

Nebraska Infrastructure HUB







NDOT Infrastructure Hub Team



Jenna Habegger, PMP

Hwy Planning Manager | Local Assistance

-  402-479-3607
-  jenna.habegger@nebraska.gov
-  <https://dot.nebraska.gov>
-  1400 Nebraska Pkwy | Lincoln, NE 68509







NEBRASKA
Good Life. Great Opportunity.



Dina Harris

Hwy Programs Specialist | Local Assistance

-  402-479-4434
-  dina.harris@nebraska.gov
-  <https://dot.nebraska.gov>
-  1400 Nebraska Pkwy | Lincoln, NE 68509



NEBRASKA
Good Life. Great Opportunity.

What is the Nebraska Infrastructure Hub?

- Launched to empower local public agencies in competing for federal discretionary grants.
- The primary aim is to bridge the gap between local needs and federal funding by providing centralized resources, expertise, and support
- Through strategic guidance, training programs, and networking opportunities, the Hub will equip agencies with the necessary tools and knowledge to navigate the complex grant application process successfully.

NDOT Awarded Discretionary Funding

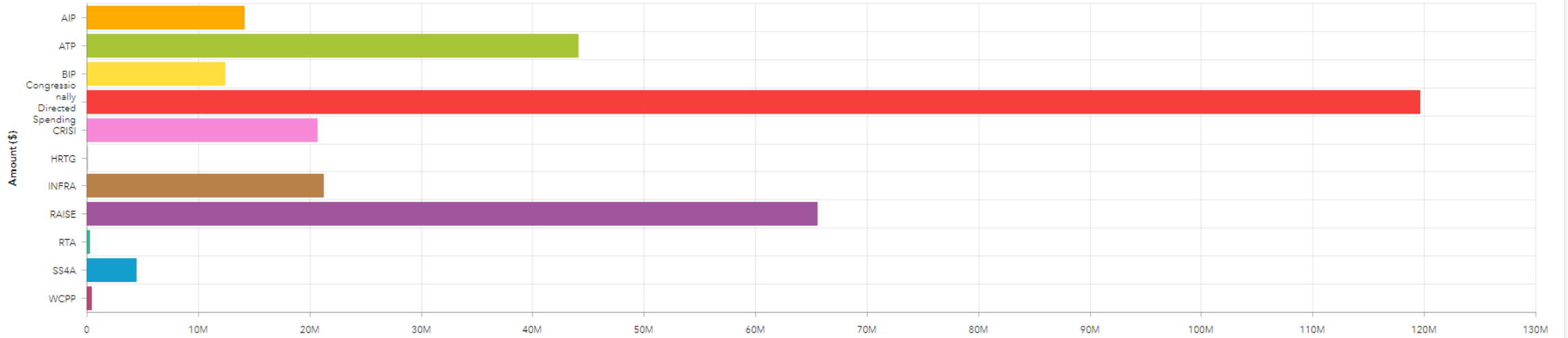
Select by Grant Type
None

Select by Year
None

Select by Congressional District
None

Select by NDOT District
None

Select an Awardee Category
None



NDOT Awarded Discretionary Funding

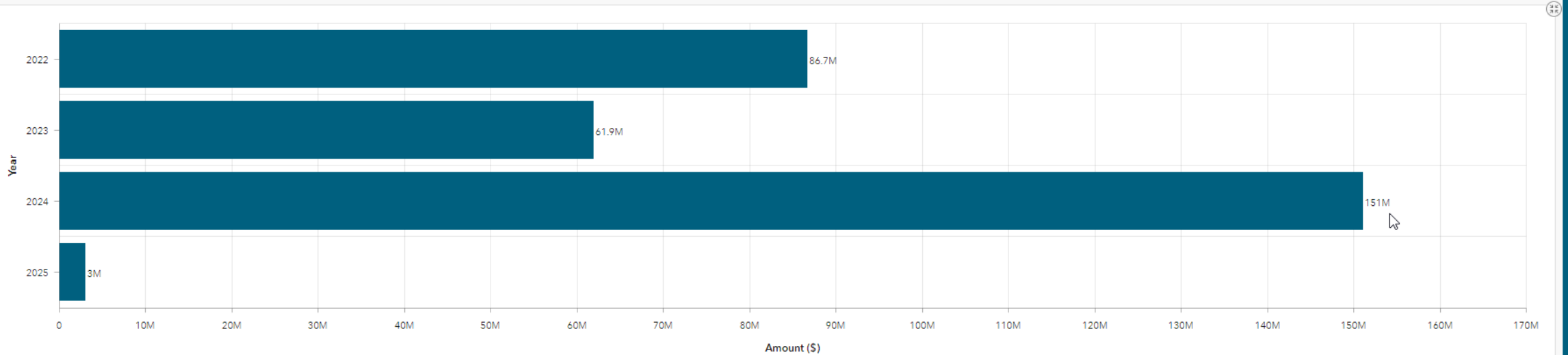
Select by Grant Type
None

Select by Year
None


Select by Congressional District
None

Select by NDOT District
None

Select an Awardee Category
None



Key Components

- **Grant Research & Database**
 - Grant Pairing Portal
 - **Technical Assistance**
 - On-Call Consultants to assist with grant writing (HDR & High Street)
 - \$2M to help write grants - 100% state funded, no local match
 - **Project Development Support**
 - 2 Full Time Employees dedicated to the Hub
 - **Partnering & Networking**
 - Using partnerships to strengthen grant applications
 - **Data & Analytics**
 - Providing support to enhance grant proposals
 - **Monitoring & Evaluation**
 - To ensure compliance and success, NDOT is available to help you manage your project once a grant is awarded
- 

Collaboration

- **Economic Development Districts**
 - **Local Public Agencies**
 - **NACO**
 - **LoNM**
 - **Railroads**
 - **Utilities**
- 

Nebraska Infrastructure Hub Website

- Grants 101
- Grant Pairing Portal
- Letters of Support
- <https://dot.nebraska.gov/business-center/nebraska-infrastructure-hub/>



GRANTS 101

If you are needing more information on grants
[click here](#)

Grants 101

What Are Discretionary Grants

A federal discretionary grant is a **competitive** grant process that the USDOT uses to help fund various state, local, and tribal projects. Applying for a discretionary grant requires a detailed proposal based on the grant's requirements, criteria and goals. Unlike other federal funds which are given to NDOT to distribute to Local Public Agencies, the USDOT reviews and then selects the most competitive applicant based on each grant's criteria.

Keep in mind, the discretionary grant opportunities discussed in this portal are **reimbursement** and often require a non-federal funding match. This means applicants must demonstrate their ability to raise the necessary funds and manage expenses, then at the end of the project, the grant funds will be given to reimburse the agreed upon costs. Moreover, depending on the grant, **the applicant may have to share part of the project cost**. Typically the grant will reimburse between 60%-80% of the project cost, with some grants providing a full 100% cost reimbursement. The amount covered by the applicant typically has to be non-federal funds.

