.

Surface 3-Way Intersection.
See Sheet G.

Sta. 237+86 Lt.
Surface Driveway.
See Sheet G.

Sta. 236+18 to
Sta. 238+77 Rt.
Remove 138 Sq. Yds. of
Asphalt Surface.

L4
 Project Number
 41-5(108)
 C.N. 42829

PLAN - PLAN



Roadway
 Design
 Division

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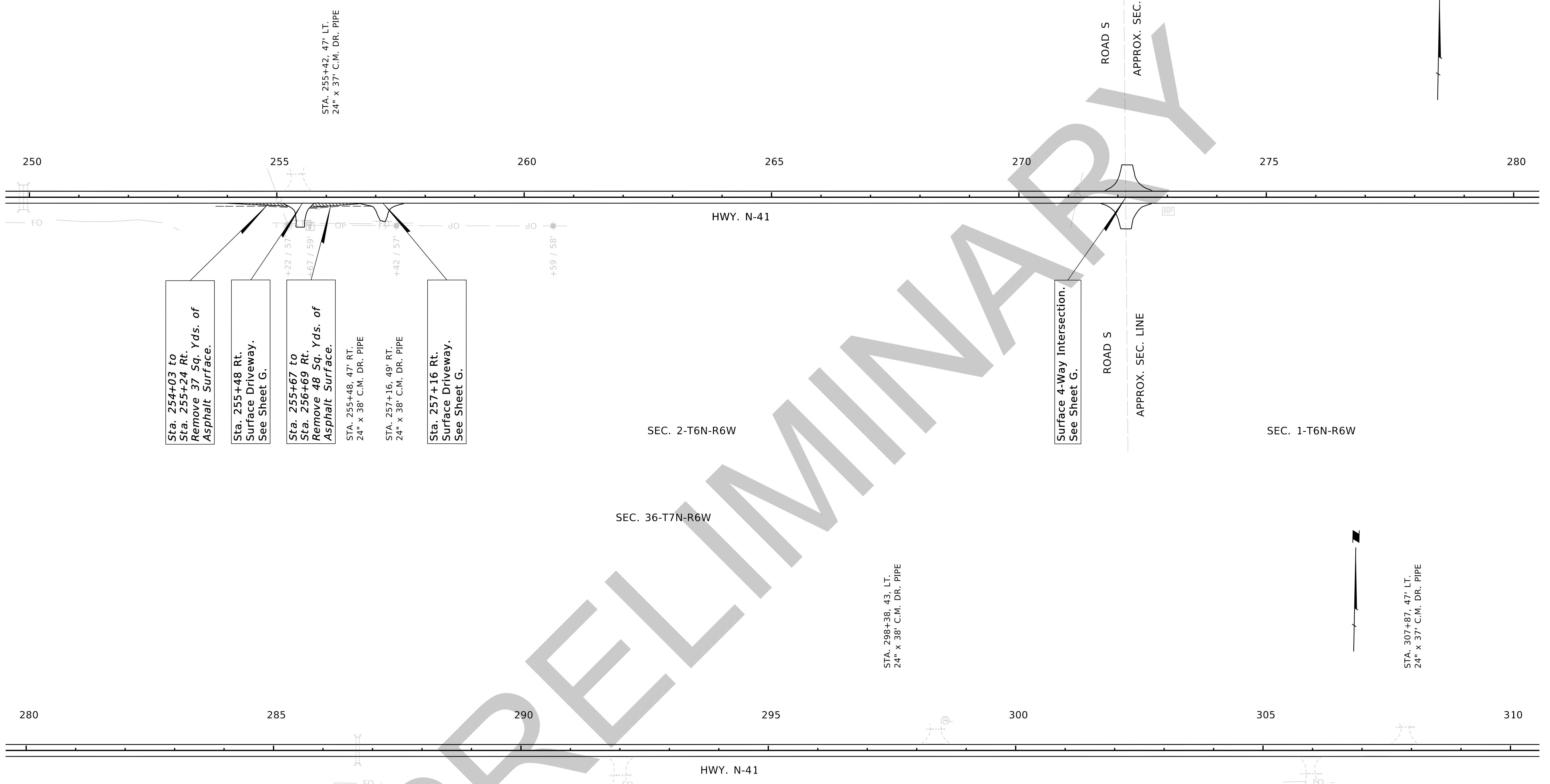
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SEC. 35-T7N-R6W

SEC. 36-T7N-R6W

For Drainage Details not shown,
See Sheet R



L5

Project Number
41-5(108)

C.N. 42829

PLAN - PLAN

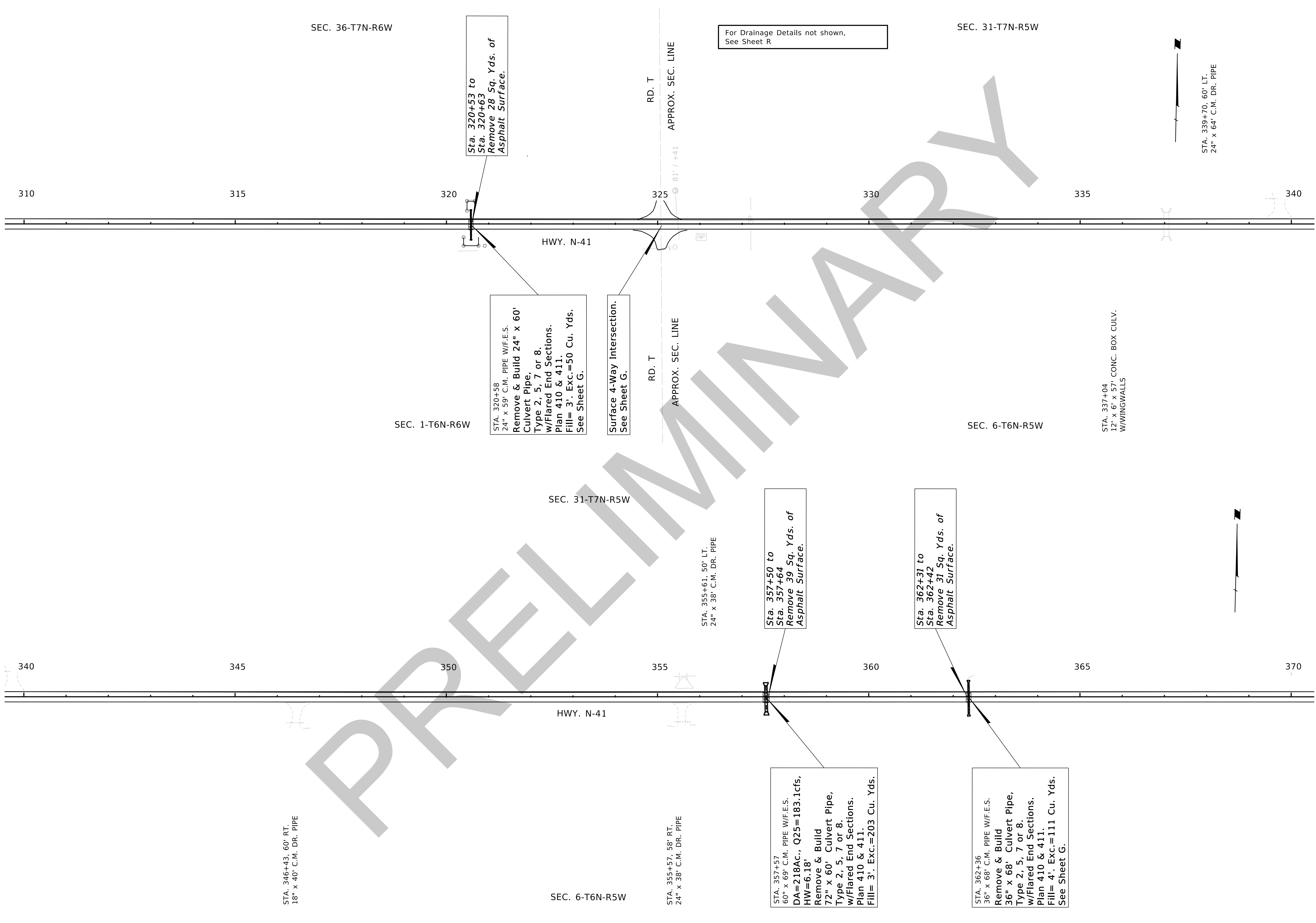
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DEPARTMENT OF TRANSPORTATION

Roadway
Design
Division

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SEC. 36-T7N-R6W

SEC. 31-T7N-R5W

SEC. 1-T6N-R6W

SEC. 6-T6N-R5W

SEC. 31-T7N-R5W

SEC. 6-T6N-R5W

For Drainage Details not shown,
See Sheet R

Sta. 320+53 to
Sta. 320+63
Remove 28 Sq. Yds. of
Asphalt Surface.

STA. 320+58
24" x 59' C.M. PIPE W/F.E.S.
Remove & Build 24" x 60'
Culvert Pipe,
Type 2, 5, 7 or 8.
w/Flared End Sections.
Plan 410 & 411.
Fill= 3'. Exc.=50 Cu. Yds.
See Sheet G.

Surface 4-Way Intersection.
See Sheet G.

Sta. 357+50 to
Sta. 357+64
Remove 39 Sq. Yds. of
Asphalt Surface.

STA. 357+57
60" x 69' C.M. PIPE W/F.E.S.
DA=218AC, Q25=183.1cfs,
HW=6.18'
Remove & Build
72" x 60' Culvert Pipe,
Type 2, 5, 7 or 8.
w/Flared End Sections.
Plan 410 & 411.
Fill= 3'. Exc.=203 Cu. Yds.

Sta. 362+31 to
Sta. 362+42
Remove 31 Sq. Yds. of
Asphalt Surface.

STA. 362+36
36" x 68' C.M. PIPE W/F.E.S.
Remove & Build
36" x 68' Culvert Pipe,
Type 2, 5, 7 or 8.
w/Flared End Sections.
Plan 410 & 411.
Fill= 4'. Exc.=111 Cu. Yds.
See Sheet G.

STA. 339+70, 60' LT.
24" x 64' C.M. DR. PIPE

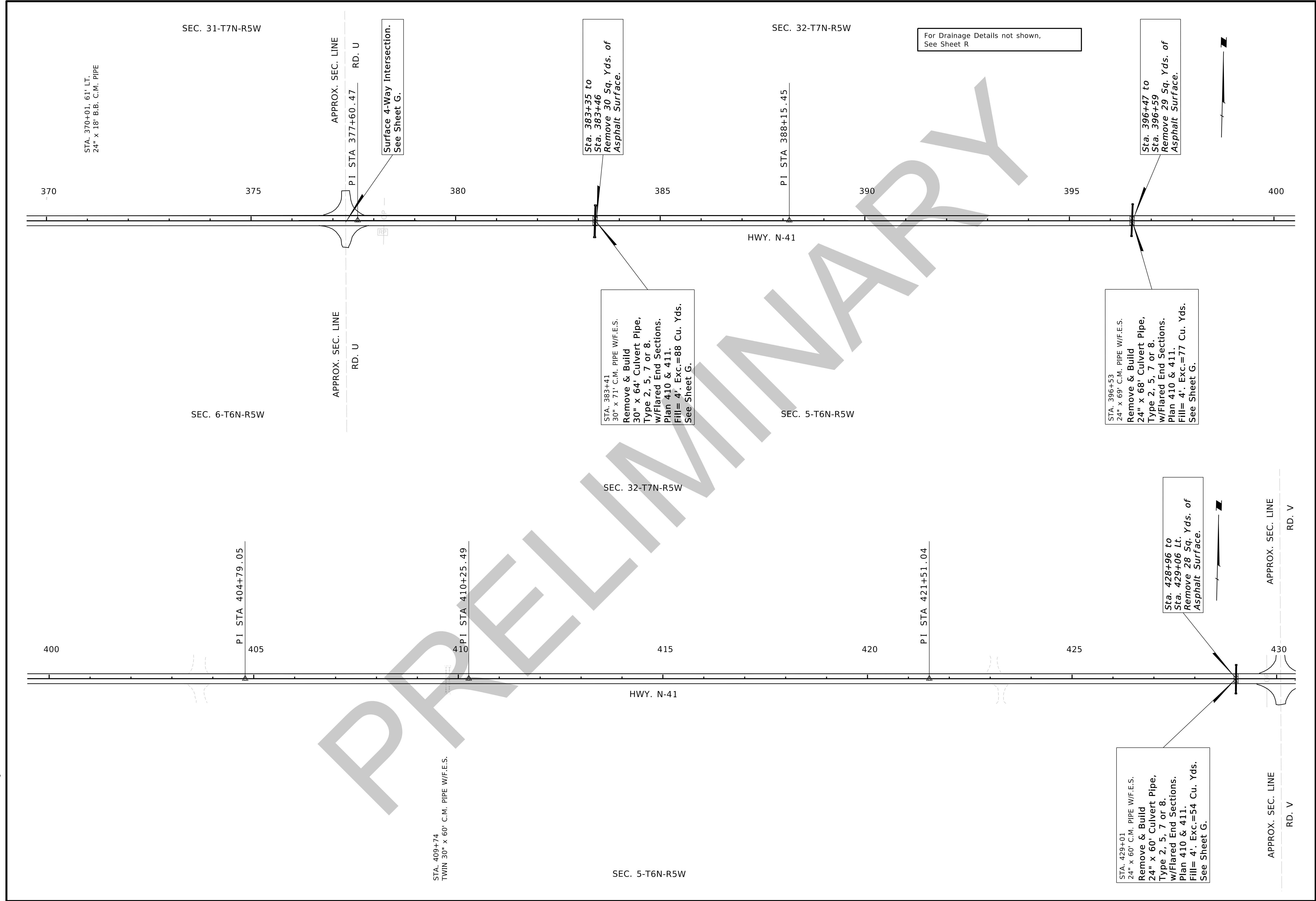
STA. 337+04
12' x 6' x 57' CONC. BOX CULV.
W/WINGWALLS

L6
Project Number
41-5(108)
C.N. 42829

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Roadway
Design
Division

PLAN - PLAN



L7

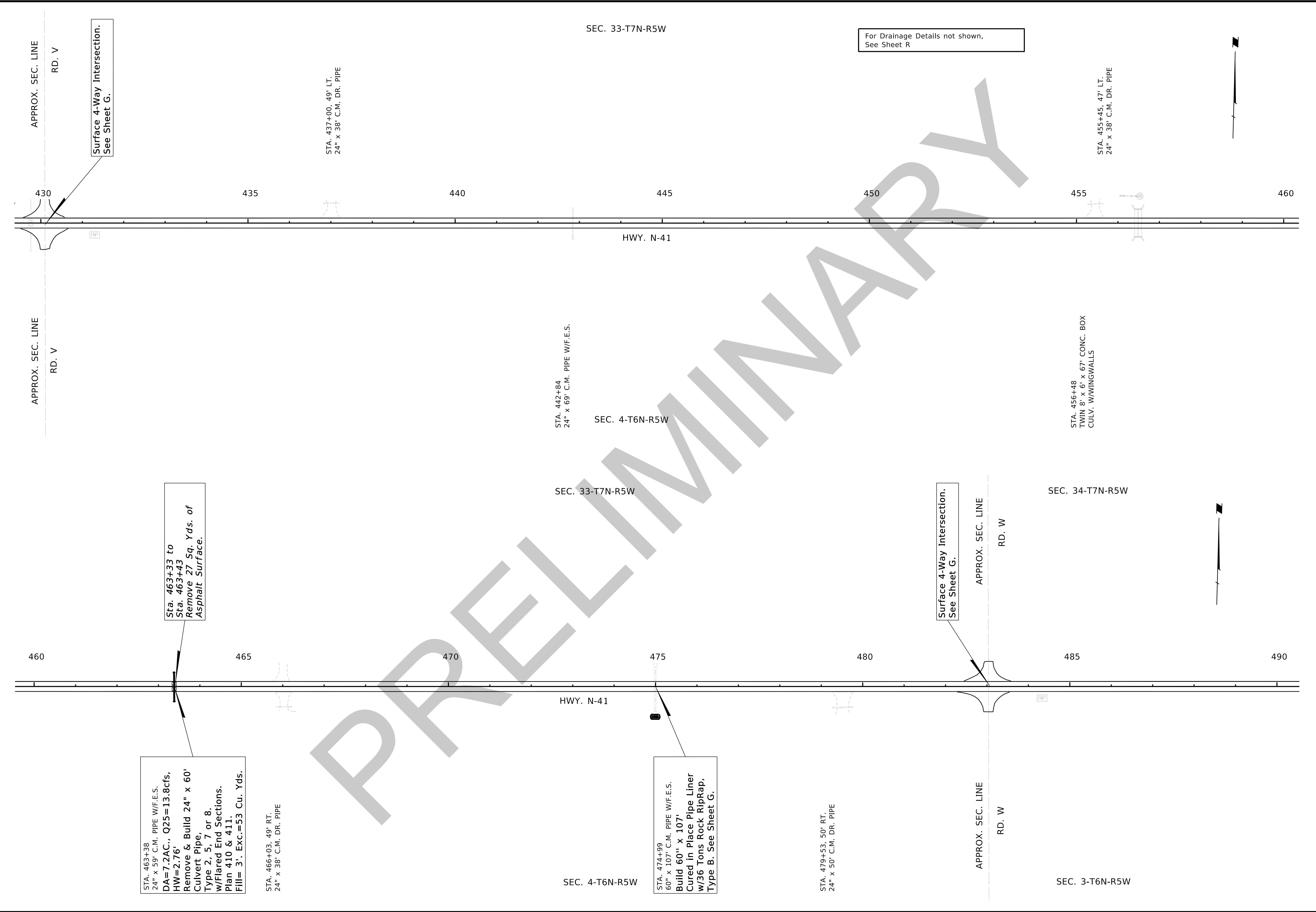
Project Number
41-5(108)

C.N. 42829

PLAN - PLAN

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Roadway Design
Division



L8

Project Number
41-5(108)

C.N. 42829

PLAN - PLAN



Roadway
Design
Division

SEC. 34-T7N-R5W

For Drainage Details not shown,
See Sheet R

L9
Project Number
41-5(108)
C.N. 42829

490 495 500 505 510 515 520

HWY. N-41

STA. 496+44, 47' RT.
24" x 38' C.M. DR. PIPE

STA. 509+75, 48' LT.
30" x 40' C.M. DR. PIPE

SEC. 3-T6N-R5W

STA. 509+42, 47' RT.
36" x 42' C.M. DR. PIPE

SEC. 34-T7N-R5W

RD. X
APPROX. SEC. LINE

Surface 3-Way Intersection.
See Sheet G.

STA. 535+82, 55' LT.
10' x 4' x 46' CONC. BOX CULV.
W/WINGWALLS

SEC. 35-T7N-R5W

STA. 547+00, 49' LT.
24" x 35' C.M. DR. PIPE

520 525 530 535 540 545 550

HWY. N-41

PRELIMINARY

STA. 535+84, 68' RT.
TWIN 36" x 44' C.M. PIPE W/F.E.S.
Remove & Build Twin
36" x 44' Culvert Pipe,
Type 2, 5, 7 or 8
w/Flared End Sections.
Plan 410 & 411.
Exc. = 150 Cu. Yds.
See Sheet G.

RD. X

APPROX. SEC. LINE

Sta. 535+17 to
Sta. 536+56 Rt.
Remove 345 Sq. Yds. of
Asphalt Surface.

Build 3-Way Intersection.
See Sheet G.

STA. 541+17
6' x 4' x 52' CONC. BOX CULV.
W/WINGWALLS

SEC. 3-T6N-R5W

SEC. 2-T6N-R5W

STA. 547+00, 47' RT.
24" x 38' C.M. DR. PIPE

PLAN - PLAN

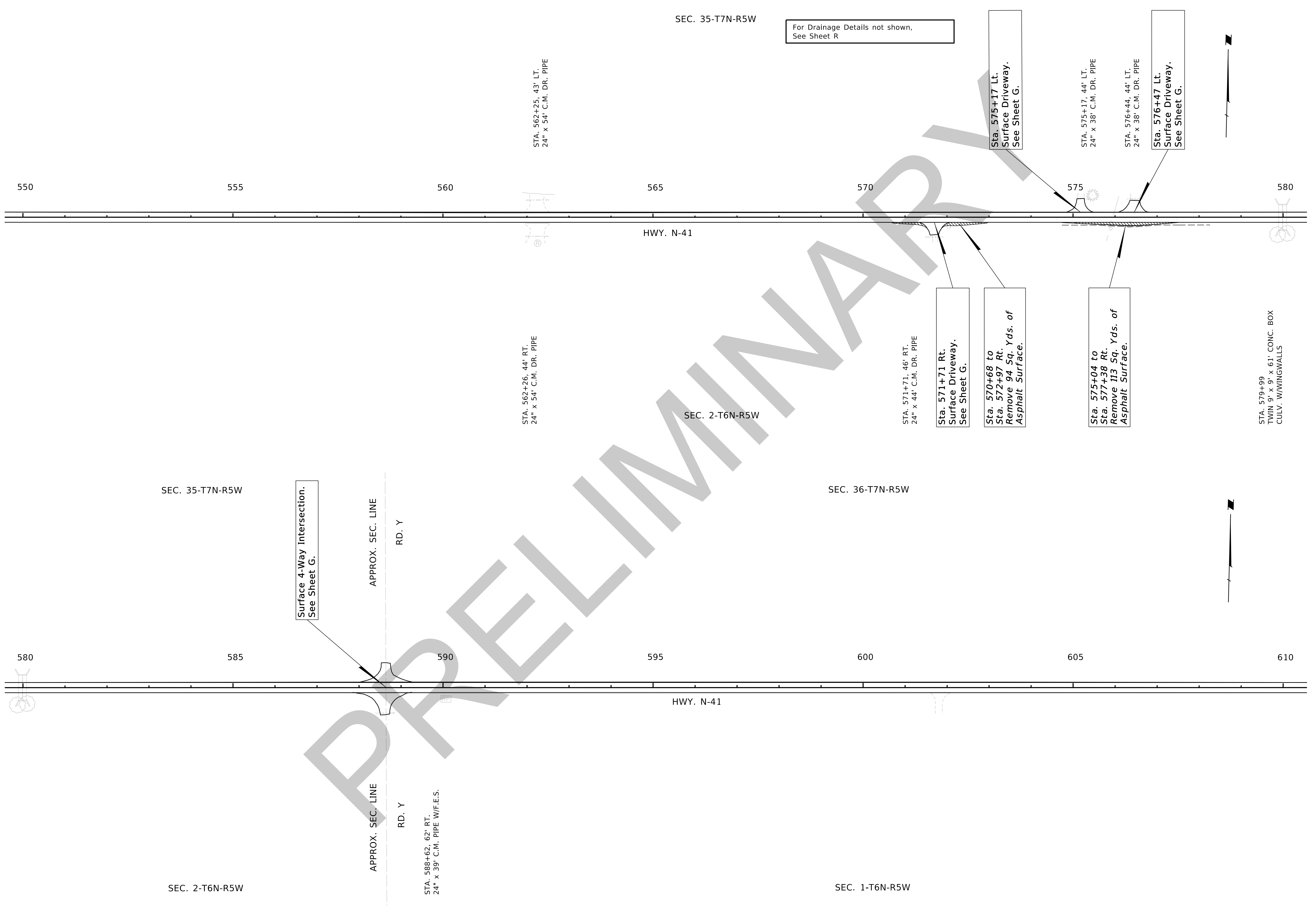
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L10

Project Number
41-5(108)

C.N. 42829

PLAN - PLAN

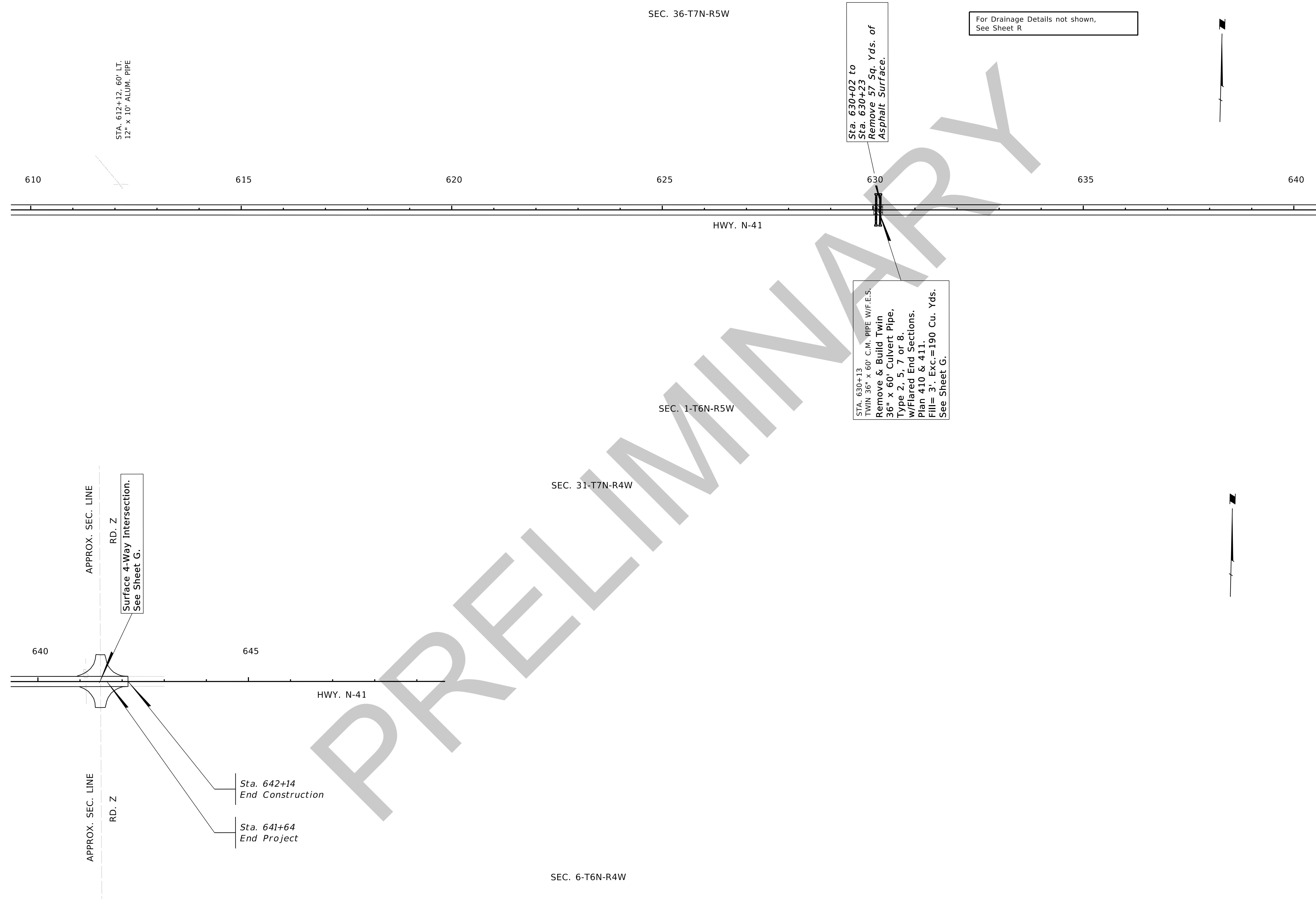
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L11

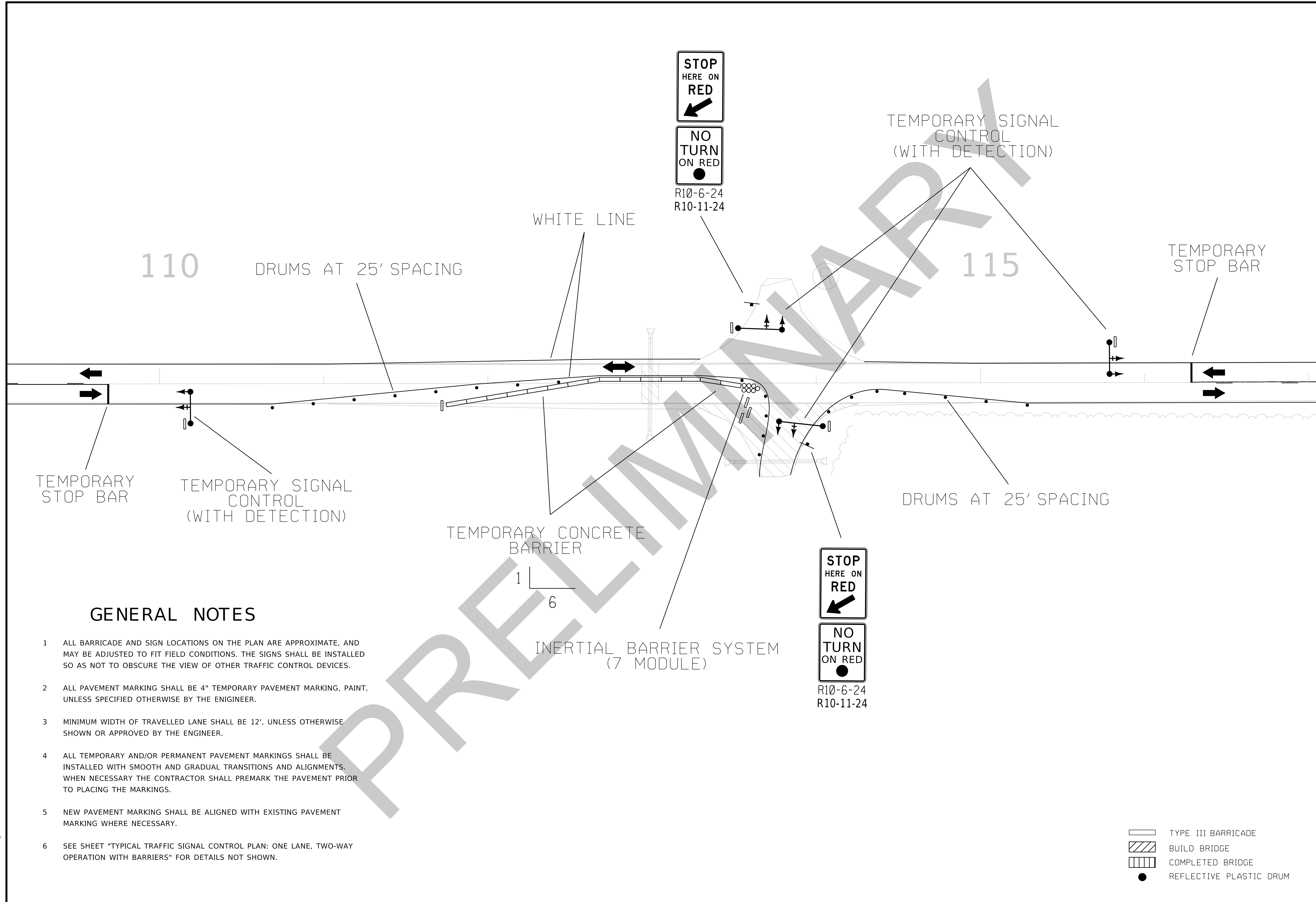
Project Number
41-5(108)

C.N. 42829

PLAN - PLAN

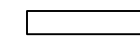



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Roadway
Design
Division



GENERAL NOTES

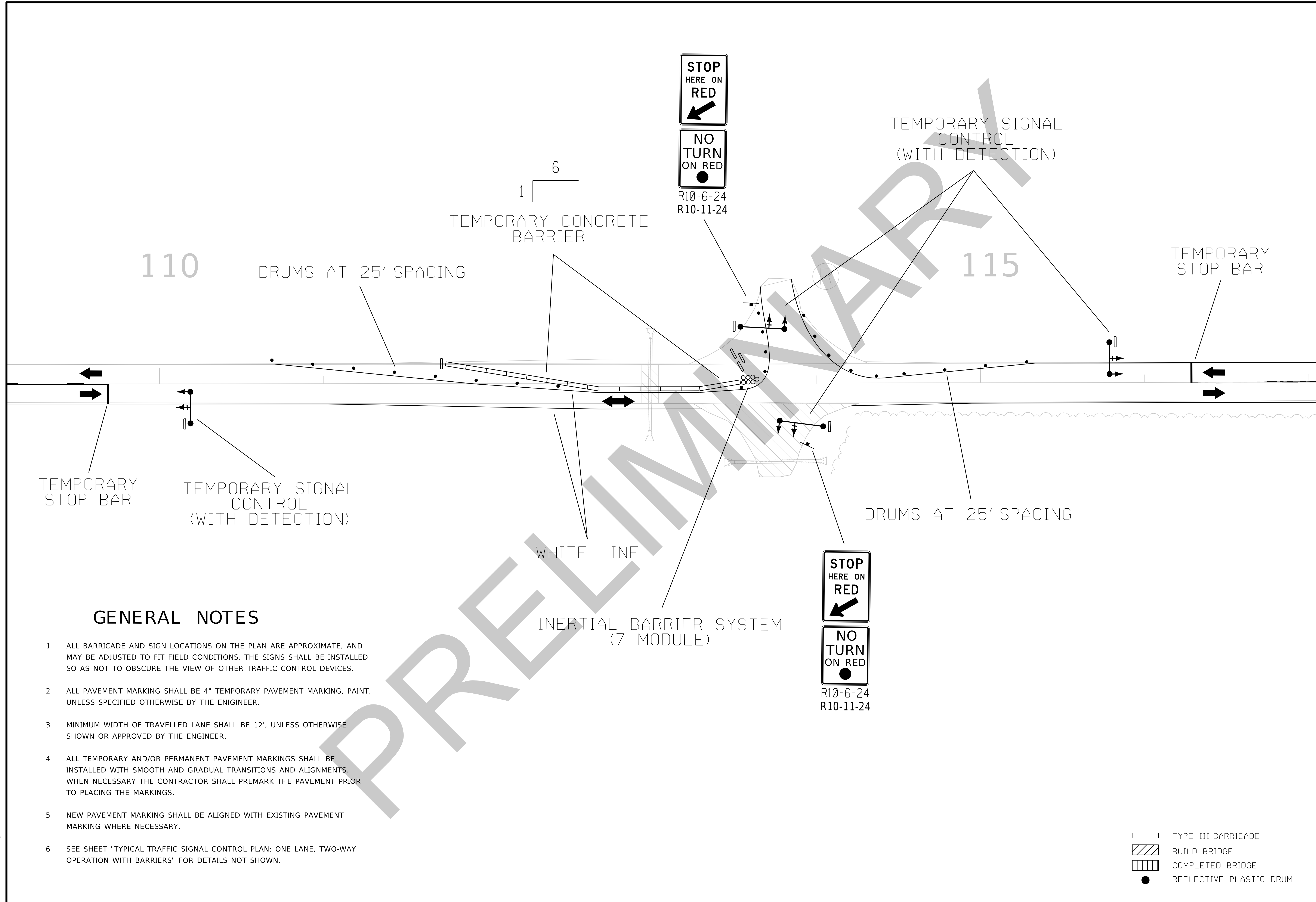
- 1 ALL BARRICADE AND SIGN LOCATIONS ON THE PLAN ARE APPROXIMATE, AND MAY BE ADJUSTED TO FIT FIELD CONDITIONS. THE SIGNS SHALL BE INSTALLED SO AS NOT TO OBSCURE THE VIEW OF OTHER TRAFFIC CONTROL DEVICES.
- 2 ALL PAVEMENT MARKING SHALL BE 4" TEMPORARY PAVEMENT MARKING, PAINT, UNLESS SPECIFIED OTHERWISE BY THE ENGINEER.
- 3 MINIMUM WIDTH OF TRAVELLED LANE SHALL BE 12', UNLESS OTHERWISE SHOWN OR APPROVED BY THE ENGINEER.
- 4 ALL TEMPORARY AND/OR PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED WITH SMOOTH AND GRADUAL TRANSITIONS AND ALIGNMENTS, WHEN NECESSARY THE CONTRACTOR SHALL PREMARK THE PAVEMENT PRIOR TO PLACING THE MARKINGS.
- 5 NEW PAVEMENT MARKING SHALL BE ALIGNED WITH EXISTING PAVEMENT MARKING WHERE NECESSARY.
- 6 SEE SHEET "TYPICAL TRAFFIC SIGNAL CONTROL PLAN: ONE LANE, TWO-WAY OPERATION WITH BARRIERS" FOR DETAILS NOT SHOWN.

-  TYPE III BARRICADE
-  BUILD BRIDGE
-  COMPLETED BRIDGE
-  REFLECTIVE PLASTIC DRUM

COMPUTER: BG0419M687

DATE: 12-MAR-2024 15:03

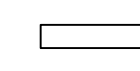
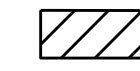
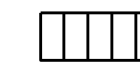

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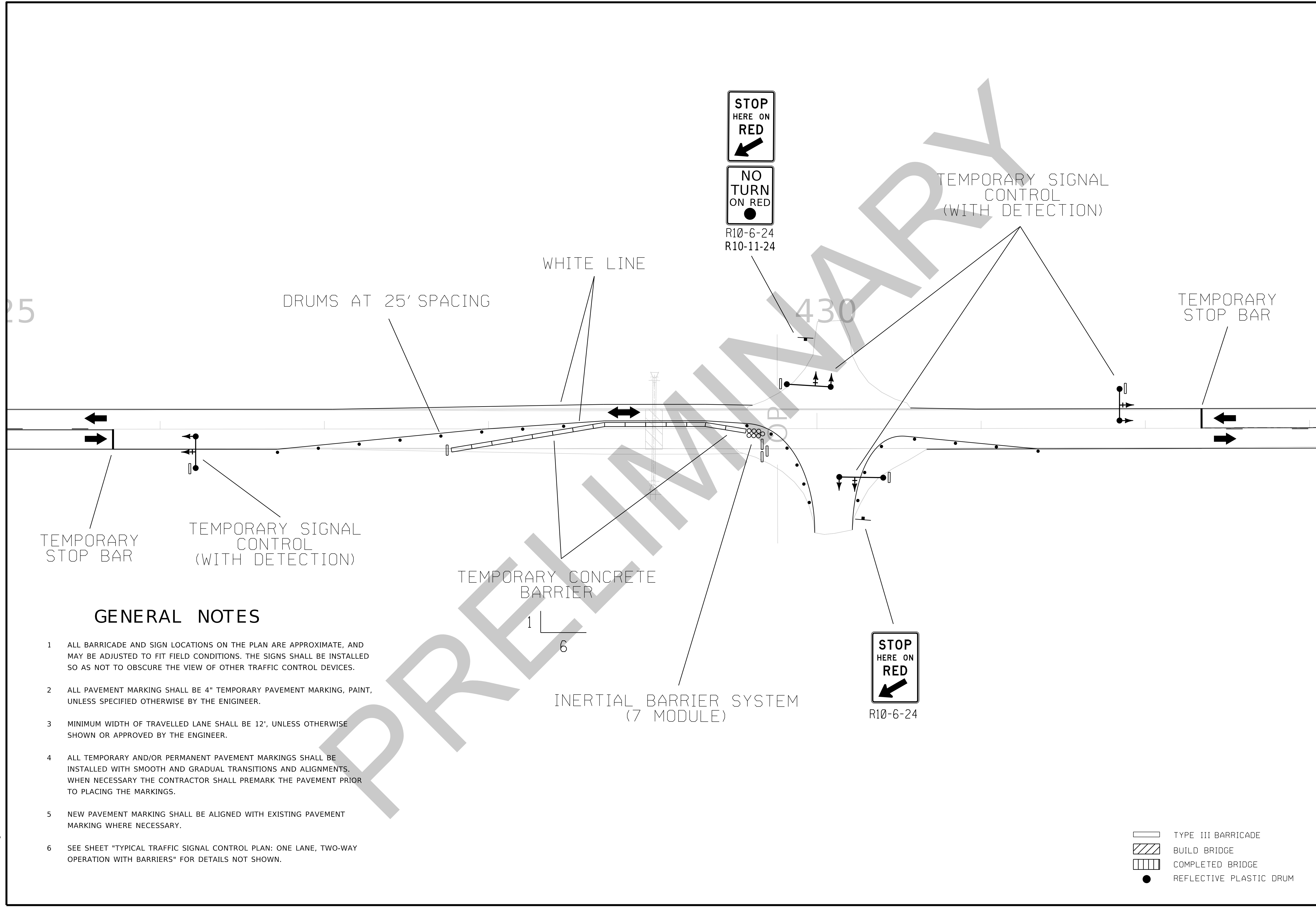
TRAFFIC CONTROL PLAN
 CULVERT @ STA 112+99 PHASE 2
 DESIGNED BY AJM
 DATE 12/23
 NEBRASKA DEPARTMENT OF TRANSPORTATION - TRAFFIC ENGINEERING DIVISION

GENERAL NOTES

- 1 ALL BARRICADE AND SIGN LOCATIONS ON THE PLAN ARE APPROXIMATE, AND MAY BE ADJUSTED TO FIT FIELD CONDITIONS. THE SIGNS SHALL BE INSTALLED SO AS NOT TO OBSCURE THE VIEW OF OTHER TRAFFIC CONTROL DEVICES.
- 2 ALL PAVEMENT MARKING SHALL BE 4" TEMPORARY PAVEMENT MARKING, PAINT, UNLESS SPECIFIED OTHERWISE BY THE ENIGINEER.
- 3 MINIMUM WIDTH OF TRAVELLED LANE SHALL BE 12', UNLESS OTHERWISE SHOWN OR APPROVED BY THE ENGINEER.
- 4 ALL TEMPORARY AND/OR PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED WITH SMOOTH AND GRADUAL TRANSITIONS AND ALIGNMENTS. WHEN NECESSARY THE CONTRACTOR SHALL PREMARK THE PAVEMENT PRIOR TO PLACING THE MARKINGS.
- 5 NEW PAVEMENT MARKING SHALL BE ALIGNED WITH EXISTING PAVEMENT MARKING WHERE NECESSARY.
- 6 SEE SHEET "TYPICAL TRAFFIC SIGNAL CONTROL PLAN: ONE LANE, TWO-WAY OPERATION WITH BARRIERS" FOR DETAILS NOT SHOWN.

-  TYPE III BARRICADE
-  BUILD BRIDGE
-  COMPLETED BRIDGE
-  REFLECTIVE PLASTIC DRUM

COMPUTER: BG0419M687
 DATE: 12-MAR-2024 15:04
 FILE: w42829.dgn



STOP
HERE ON
RED
↙

NO
TURN
ON RED
●

R10-6-24
R10-11-24

TEMPORARY SIGNAL
CONTROL
(WITH DETECTION)

DRUMS AT 25' SPACING

WHITE LINE

430

TEMPORARY
STOP BAR

TEMPORARY
STOP BAR

TEMPORARY SIGNAL
CONTROL
(WITH DETECTION)

TEMPORARY CONCRETE
BARRIER

STOP
HERE ON
RED
↙

R10-6-24

INERTIAL BARRIER SYSTEM
(7 MODULE)

GENERAL NOTES

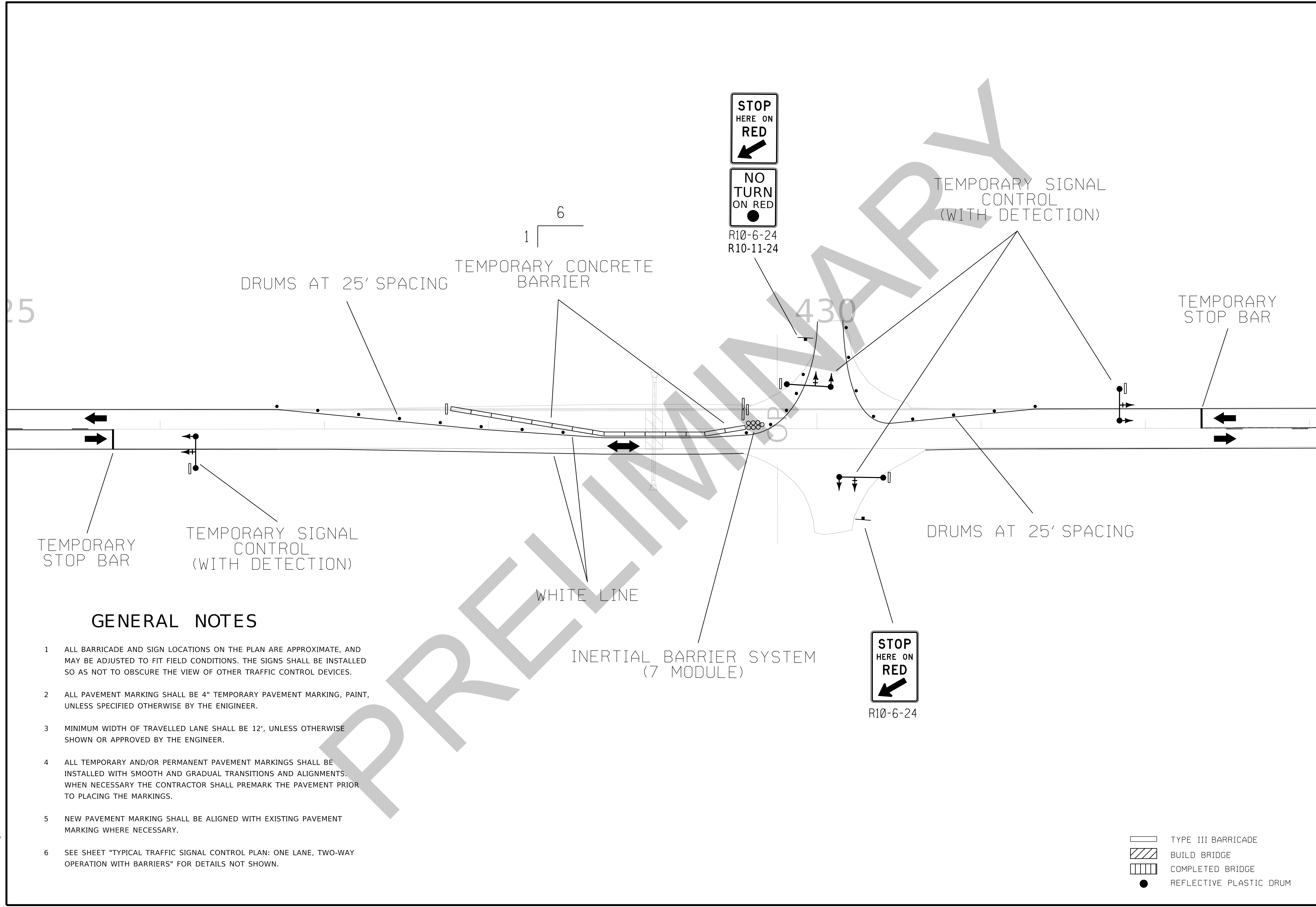
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- 2 ALL PAVEMENT MARKING SHALL BE 4" TEMPORARY PAVEMENT MARKING, PAINT, UNLESS SPECIFIED OTHERWISE BY THE ENIGINEER.
- 3 MINIMUM WIDTH OF TRAVELLED LANE SHALL BE 12', UNLESS OTHERWISE SHOWN OR APPROVED BY THE ENGINEER.
- 4 ALL TEMPORARY AND/OR PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED WITH SMOOTH AND GRADUAL TRANSITIONS AND ALIGNMENTS. WHEN NECESSARY THE CONTRACTOR SHALL PREMARK THE PAVEMENT PRIOR TO PLACING THE MARKINGS.
- 5 NEW PAVEMENT MARKING SHALL BE ALIGNED WITH EXISTING PAVEMENT MARKING WHERE NECESSARY.
- 6 SEE SHEET "TYPICAL TRAFFIC SIGNAL CONTROL PLAN: ONE LANE, TWO-WAY OPERATION WITH BARRIERS" FOR DETAILS NOT SHOWN.

- ▬ TYPE III BARRICADE
- ▨ BUILD BRIDGE
- ▧ COMPLETED BRIDGE
- REFLECTIVE PLASTIC DRUM

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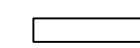



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GENERAL NOTES

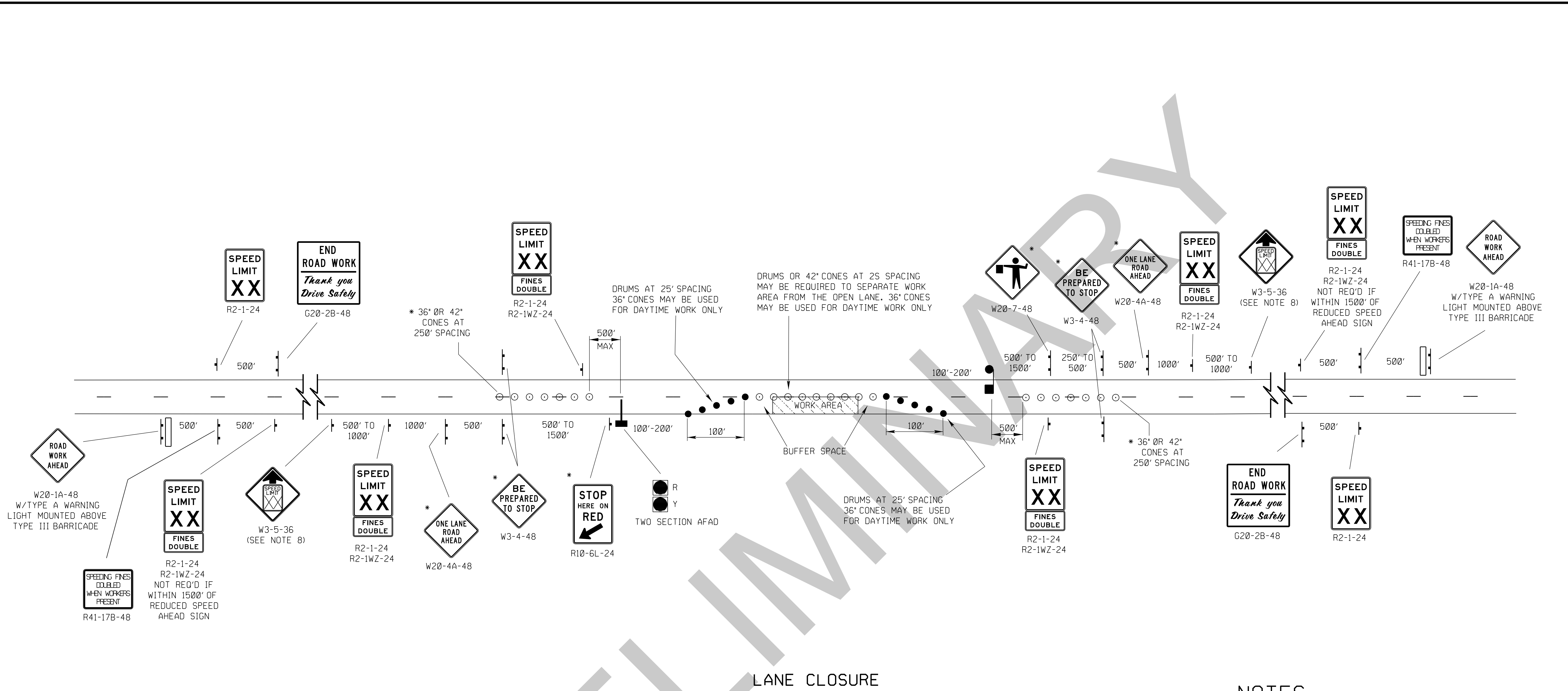
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- 2 ALL PAVEMENT MARKING SHALL BE 4" TEMPORARY PAVEMENT MARKING, PAINT, UNLESS SPECIFIED OTHERWISE BY THE ENGINEER.
- 3 MINIMUM WIDTH OF TRAVELLED LANE SHALL BE 12', UNLESS OTHERWISE SHOWN OR APPROVED BY THE ENGINEER.
- 4 ALL TEMPORARY AND/OR PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED WITH SMOOTH AND GRADUAL TRANSITIONS AND ALIGNMENTS. WHEN NECESSARY THE CONTRACTOR SHALL PREMARK THE PAVEMENT PRIOR TO PLACING THE MARKINGS.
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- 6 SEE SHEET "TYPICAL TRAFFIC SIGNAL CONTROL PLAN: ONE LANE, TWO-WAY OPERATION WITH BARRIERS" FOR DETAILS NOT SHOWN.

-  TYPE III BARRICADE
-  BUILD BRIDGE
-  COMPLETED BRIDGE
-  REFLECTIVE PLASTIC DRUM

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LANE CLOSURE

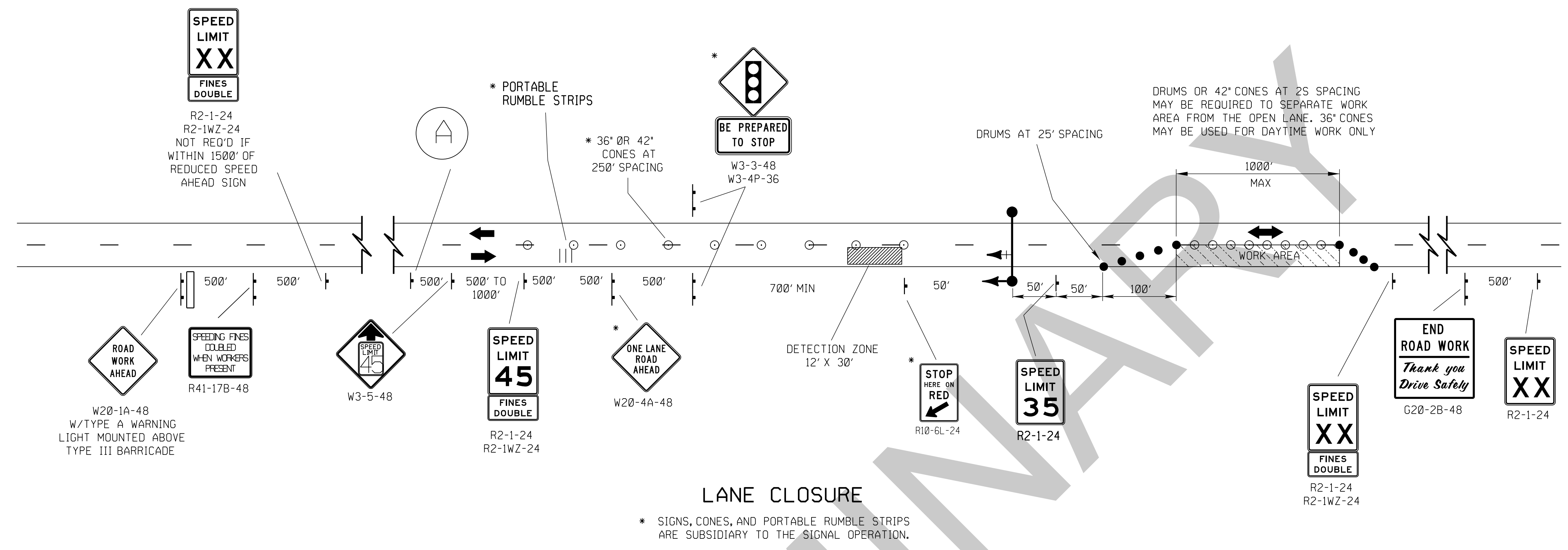
NOTES

* SIGNS AND CONES ARE SUBSIDIARY TO THE FLAGGING/AFAD OPERATION.

1. "FLAGGER AHEAD SYMBOL" SIGN (W20-7) SHALL BE USED WHEN A FLAGGER IS PRESENT, AND REMOVED WHEN NOT APPLICABLE.
2. THE CONTRACTOR SHALL INSTALL, MAINTAIN, AND REMOVE ALL SIGNS IN ACCORDANCE WITH THE DETAILS OF AND AT THE LOCATIONS SHOWN IN THE PLANS. SIGNS INSTALLED BY THE DEPARTMENT OF TRANSPORTATION OR OTHER GOVERNMENT AGENCY SHALL BE MAINTAINED AND REMOVED BY THEIR FORCES.
3. WHEN MESSAGE IS NOT PERTINENT, SIGNS SHALL BE TAKEN DOWN, COVERED OR FOLDED. TAPE IS NOT PERMITTED ON THE FACE OF THE SIGN.
4. VEHICLES OR EQUIPMENT SHALL NOT BE PARKED SO AS TO OBSCURE OR DISTRACT FROM TRAFFIC CONTROL DEVICES.
5. ORANGE FLAGS MAY BE USED TO CALL ATTENTION TO WARNING SIGNS.
6. REFER TO STANDARD PLAN 920 FOR GENERAL INFORMATION NOT SHOWN.
7. A MINIMUM OF 7-36" OR 42" CONES SHALL BE PLACED ON THE CENTERLINE IN ADVANCE OF THE FLAGGER. THE CONES SHOULD BE SPACED AT 250 FEET.
8. THE SPEED IN FLAGGING/PILOT CAR OPERATIONS IS GENERALLY CONTROLLED BY THE PILOT CAR, A SPEED REDUCTION MAY NOT BE NECESSARY IF THE WORK ZONE CONDITIONS WILL NOT EXIST UPON COMPLETION OF EACH DAYS WORK. W3-5 SIGN IS NOT NEEDED IF SPEED LIMIT IS NOT REDUCED.

LEGEND

- ● FLAGGER
- REFLECTORIZED PLASTIC DRUM
- REFLECTORIZED PLASTIC DRUM OR 42" CONE
- ▬ TYPE III BARRICADE
- ┆ SINGLE POSTED SIGN
- ┆ DOUBLE POSTED SIGN
- ┆ AFAD



GENERAL NOTES

- SIGNS SHOWN ARE USUALLY FOR ONE DIRECTION OF TRAVEL ONLY.
- THE CONTRACTOR SHALL INSTALL, MAINTAIN, AND REMOVE ALL SIGNS IN ACCORDANCE WITH THE DETAILS OF AND AT THE LOCATIONS SHOWN IN THE PLANS. SIGNS INSTALLED BY THE DEPARTMENT OF TRANSPORTATION OR OTHER GOVERNMENT AGENCIES SHALL BE MAINTAINED AND REMOVED BY THEIR FORCES.
- WHEN MESSAGE IS NOT PERTINENT, SIGNS SHALL BE TAKEN DOWN, COVERED OR FOLDED. TAPE WILL NOT BE PERMITTED ON THE FACE OF THE SIGN.
- ALL BARRICADE AND SIGN LOCATIONS ARE APPROXIMATE, AND MAY BE ADJUSTED TO FIT FIELD CONDITIONS. THE SIGNS SHALL BE INSTALLED SO AS TO NOT OBSCURE THE VIEW OF OTHER TRAFFIC CONTROL DEVICES FROM MOTORISTS.
- VEHICLES OR EQUIPMENT SHALL NOT BE PARKED AS TO OBSCURE OR DISTRACT FROM TRAFFIC CONTROL DEVICES.
- "NO PASSING ZONES NOT MARKED" SIGN (W25-6-48) SHOULD BE INSTALLED AT EACH END OF THE PROJECT WHENEVER THE EXISTING NO PASSING ZONE PAVEMENT MARKINGS HAVE BEEN REMOVED OR COVERED AND NO PASSING ZONE PAVEMENT MARKINGS ARE NOT INCLUDED IN THE PROJECT.
- SPEED LIMIT SIGN IS NOT REQUIRED IF WITHIN 1500 FT OF A REDUCED SPEED AHEAD SIGN.
- WORK ZONE SPEED LIMITS SHALL NOT BE INSTALLED WITHOUT A SPEED ZONE AUTHORIZATION COMPLETED BY THE DEPARTMENT. THE WORK ZONE SPEED LIMIT SHALL BE ESTABLISHED ACCORDING TO DOT-01 60-18. SEE WORK ZONE SPEED LIMIT NOTES ON STANDARD PLAN 920.
- A SPEED LIMIT SIGN ENDING THE REDUCED SPEED ZONE SHALL BE INSTALLED AT THE END OF EACH ZONE.
- PLACE TYPE II BARRICADES, REFLECTORIZED PLASTIC DRUMS, OR 42" CONES ON THE TRAFFIC SIDE OF THE DROP-OFF WHERE SUFFICIENT LATERAL DISTANCE EXISTS BETWEEN THE TRAVEL LANE AND THE DROP-OFF (DROP-OFF DETAIL ON STANDARD PLAN 922).
- THE LEAD SIGNS ARE NOT NEEDED IF TWO PROJECTS ARE LESS THAN 1 MILE APART. THE "END CONSTRUCTION" SIGN (G20-2B-48) SHOULD NOT BE INSTALLED BETWEEN THE PROJECTS.
- ON ARMOR COAT SURFACING, A "LOOSE GRAVEL" SIGN (W8-7-36) IS REQUIRED AT THE BEGINNING OF THE DAYS WORK AND SHALL REMAIN IN PLACE UNTIL THE LOOSE GRAVEL HAS BEEN SWEEPED OFF.
- SIGN SIZES SHOWN ARE FOR TYPICAL SITUATIONS- REFER TO NEBRASKA SUPPLEMENT TO THE MUTCD FOR FURTHER SIZE INFORMATION.
- REFER TO STANDARD PLAN 920 FOR GENERAL INFORMATION NOT SHOWN.
- A MINIMUM OF 7-36" OR 42" CONES SHALL BE PLACED ON CENTERLINE IN ADVANCE OF THE TEMPORARY SIGNAL. THE CONES SHOULD BE SPACED AT 250 FEET.

LANE CLOSURE

* SIGNS, CONES, AND PORTABLE RUMBLE STRIPS ARE SUBSIDIARY TO THE SIGNAL OPERATION.

LEGEND

- FLAGGER
- REFLECTORIZED PLASTIC DRUM
- REFLECTORIZED PLASTIC DRUM OR 42" CONE
- TYPE III BARRICADE
- SINGLE POSTED SIGN
- DOUBLE POSTED SIGN
- TRAFFIC SIGNAL

TAPER FORMULA

$L = S \times W$ FOR SPEEDS OF 45 MPH OR MORE.
 $L = \frac{WS^2}{60}$ FOR SPEEDS OF 40 MPH OR LESS.

WHERE:
 L = MINIMUM LENGTH OF TAPER.
 S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK.
 W = WIDTH OF OFFSET (LANE WIDTH).

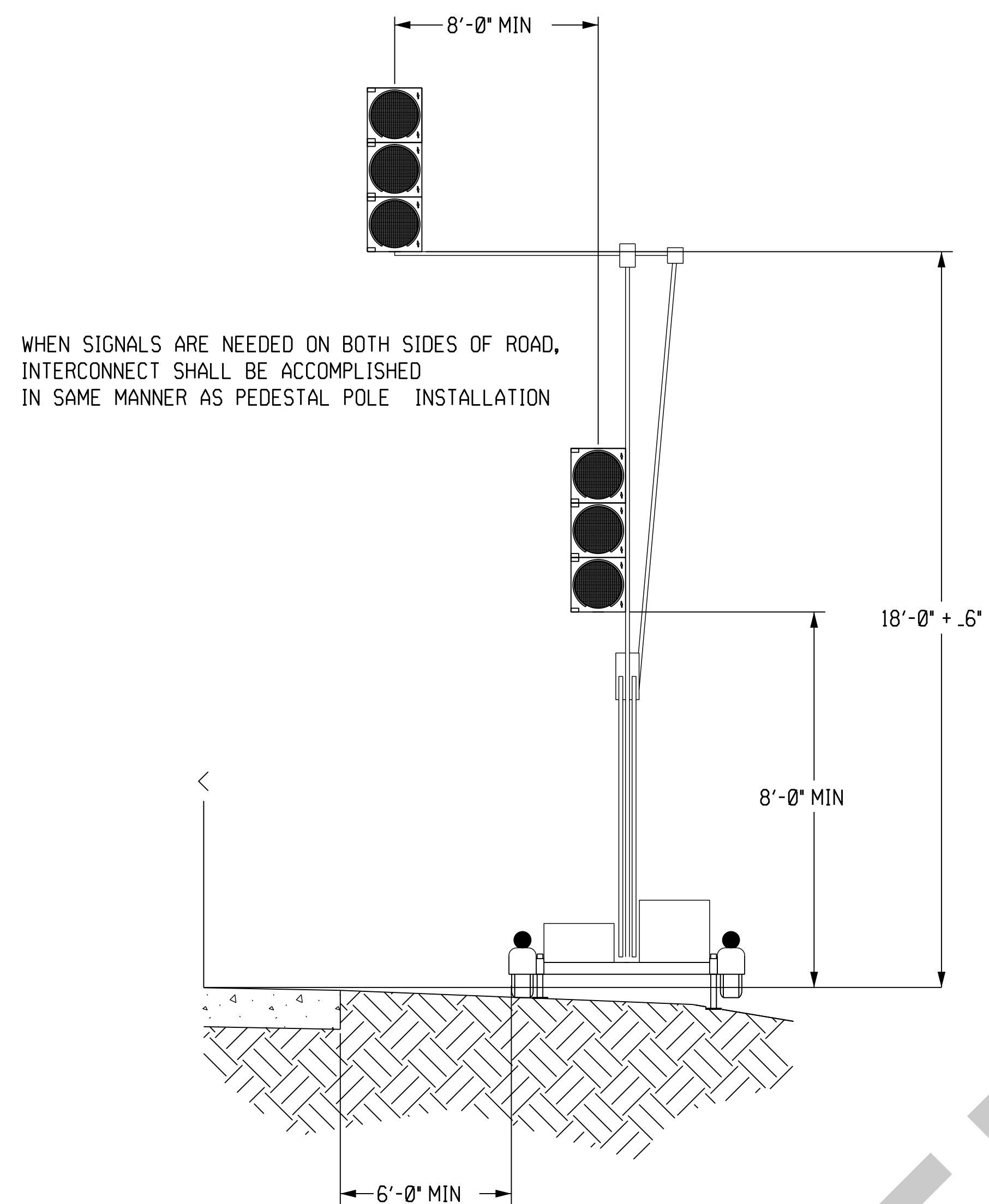
TABLE A: TEMPORARY TRAFFIC SIGNAL TIMING

RECOMMENDED SETTINGS (SEC.)	DISTANCE BETWEEN "STOP HERE ON RED" SIGN LOCATIONS (FT)	ALL RED (SEC.)
INITIAL GREEN - 15	500' - 700'	19
EXTENSION - 2.5	700' - 850'	24
MAX. GREEN - 45	850' - 1,000'	28
YELLOW - 5	1,000' - 1,250'	34
	1,250' - 1,500'	41

- FRESH OIL
W21-2-36
- REDUCE SPEED
LOOSE GRAVEL ON SURFACE
W8-7C-48
- MILLED SURFACE
W41-24-36
- LOOSE GRAVEL
W8-7-36
WHERE REQUIRED BY THE ENGINEER

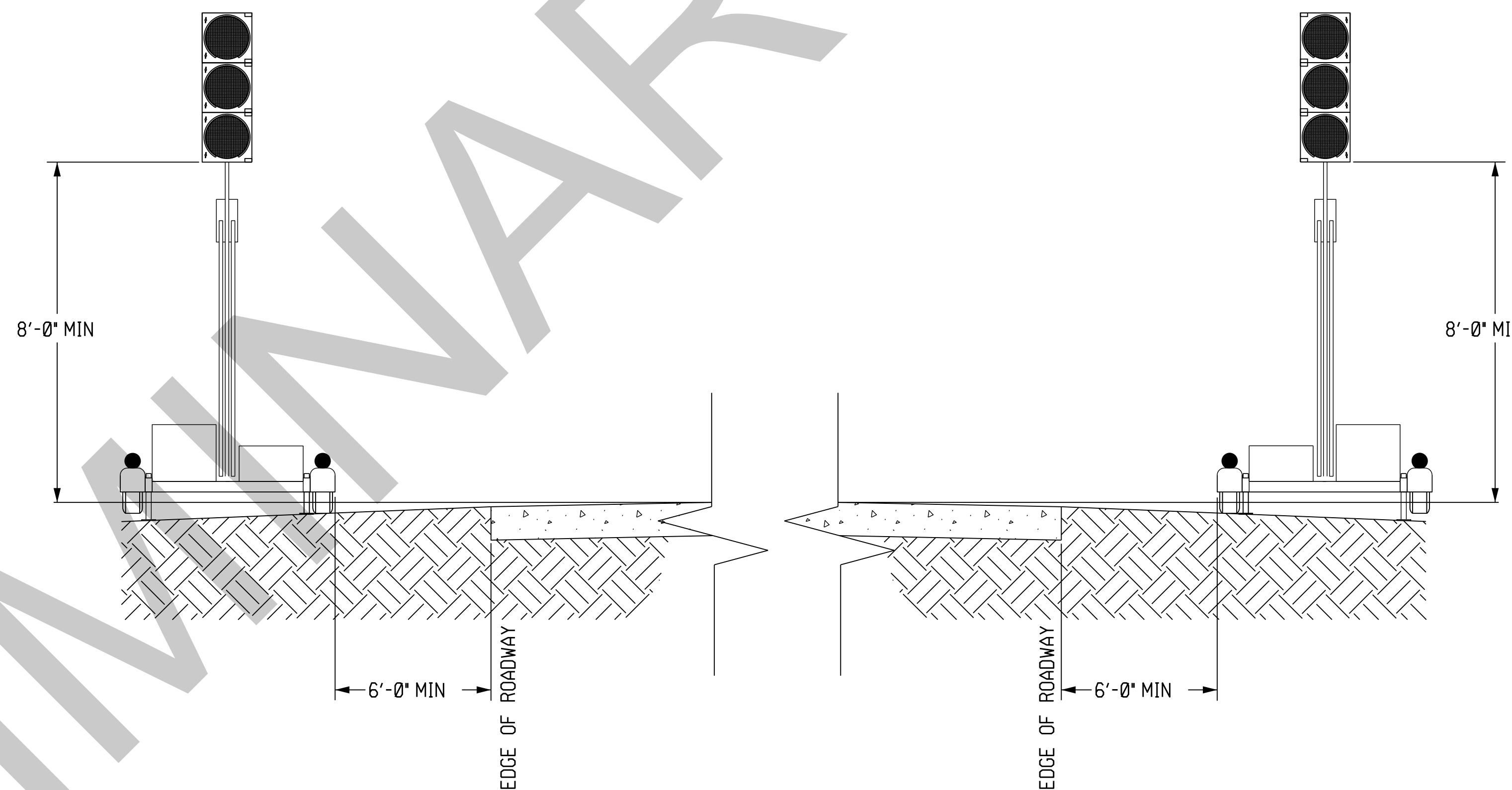
ADDITIONAL SIGNS
USE WHERE APPLICABLE

PORTABLE SIGNAL TRAILER (MULTI SIGNAL UNIT) INSTALLATION



WHEN SIGNALS ARE NEEDED ON BOTH SIDES OF ROAD, INTERCONNECT SHALL BE ACCOMPLISHED IN SAME MANNER AS PEDESTAL POLE INSTALLATION

PORTABLE SIGNAL TRAILER (SINGLE SIGNAL UNIT) INSTALLATION

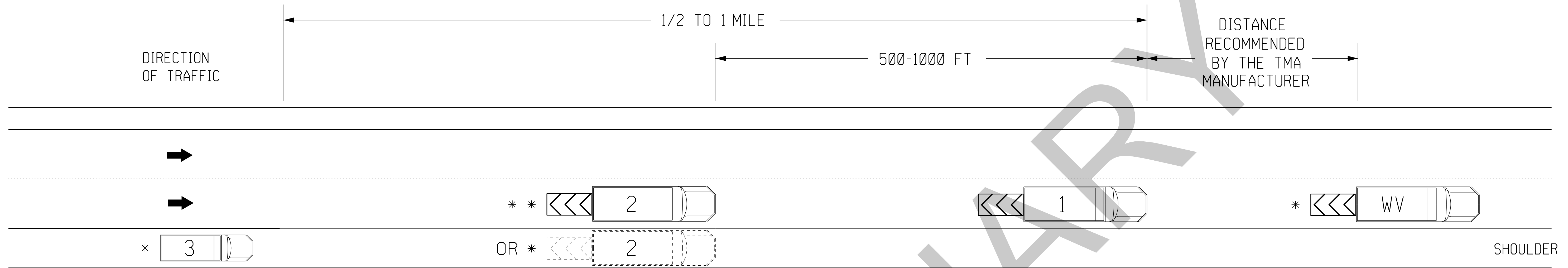


NOTES

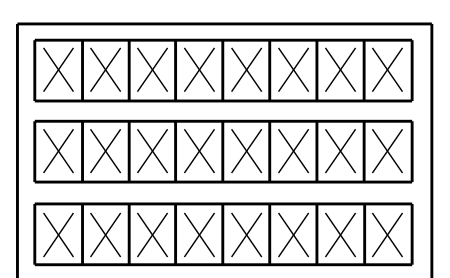
1. THE CONTRACTOR SHALL FURNISH ALL EQUIPMENT AND MATERIAL REQUIRED FOR THE TEMPORARY SIGNAL UNLESS OTHERWISE SPECIFIED. ALL EQUIPMENT AND MATERIAL FURNISHED BY THE CONTRACTOR SHALL REMAIN HIS PROPERTY.
2. ANY STATE SUPPLIED EQUIPMENT OR MATERIAL SHALL REMAIN THE PROPERTY OF THE STATE OF NEBRASKA.
3. THE SIGNAL HEAD LENSES SHALL BE A MINIMUM OF 12 INCHES IN DIAMETER.
4. ALL SIGNAL LAMPS SHALL BE EXTENDED ANGLE LED.
5. MAINTENANCE OF THE TEMPORARY SIGNAL SHALL BE THE FULL RESPONSIBILITY OF THE CONTRACTOR.
6. TRAFFIC SIGNALS POWERED BY MEANS OF A PORTABLE GENERATOR SHALL HAVE STANDBY BATTERY POWER CAPABLE OF OPERATING THE SIGNAL NOT LESS THAN 26 HOURS. PORTABLE TRAFFIC SIGNALS SHALL BE CHECKED EVERY 24 HOURS TO INSURE PROPER OPERATION.
7. THE SIGNAL SHALL BE PLACED INTO FLASHING AMBER OPERATION FOR BOTH DIRECTIONS DURING PERIODS WHEN THE LANE ARE OPEN TO TWO-WAY TRAFFIC. UNDER NO CIRCUMSTANCES SHALL AN INOPERATIVE TRAFFIC SIGNAL BE LEFT UNCOVERED ON AN OPEN ROAD TO THE PUBLIC.
8. INSTALLATION OF THE TEMPORARY SIGNAL SHALL BE IN COMPLIANCE WITH THE NATIONAL ELECTRIC CODE (LATEST EDITION), THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, ALL LOCAL ORDINANCES AND REGULATIONS, THE SPECIFICATIONS AND THE PROJECT PLANS.
9. SEE SIGNING STANDARD FOR REQUIRED SIGNING TO ACCOMPANY SIGNAL.
10. WHEN REQUIRED, THE CONTRACTOR SHALL FURNISH ALL EQUIPMENT AND MATERIAL REQUIRED FOR VEHICLE DETECTION ON ALL APPROACHES.

MOBILE OPERATIONS ON RURAL MULTI-LANE ROADWAY

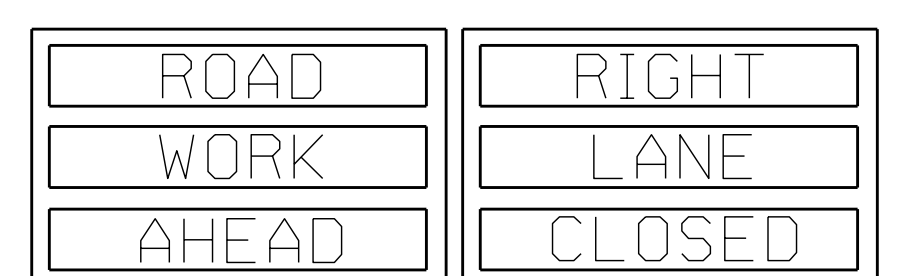
M8
Project Number
41-5(108)
C.N. 42829



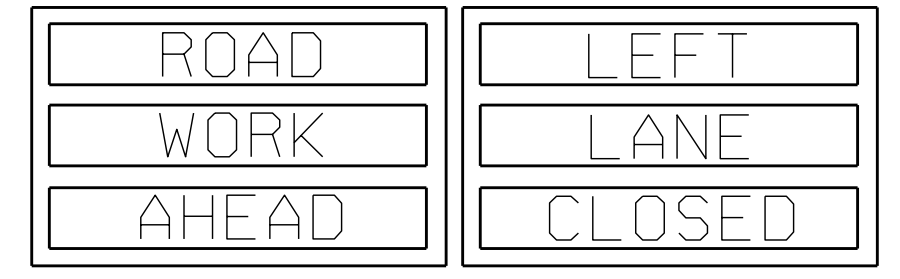
SHADOW VEHICLE 3
& PORTABLE DYNAMIC MESSAGE SIGN
WITH 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT
* TMA OPTIONAL



PORTABLE DYNAMIC MESSAGE SIGN

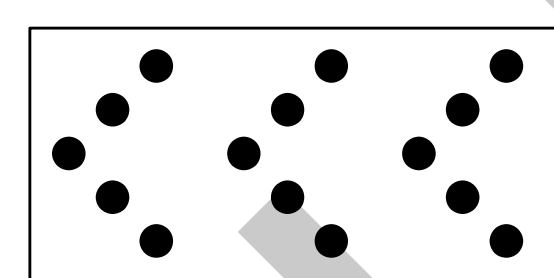


-OR-



TWO MESSAGES (1.6 - 2.0 SECONDS
PER MESSAGE)

OPTIONAL SHADOW VEHICLE 2
& FLASHING ARROW PANEL ON REAR
WITH 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT
* TMA OPTIONAL IF COMPLETELY ON
SHOULDER AND NOT STRADDLING EDGE LINE.
* * TMA REQUIRED IF STRADDLING EDGE
LINE OR IN CLOSED LANE.

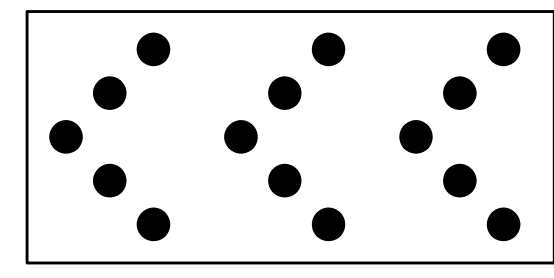


FLASHING ARROW PANEL ON REAR

TRUCK MOUNTED ATTENUATOR SYSTEM:

1. THE CONTACTOR SHALL FURNISH A FEDERALLY APPROVED TRUCK MOUNTED ATTENUATOR SYSTEM, MOUNTED ON A MINIMUM 16,000 POUND TRUCK. THE TRUCK SHALL BE EQUIPPED WITH 60" X 30" FLASHING ARROW PANEL, SECURLY MOUNTED ON THE TRUCK. THE TMA SYSTEM SHALL BE LOCATED IN THE FIELD AS REQUIRED BY THE MANUFACTURER. A COMPLETE SET OF REPLACEMENT MODULES SHALL BE AVAILABLE NEAR THE PROJECT SITE IN THE EVENT OF DAMAGE TO THE INSTALLED TMA. DAMAGED TMA'S SHALL BE REMOVED FROM THE ROADWAY AND PROJECT WORK STOPPED UNTIL REPAIRS TO THE UNIT HAVE BEEN COMPLETED.
2. THE TRUCK MOUNTED ATTENUATOR SHALL BE AN NCHRP 350 TEST LEVEL 3 (OR MASH EQUIVALENT) APPROVED TMA FOR 100 km PER HOUR (60 MPH).
3. THE TRUCK SHALL BE A 16,000 TO 35,000 POUND (GVW) VEHICLE AS REQUIRED BY THE TMA MANUFACTURER.
4. THE FLASHING ARROW PANEL SHALL BE SECURLY MOUNTED AS HIGH AS PRACTICABLE ON THE VEHICLE. THE ARROW PANEL SHALL NOT COME LOOSE UPON IMPACT TO THE TMA.

SHADOW VEHICLE 1 WITH TMA
& FLASHING ARROW PANEL ON REAR
WITH 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT



FLASHING ARROW PANEL ON REAR

NOTES:

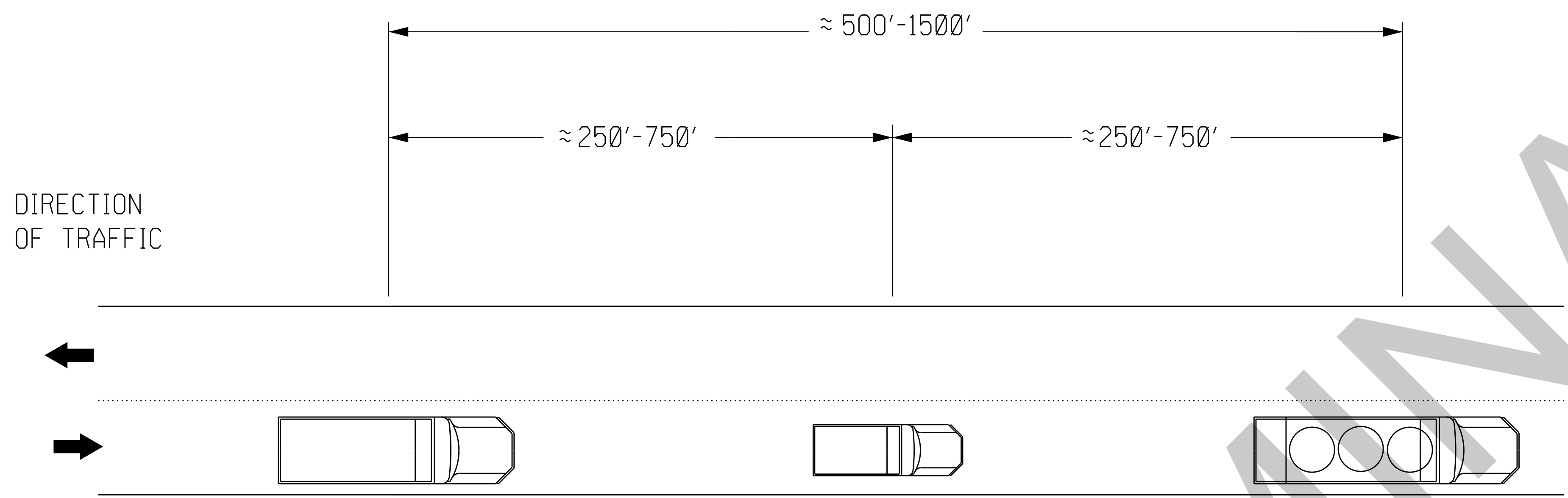
1. SHADOW VEHICLE 1 SHALL HAVE A TRUCK MOUNTED ATTENUATOR. SHADOW VEHICLE 2 IS OPTIONAL AND SHALL HAVE A TRUCK-MOUNTED ATTENUATOR IF IN THE CLOSED LANE OR STRADDLING THE EDGE LINE. SHADOW VEHICLE 3 MAY HAVE A TMA.
2. SHADOW VEHICLE 1 AND WHEN USED, SHADOW VEHICLE 2 SHALL HAVE A FLASHING ARROW PANEL.
3. SHADOW VEHICLE NUMBER 3 SHALL HAVE A PORTABLE DYNAMIC MESSAGE SIGN.
4. WHEN USED, SHADOW VEHICLE 2 SHALL BE PLACED IN ADVANCE OF HORIZONTAL OR VERTICAL CURVES TO PROVIDE ADVANCE WARNING FOR WORK OPERATIONS HIDDEN BY CURVES.
5. FOR LEFT LANE CLOSURES, SHADOW VEHICLE 3 WILL REMAIN ON RIGHT SHOULDER WHEN AN 8' OR WIDER PAVED INSIDE (LEFT) SHOULDER DOES NOT EXIST.
6. IN URBAN AREAS THE DISTANCE MAINTAINED BETWEEN VEHICLES MAY BE DECREASED AS NEEDED.
7. VEHICLE HAZARD LIGHT WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING OSCILLATING, OR STROBE LIGHTS.
8. DETAIL MAY BE USED FOR LEFT OR RIGHT LANE/SHOULDER WORK.

TRAFFIC CONTROL PLAN
 MOBILE OPERATIONS
 TWO-LANE AND MULTI-LANE
 NEBRASKA DEPARTMENT OF TRANSPORTATION - TRAFFIC ENGINEERING DIVISION
 DESIGNED BY NRL
 DATE 12/22

PLAN SHEET NUMBER
1 / 2

COMPUTER: BG0419M687
 DATE: 12-MAR-2024 15:04
 FILE: Mobile Operations.dgn

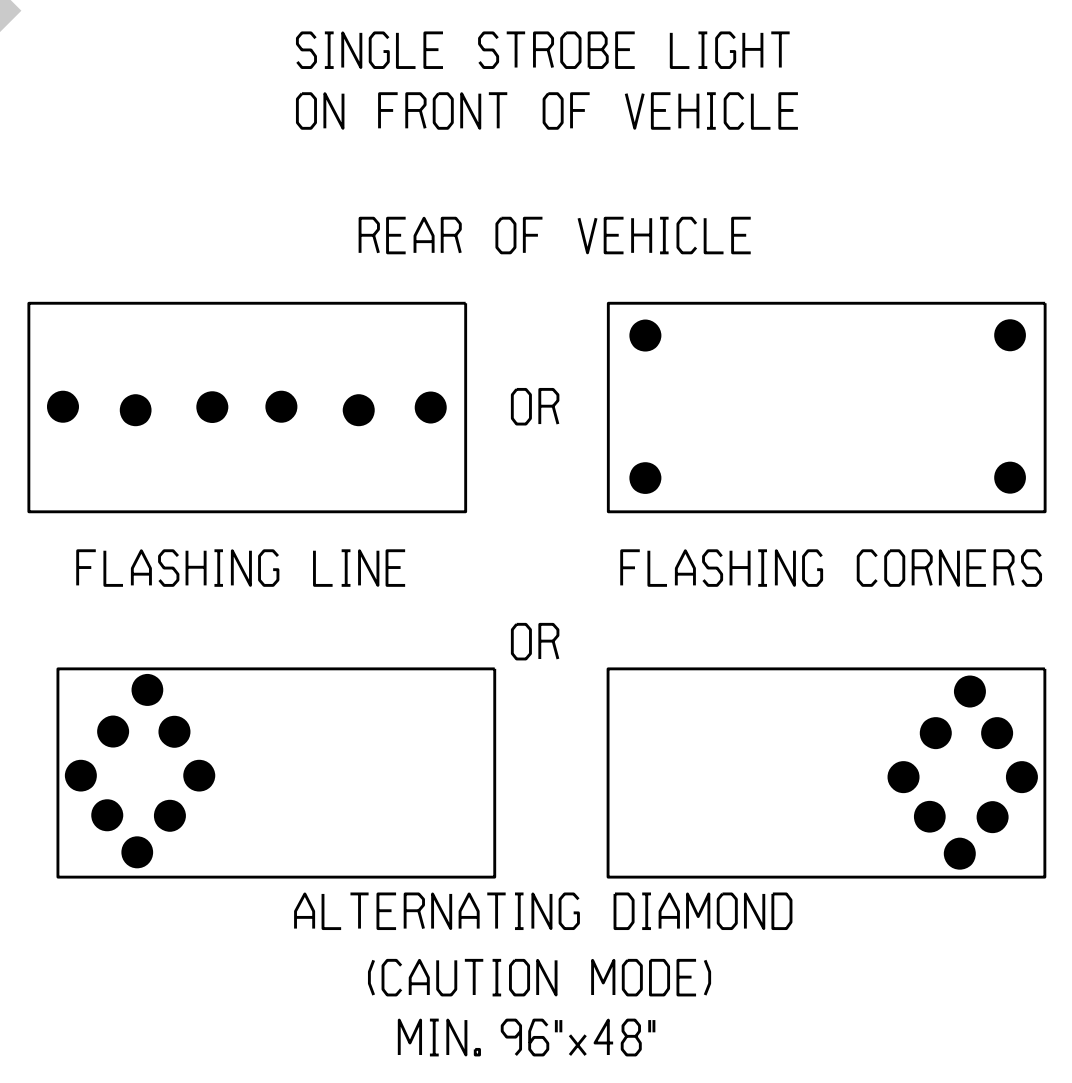
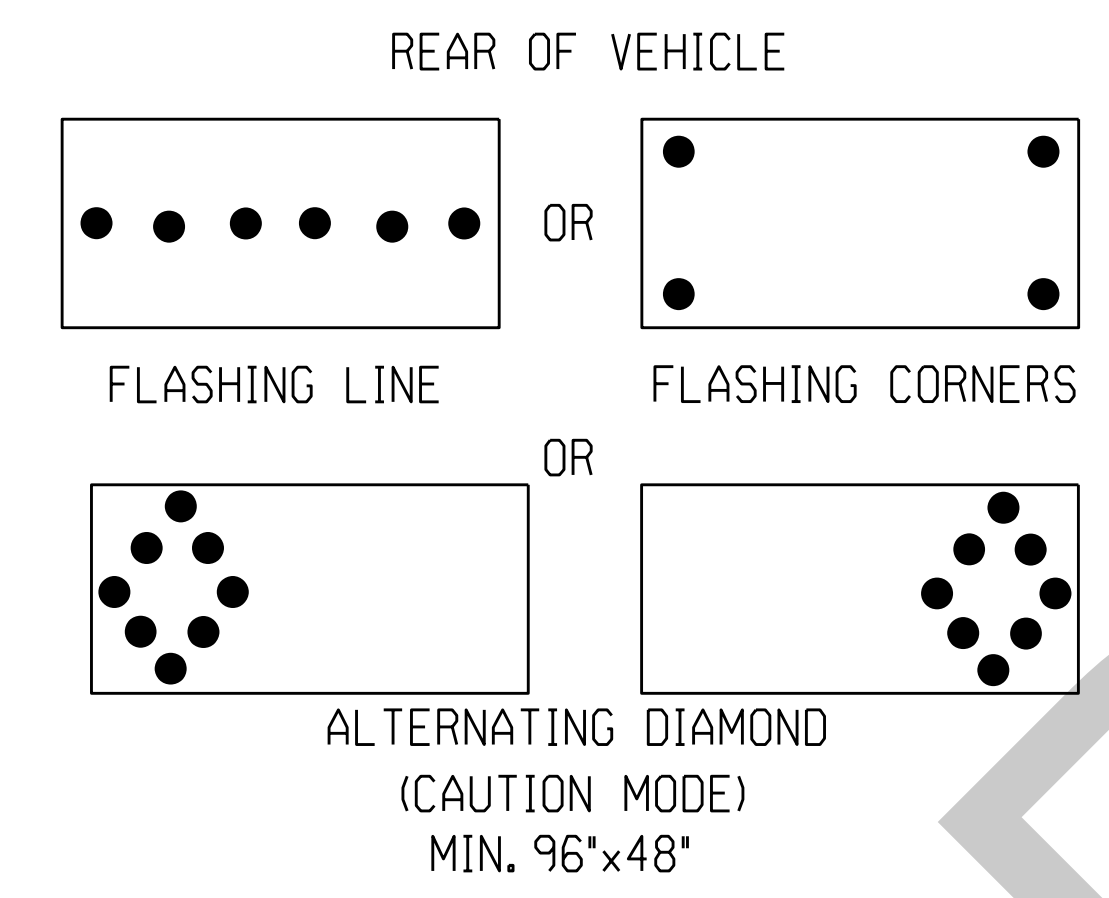
MOBILE OPERATIONS RURAL TWO-LANE TWO-WAY ROADS



REAR VEHICLE
WITH 2 HIGH INTENSITY
FLASHING LIGHTS
MOUNTED ON THE REAR
AND 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT
(TMA OPTIONAL)

OPTIONAL VEHICLE
WITH 2-360° BEACONS OR
APPROVED MINI-BAR LIGHT

WORK VEHICLE
WITH 4 HIGH INTENSITY
FLASHING LIGHTS MOUNTED
ON REAR AND 2-360°
BEACONS OR APPROVED
MINI-BAR LIGHT



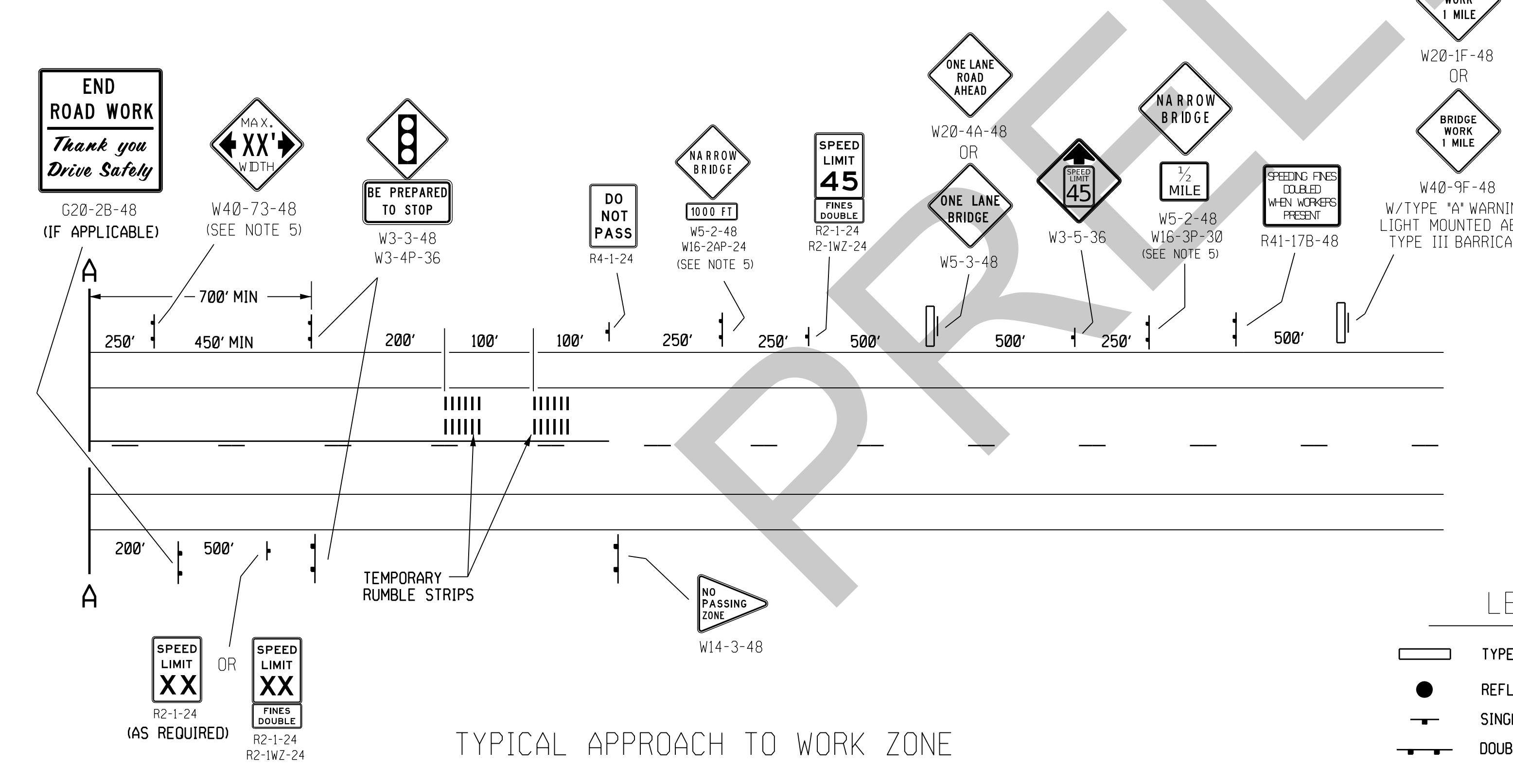
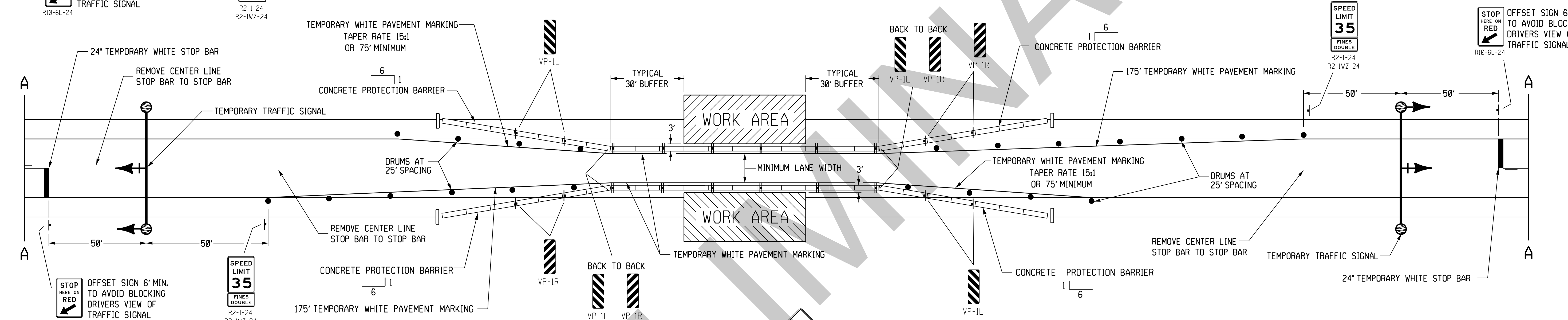
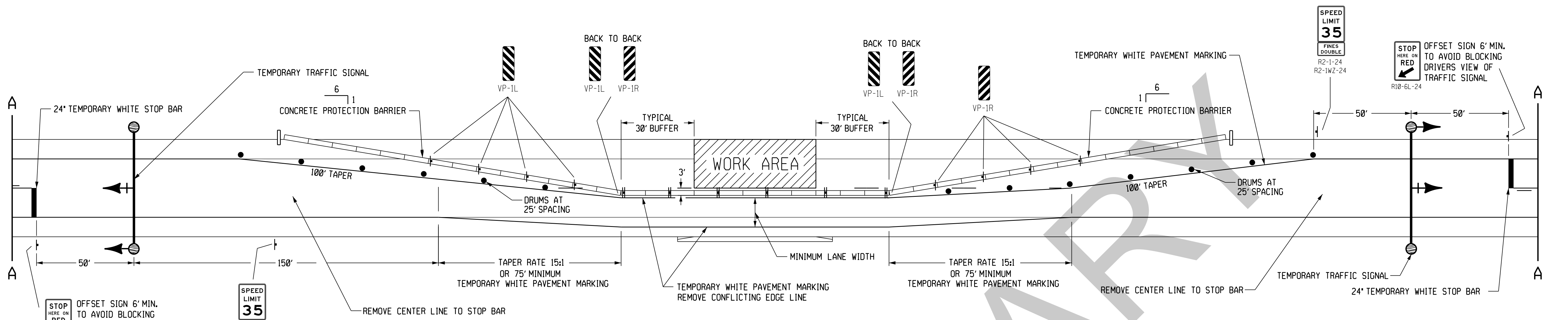
NOTES:

- CAUTION MODE ON WORK VEHICLE AND REAR VEHICLE SHALL BE ALTERNATING DIAMOND OR THE FLASHING 4 CORNER LIGHTS IF THE DIAMOND MODE IS NOT AVAILABLE.
- REAR VEHICLE SHALL BE PLACED IN ADVANCE OF HORIZONTAL OR VERTICAL CURVES TO PROVIDE ADVANCE WARNING FOR WORK OPERATIONS HIDDEN BY CURVES.

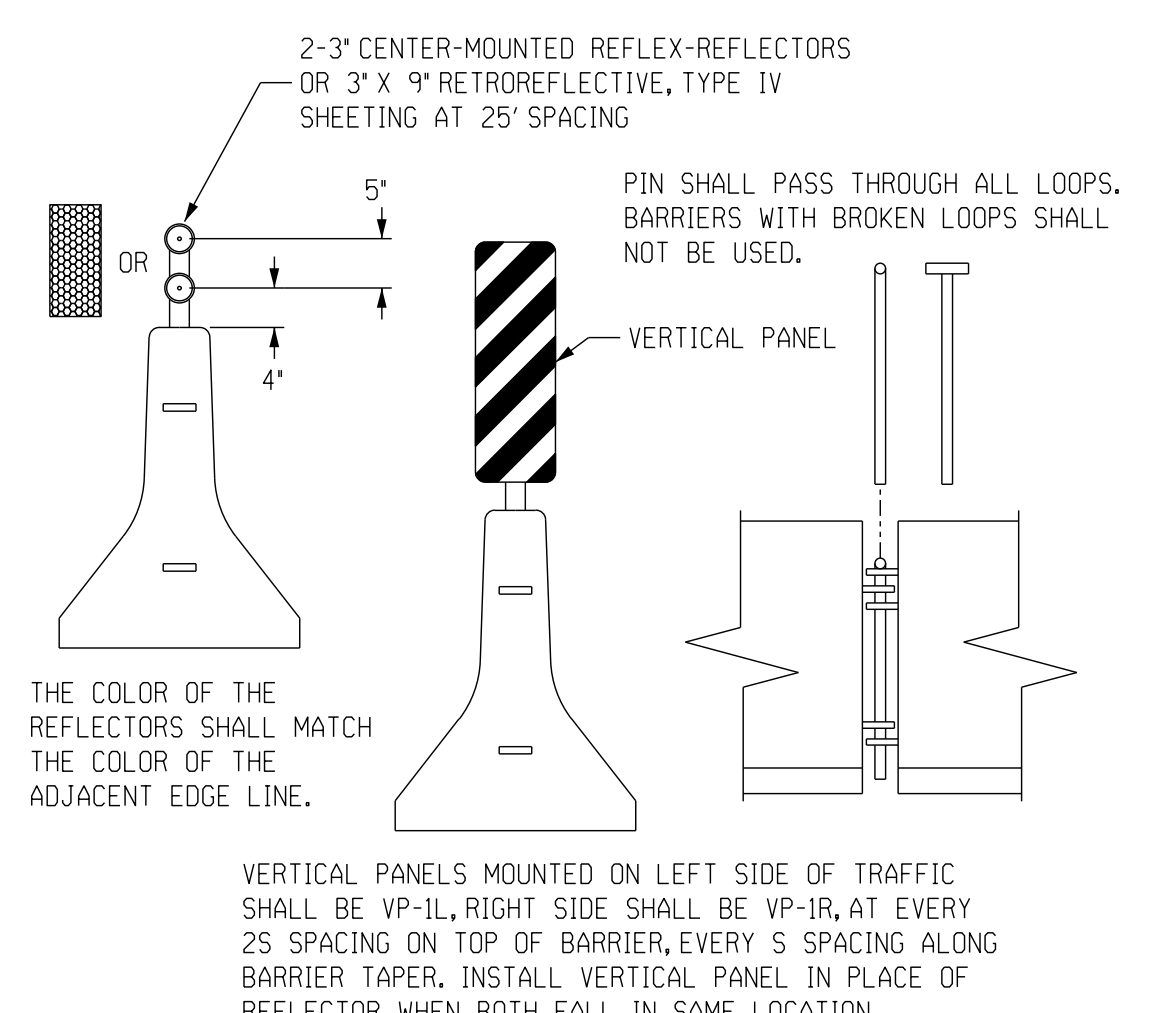
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TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
TWO-LANE AND MULTI-LANE

DESIGNED BY NRL
DATE 12/22
NEBRASKA DEPARTMENT OF TRANSPORTATION - TRAFFIC ENGINEERING DIVISION



CONCRETE PROTECTION BARRIER DETAIL



- NOTES
1. THE CONTRACTOR SHALL FURNISH REFLECTORS, VERTICAL PANELS AND A BRACKET TO SUPPORT THEM IN A STABLE POSITION ON THE CONCRETE BARRIERS. THE BRACKET SHALL BE CONSTRUCTED OF MATERIAL THAT MAY BEND, BUT NOT COME LOOSE IF STRUCK BY A PASSING VEHICLE.
 2. CONCRETE PROTECTION BARRIERS SHOULD EXTEND TO EDGE OF PAVEMENT. NO EQUIPMENT OR CONSTRUCTION MATERIAL IS TO BE PLACED IN FRONT OF THE PROTECTION BARRIERS AT ANY TIME. IF BARRIERS ARE REQUIRED TO BE MOVED FOR WORK ACCESS THEY SHALL BE REPOSITIONED BACK EACH NIGHT. AT NO TIME WILL A BLUNT END OF THE BARRIER BE ALLOWED IN THE TRAVEL LANE OF APPROACHING TRAFFIC.
 3. REFLECTORS USED FOR WORK ZONE TRAFFIC CONTROL SHALL BE SUBSIDIARY TO OTHER TRAFFIC CONTROL DEVICES.
 4. MINIMUM WIDTH OF TRAVELED LANE SHALL BE DETERMINED BY THE ENGINEER.
 5. INSTALL WHEN LANE WIDTH ACROSS IS LESS THAN APPROACH LANE WIDTH.
 6. SIGNS R41-17B-48, W20-1F-48 AND W40-9F-48 ARE NOT REQUIRED IF INSTALLED ON THE PROJECT IN ADVANCE OF THIS WORK SITE.

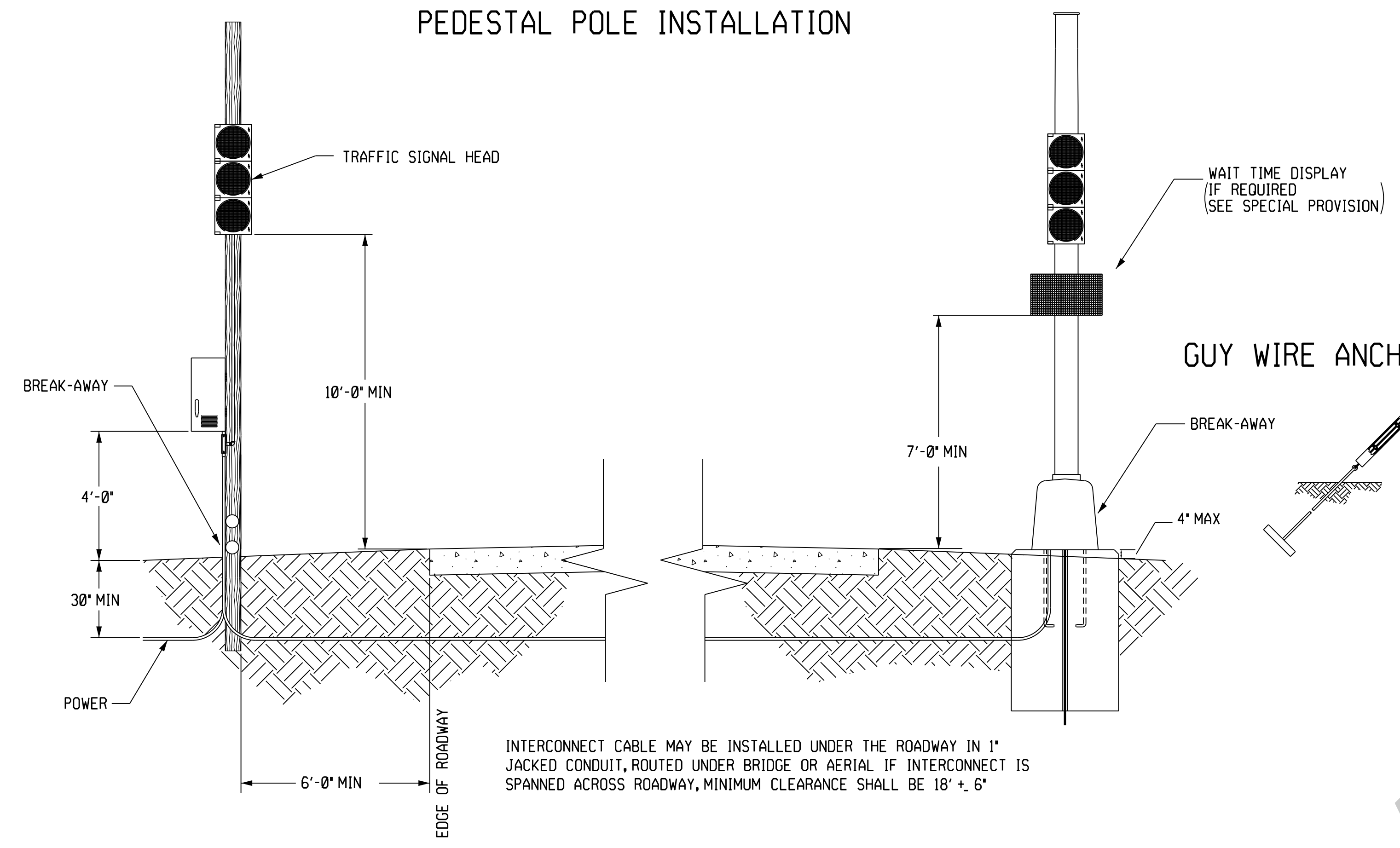
TYPICAL APPROACH TO WORK ZONE

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DATE: 12-MAR-2024 15:04

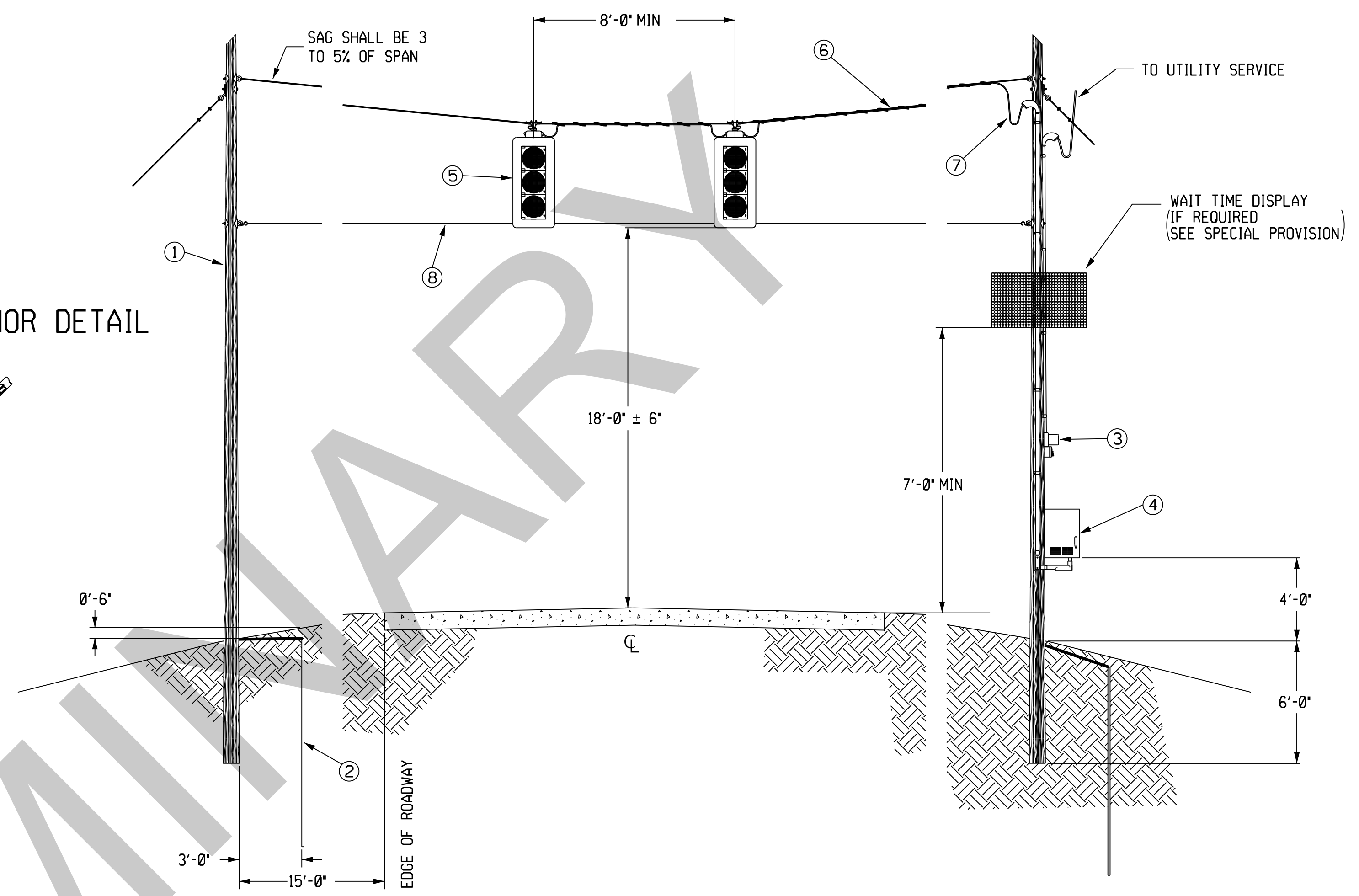
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PEDESTAL POLE INSTALLATION

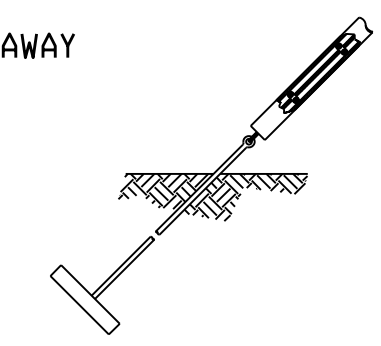


INTERCONNECT CABLE MAY BE INSTALLED UNDER THE ROADWAY IN 1" JACKED CONDUIT, ROUTED UNDER BRIDGE OR AERIAL IF INTERCONNECT IS SPANNED ACROSS ROADWAY, MINIMUM CLEARANCE SHALL BE 18' ± 6"

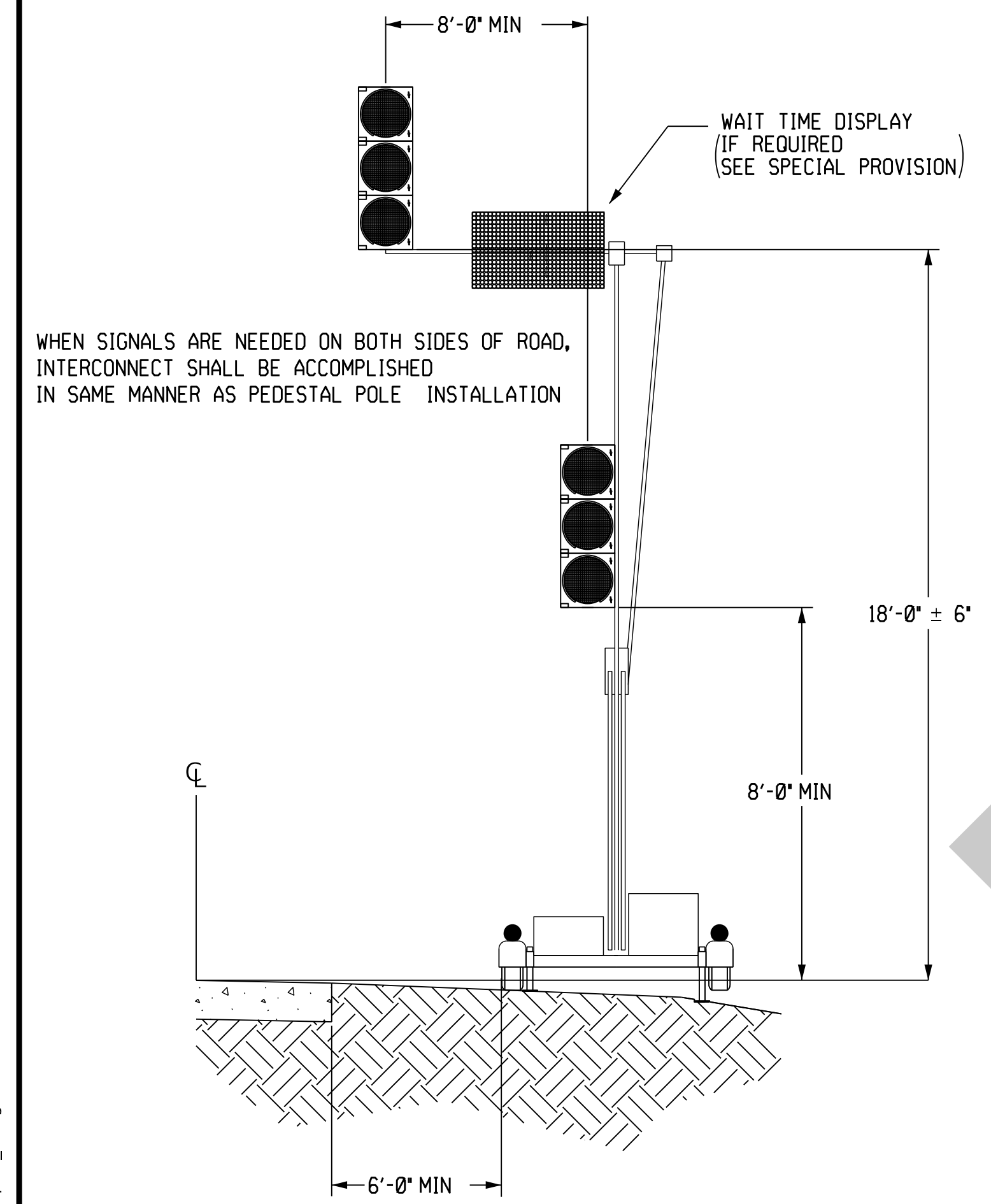
SPAN WIRE INSTALLATION



GUY WIRE ANCHOR DETAIL



PORTABLE SIGNAL



WHEN SIGNALS ARE NEEDED ON BOTH SIDES OF ROAD, INTERCONNECT SHALL BE ACCOMPLISHED IN SAME MANNER AS PEDESTAL POLE INSTALLATION

NOTES

1. THE LOCATIONS OF ALL AERIAL AND UNDERGROUND UTILITY FACILITIES MAY NOT BE INDICATED IN THESE PLANS, UNDERGROUND UTILITIES, WHETHER INDICATED OR NOT WILL BE LOCATED AND FLAGGED BY THE UTILITIES AT THE REQUEST OF THE CONTRACTOR. NO EXCAVATION WILL BE PERMITTED IN THE AREA OF UNDERGROUND UTILITY FACILITIES UNTIL ALL SUCH FACILITIES HAVE BEEN LOCATED AND IDENTIFIED TO THE SATISFACTION OF ALL PARTIES. THE EXCAVATION MUST BE ACCOMPLISHED WITH EXTREME CARE IN ORDER TO AVOID ANY POSSIBILITY OF DAMAGE TO THE UTILITY FACILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL AERIAL AND UNDERGROUND UTILITIES AND CONSTRUCTIONS.
2. THE CONTRACTOR SHALL FURNISH ALL EQUIPMENT AND MATERIAL REQUIRED FOR THE TEMPORARY SIGNAL UNLESS OTHERWISE SPECIFIED. ALL EQUIPMENT AND MATERIAL FURNISHED BY THE CONTRACTOR SHALL REMAIN HIS PROPERTY.
3. ANY STATE SUPPLIED EQUIPMENT OR MATERIAL SHALL REMAIN THE PROPERTY OF THE STATE OF NEBRASKA.
4. THE SIGNAL HEAD LENSES SHALL BE A MINIMUM OF 12 INCHES IN DIAMETER.
5. ALL SIGNAL LAMPS SHALL BE EXTENDED ANGLE LED.
6. MAINTENANCE OF THE TEMPORARY SIGNAL SHALL BE THE FULL RESPONSIBILITY OF THE CONTRACTOR.
7. TRAFFIC SIGNALS POWERED BY MEANS OF A PORTABLE GENERATOR SHALL HAVE STANDBY BATTERY POWER CAPABLE OF OPERATING THE SIGNAL NOT LESS THAN 26 HOURS. PORTABLE TRAFFIC SIGNALS SHALL BE CHECKED EVERY 24 HOURS TO INSURE PROPER OPERATION.
8. SIGNAL POLE LOCATIONS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER UNLESS THE EXACT PLACEMENT IS NOTED IN THE PLANS.
9. THE TIMING OF THE SIGNAL CYCLE SHALL BE DETERMINED BY THE NDOT TRAFFIC ENGINEERING DIVISION. FOR THE SPECIFIC INSTALLATION, CALL TRAFFIC ENGINEERING DIVISION AT 402-479-4594. HAVE THE FOLLOWING INFORMATION READY WHEN YOU CALL. PROJECT NAME, CONTROL NUMBER, DISTANCE BETWEEN STOP BARS, NUMBER OF SIDE STREET SIGNALS AND ADT FOR HIGHWAYS. TIMING SHOULD BE REQUESTED ONE WEEK PRIOR TO INSTALLATION TO AVOID UNEXPECTED PROJECT DELAYS.
10. THE SIGNAL SHALL BE PLACED INTO FLASHING AMBER OPERATION FOR BOTH DIRECTIONS DURING PERIODS WHEN THE BRIDGE IS OPEN TO TWO-WAY TRAFFIC. UNDER NO CIRCUMSTANCES SHALL AN INOPERATIVE TRAFFIC SIGNAL BE LEFT UNCOVERED ON AN OPEN ROAD TO THE PUBLIC.

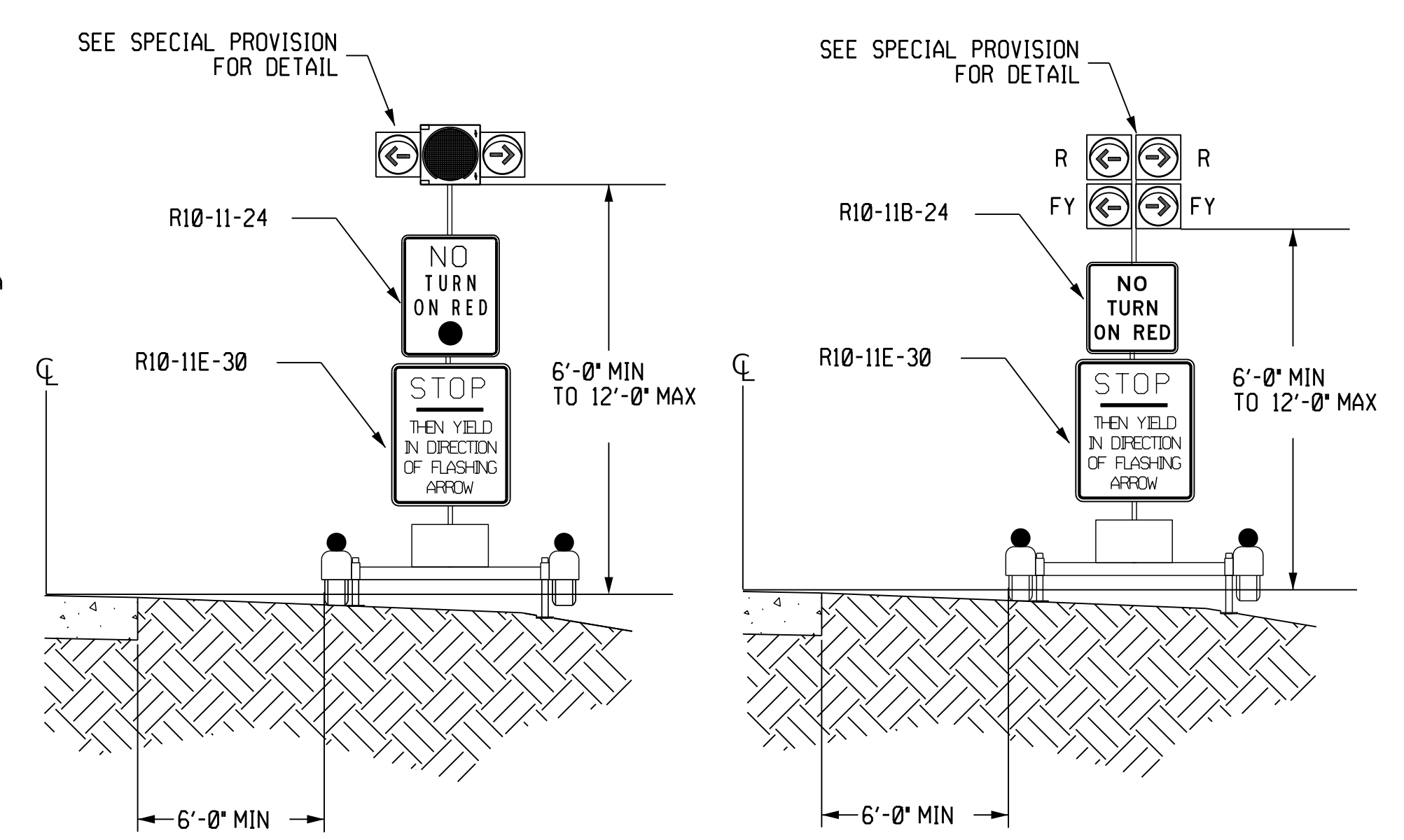
ITEM NO.	DESCRIPTION
1	CLASS IV BUTT TREATED WOOD POLE W/DOWN GUYS
2	5/8" X 10' COPPERWELDED GROUND ROD
3	METER SOCKET (IF REQ'D BY UTILITY)
4	CONTROLLER CABINET
5	TRAFFIC SIGNAL W/BACKPLATE
6	3/8" HIGH STRENGTH 7 STRAND WIRE ROPE
7	600 VOLT NO. 12 AWG 5/C TRAFFIC SIGNAL CABLE
8	1/4" SIEMANS MARTIN 7 STRAND WIRE ROPE

* CONTRACTOR SHALL FURNISH A SOLID STATE DIGITAL CONTROLLER WITH A PROGRAMMING MANUAL.

SIGNAL	APPROXIMATE LOCATION OF TEMP TRAFFIC SIGNAL
1	CULVERT @ STA 112+99 WITH CO. RD. P
2	CULVERT @ STA 429+01 WITH CO. RD. V
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	

11. INSTALLATION OF THE TEMPORARY SIGNAL SHALL BE IN COMPLIANCE WITH THE NATIONAL ELECTRIC CODE (LATEST EDITION), THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, ALL LOCAL ORDINANCES AND REGULATIONS, THE SPECIFICATIONS AND THE PROJECT PLANS.
12. THE TRAFFIC SIGNAL SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE ITEM 'TEMPORARY TRAFFIC SIGNAL' OR 'TEMPORARY TRAFFIC SIGNAL WITH WAIT TIME DISPLAY'. THIS PRICE SHALL BE FULL COMPENSATION FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE COMPLETE TEMPORARY SIGNAL.
13. SEE SIGNING STANDARD FOR REQUIRED SIGNING TO ACCOMPANY SIGNAL.
14. WHEN REQUIRED, THE CONTRACTOR SHALL FURNISH ALL EQUIPMENT AND MATERIAL REQUIRED FOR VEHICLE DETECTION ON ALL APPROACHES.

DRIVEWAY ASSISTANCE DEVICE

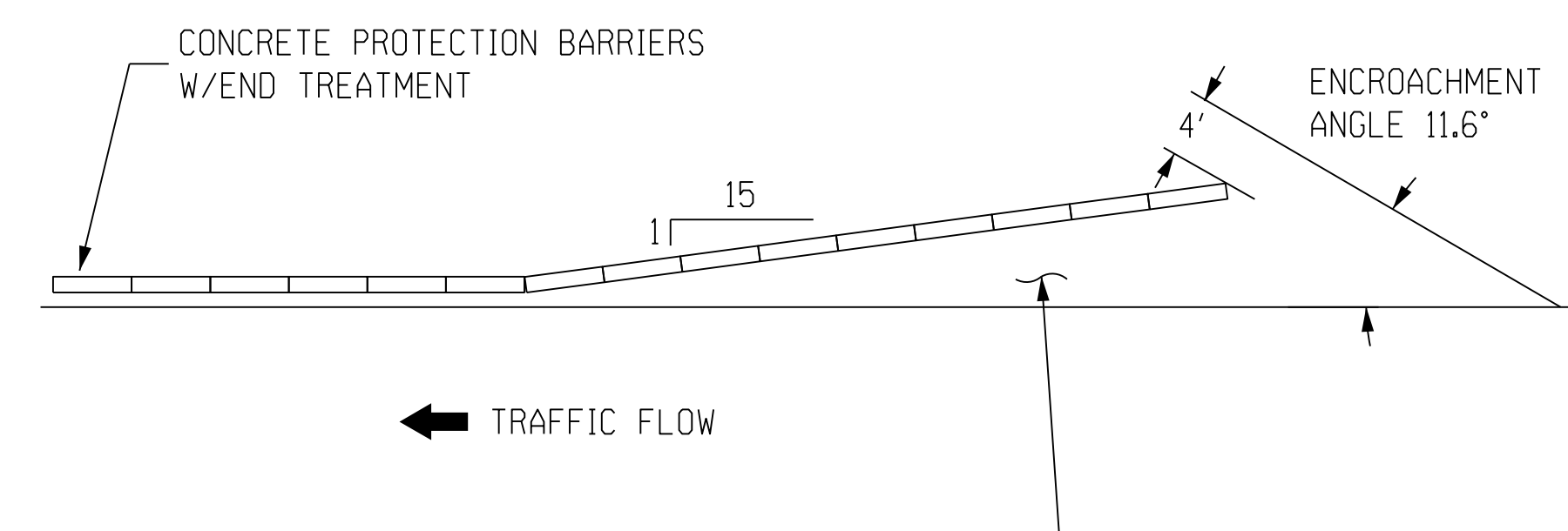


GENERAL NOTES

1. THE INERTIAL BARRIER SYSTEM SHALL BE NCHRP 350 OR MASH APPROVED.
2. THE INERTIAL BARRIER SYSTEM SHALL CONSIST OF THE UNITS AS SHOWN FOR THE SPECIFIED ROADWAY SPEED AND ALL HARDWARE AND ATTACHMENTS.
3. ALL MATERIAL FOR THE MODULES AND THE METHOD OF INSTALLATION SHALL CONFORM TO THE MANUFACTURER'S RECOMMENDATIONS.
4. THE BARRIER SYSTEM SHALL BE INSTALLED ON A FLAT, STABLE BASE WITH CROSS SLOPE NO STEEPER THAN 10:1.
5. NO PORTION OF THE SYSTEM SHALL ENCROACH INTO THE APPROACH TRAFFIC LANE.
6. THE MIXTURE FOR THE MODULES SHALL MEET THE REQUIREMENTS OF THE NDOT STANDARD SPECIFICATIONS.
7. A 6 INCH SPACING BETWEEN MODULES AND 12 INCH SPACING BETWEEN THE MODULES AND THE END OF CONCRETE BARRIER OR OTHER RIGID OBJECT SHALL BE PROVIDED.
8. THE NUMBER IN EACH MODULE REPRESENTS THE REQUIRED WEIGHT OF FILLER MATERIAL, SHOWN IN POUNDS.
9. WHERE SUFFICIENT SPACE IS AVAILABLE, THE INERTIAL BARRIER SYSTEM SHOULD BE ALIGNED AT AN ANGLE, NOT TO EXCEED 10°, IN THE DIRECTION OF APPROACH TRAFFIC.
10. A 13:1 BARRIER TAPER IS ALLOWED ON OMAHA URBAN INTERSTATE LOCATIONS.

GRADING FOR CONCRETE BARRIER PLACEMENT

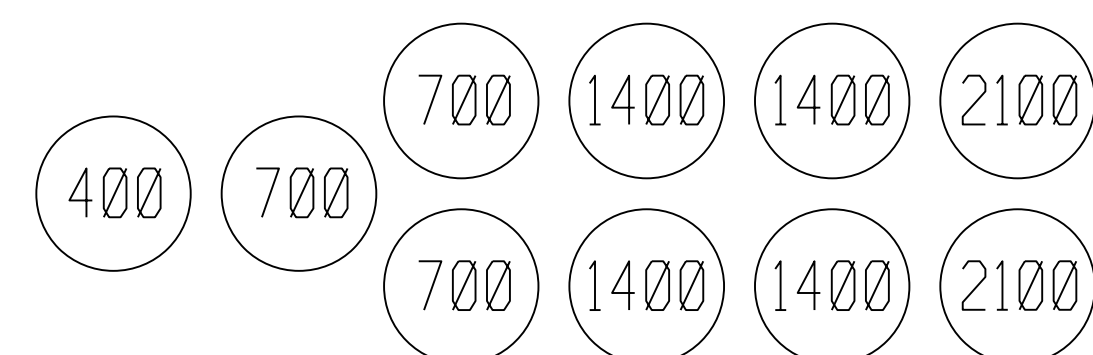
FOR BARRIERS PLACED IN THE MEDIAN IT MAY REQUIRE GRADING FROM BOTH DIRECTIONS



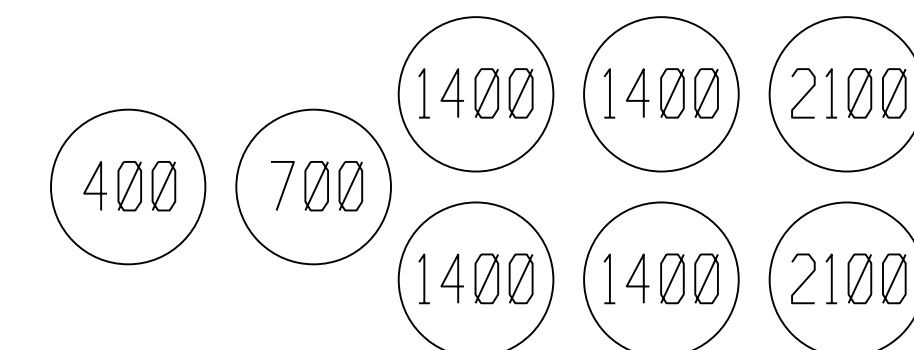
FOR CONCRETE BARRIERS PLACED AT 13:1 USE AN ENCROACHMENT ANGLE OF 13° (SEE NOTE 10)

AREA TO BE GRADED AT A SLOPE OF 10:1 OR FLATTER

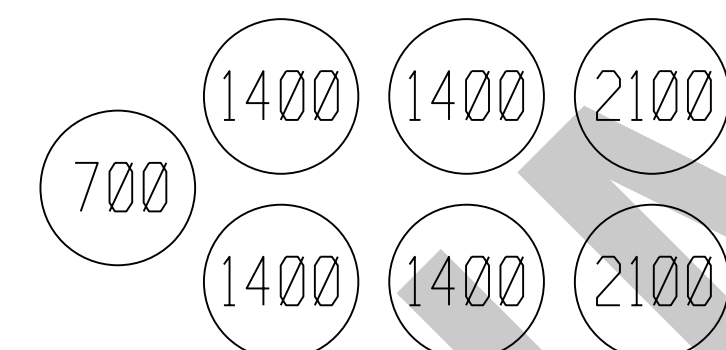
45 mph
APPROX. WT. 12,300 LBS
LENGTH 20.5 FT.



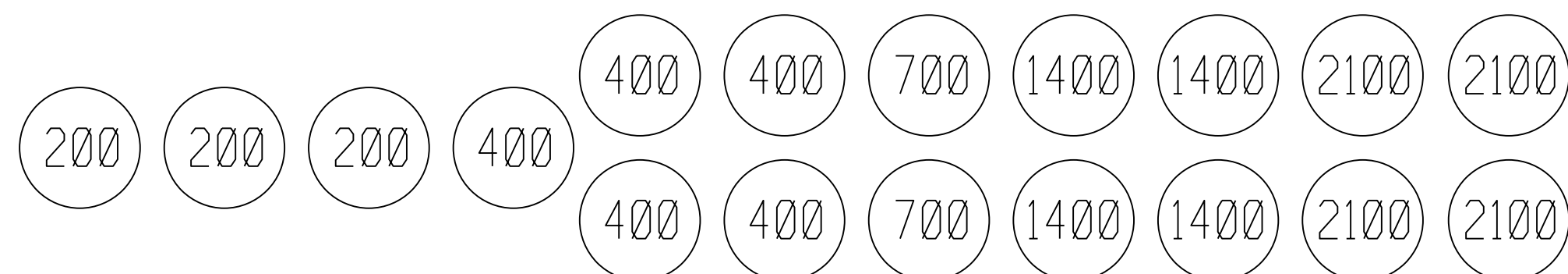
40 mph
APPROX. WT. 10,900 LBS
LENGTH 17 FT.



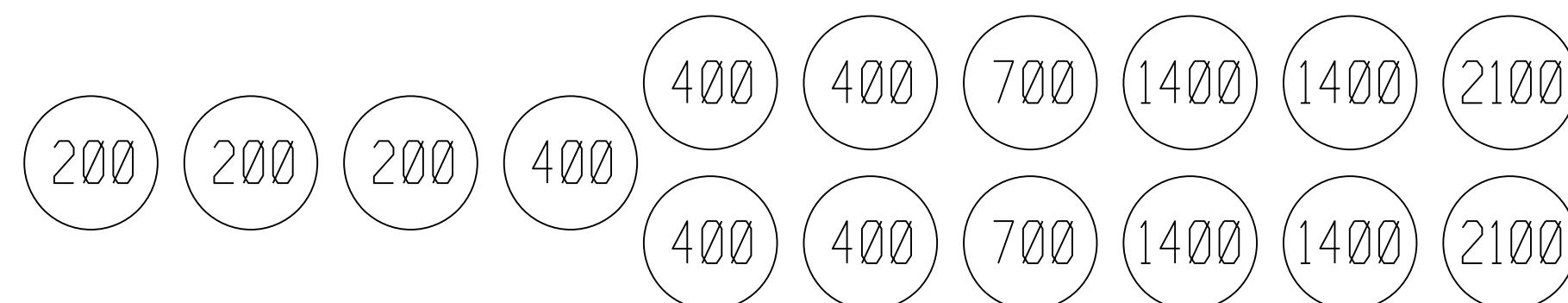
35 mph
APPROX. WT. 10,500 LBS
LENGTH 13.5 FT.



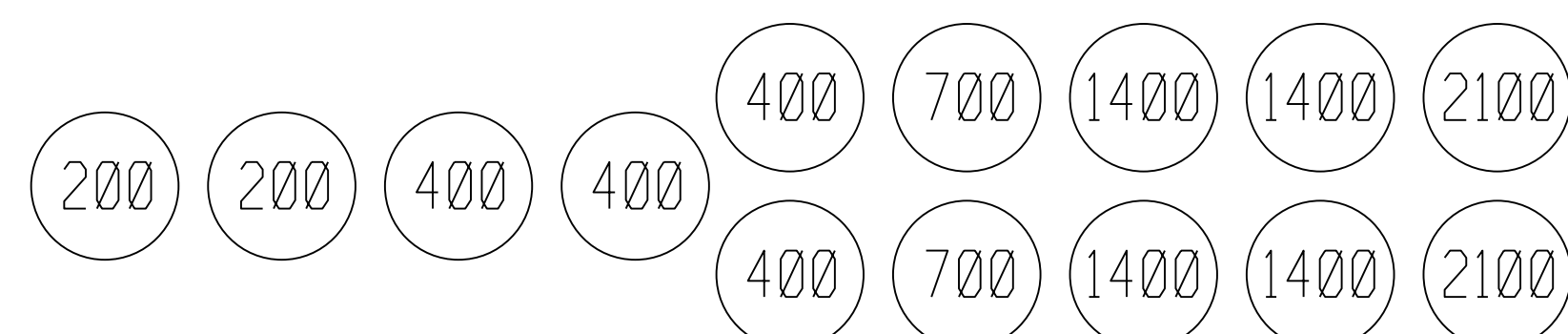
70 and 75 mph
APPROX. WT. 18,000 LBS, LENGTH 38 FT.



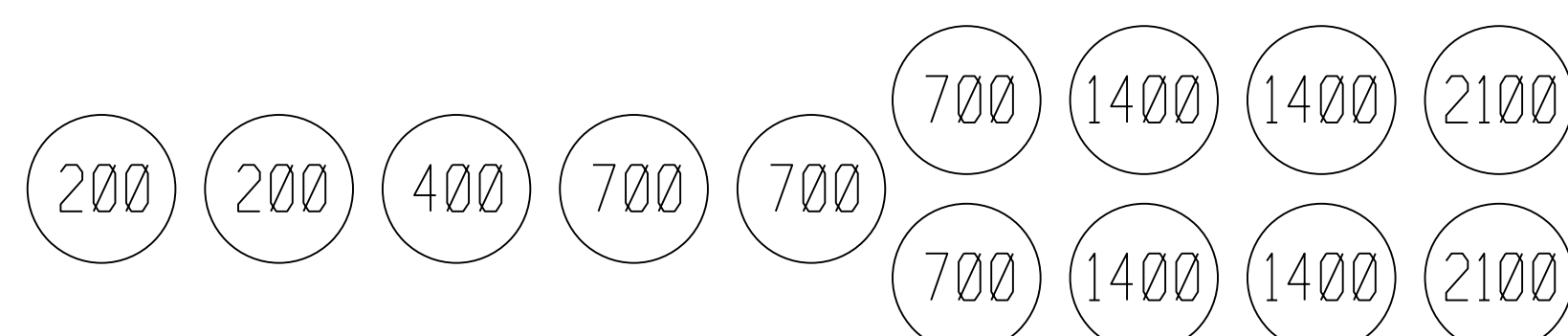
65 mph
APPROX. WT. 13,800 LBS, LENGTH 34.5 FT.



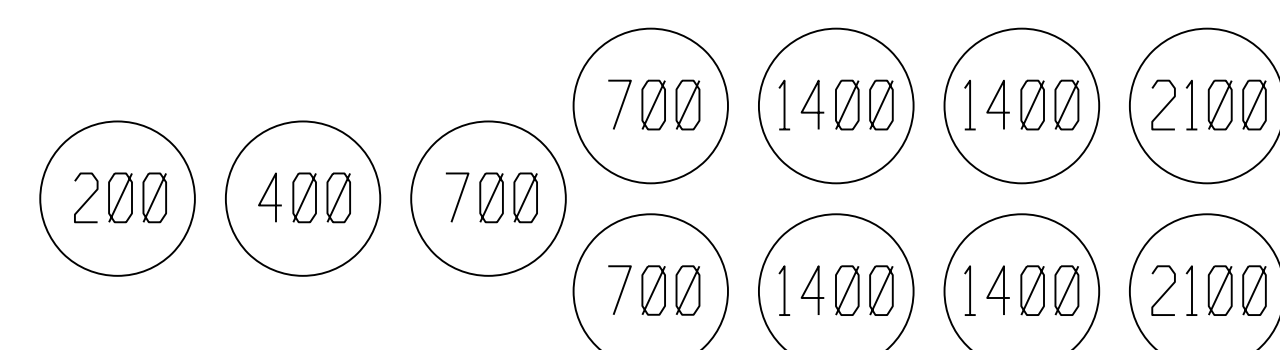
60 mph
APPROX. WT. 13,200 LBS, LENGTH 31 FT.



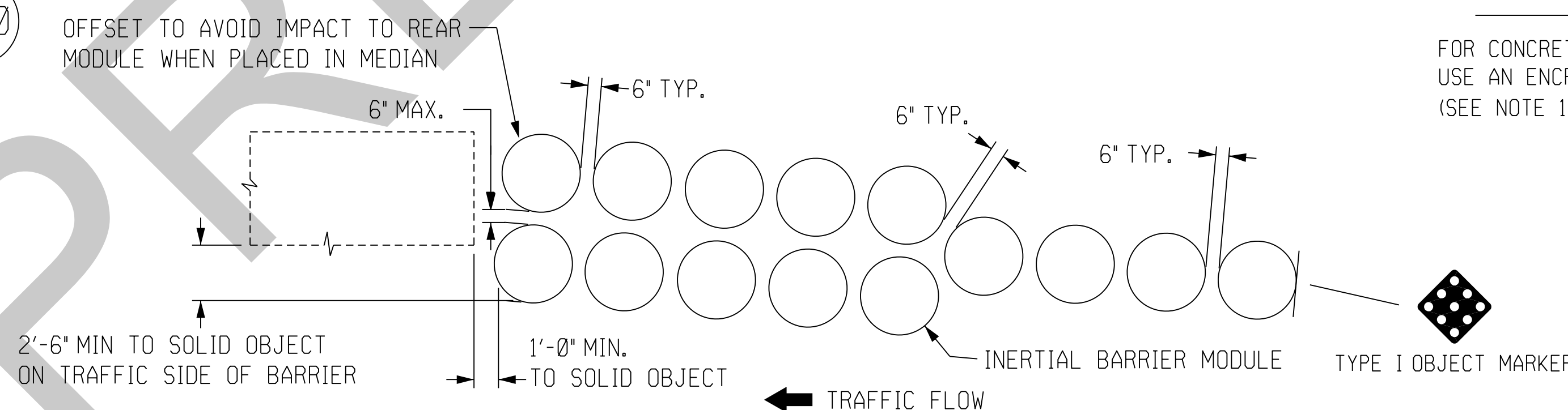
55 mph
APPROX. WT. 13,400 LBS, LENGTH 31 FT.



50 mph
APPROX. WT. 12,500 LBS, LENGTH 24 FT.



TYPICAL INERTIAL BARRIER INSTALLATION



THE TYPE I OBJECT MARKER SHALL CONSIST OF 9 YELLOW PRISMATIC REFLECTORS, EACH WITH A MINIMUM DIMENSION OF 3 INCHES, MOUNTED SYMMETRICALLY ON AN 18" YELLOW OR BLACK DIAMOND PANEL; OR AN 18" ALL YELLOW DIAMOND PANEL OF ASTM TYPE IV REFLECTIVE SHEETING. PLACE ON THE FIRST MODULE WITHIN 4" OF TOP.

TYPICAL TRAFFIC CONTROL PLAN
INERTIAL BARRIER SYSTEM

DESIGNED BY NRL
DATE 05/22
NEBRASKA DEPARTMENT OF TRANSPORTATION - TRAFFIC ENGINEERING DIVISION

----- Cut -----					----- Fill -----				Mass Ordinate
Station	Factor	Area (SF)	Volume (CY)	Adjusted (CY)	Factor	Area (SF)	Volume (CY)	Adjusted (CY)	
27+43.00	1	2.913	0	0	1	0	0	0	0
28+22.00	1	1.457	6.393	6.393	1	1.291	1.889	1.889	4.504
28+47.00	1	1.479	1.359	1.359	1	1.662	1.367	1.367	4.496
28+94.00	1	0.851	2.028	2.028	1	0.179	1.602	1.602	4.922
	Factor		Volume	Adjusted	Factor		Volume	Adjusted	
Grand Total	1		10	10	1		5	5	

----- Cut -----					----- Fill -----				Mass Ordinate
Station	Factor	Area (SF)	Volume (CY)	Adjusted (CY)	Factor	Area (SF)	Volume (CY)	Adjusted (CY)	
236+90.00	1	0	0	0	1	0.041	0	0	0
237+69.00	1	0	0	0	1	0.549	0.864	0.864	4.087
237+94.00	1	0	0	0	1	0.861	0.653	0.653	4.685
238+41.00	1	0	0	0	1	0.226	0.946	0.946	5.456
	Factor		Volume	Adjusted	Factor		Volume	Adjusted	
Grand Total	1		0	0	1		3	3	

----- Cut -----					----- Fill -----				Mass Ordinate
Station	Factor	Area (SF)	Volume (CY)	Adjusted (CY)	Factor	Area (SF)	Volume (CY)	Adjusted (CY)	
254+76.00	1	0	0	0	1	0.123	0	0	0
255+24.00	1	0	0	0	1	0.186	0.275	0.275	2.845
255+68.00	1	0	0	0	1	0.314	0.407	0.407	5.009
255+80.00	1	0	0	0	1	0.213	0.117	0.117	5.61
256+27.00	1	0	0	0	1	0.208	0.366	0.366	7.845
	Factor		Volume	Adjusted	Factor		Volume	Adjusted	
Grand Total	1		0	0	1		1	1	

----- Cut -----					----- Fill -----				Mass Ordinate
Station	Factor	Area (SF)	Volume (CY)	Adjusted (CY)	Factor	Area (SF)	Volume (CY)	Adjusted (CY)	
575+52.00	1	0	0	0	1	0.143	0	0	0
576+30.00	1	0	0	0	1	0.131	0.396	0.396	6.06
576+55.00	1	0	0	0	1	0.424	0.257	0.257	8.185
577+02.00	1	0	0	0	1	0.448	0.759	0.759	10.033
	Factor		Volume	Adjusted	Factor		Volume	Adjusted	
Grand Total	1		0	0	1		1	1	

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