Why Local Public Agencies Should Care

Federal discretionary grants offer significant opportunities for Local Public Agencies (LPAs) to secure funding for various transportation projects. These grants are designed to support projects that enhance infrastructure, improve safety, promote economic development, and advance sustainability. While federal discretionary grants offer significant funding opportunities, not all local projects are well-suited for this type of funding. Smaller local projects often face challenges in meeting stringent criteria and competitive nature of federal discretions grants.

NDOT's Role

NDOT can provide valuable assistance to LPA's throughout the discretionary grant process in several way through the Infrastructure Hub:

- Technical Assistance though all phases of the grant process
- Guidance on Eligibility and Requirements
- Capacity Building
- Review and Feedback
- Partnership Opportunities

What are the First Steps in Considering a Discretionary Grant?

If you are looking for more information about grants in general, the side panel of this page includes a curated list of links to help provide you with more information about discretionary grants.

If you are ready to review specific grants that you could consider to fund your project, the [Grant Pairing](#) page will match you to grant opportunities that your organization and project may be eligible for.

Another good step if you wish to apply for a grant is to locate or apply for a Unique Entity Identify on [SAM.gov](#). This ID is required because it helps the USDOT identify each applicant in the system. This is a good to do early because it could take multiple weeks to receive your Unique Entity Identify.

Finally, NDOT is also a great resource to help select a grant and develop an application. Contact information for the appropriate NDOT team member can be found on the side panel.

Project Types Suitable for Federal Discretionary Grants

Infrastructure Modernization and Resilience

- **Roadway Rehabilitation and Reconstruction:** Projects aimed at repairing and upgrading existing roadways to meet current standards and extend their lifespan.
- **Bridge Replacement and Rehabilitation:** Initiatives focused on replacing or rehabilitating aging bridges to ensure safety and reliability.
- **Resilient Infrastructure:** Projects that incorporate resilience measures to withstand natural disasters and climate change impacts.

Safety Improvements

- **Pedestrian and Bicycle Safety:** Enhancements such as crosswalks, bike lanes, pedestrian bridges, and traffic calming measures to protect non-motorized road users.
- **Traffic Signal Upgrades:** Installation of advanced traffic signal systems to improve traffic flow and reduce accidents.
- **Safety Enhancements at High-Risk Locations:** Targeted improvements at intersections and stretches of road with high accident rates.


Economic Development and Community Revitalization

- **Freight and Logistics Infrastructure:** Projects that enhance the efficiency of freight movement, including intermodal facilities and improved access to industrial areas.
- **Downtown Revitalization:** Transportation projects that support the revitalization of downtown areas and promote economic development.
- **Tourism and Recreation Access:** Infrastructure improvements that enhance access to tourist attractions and recreational areas.

Environmental Sustainability and Innovation

- **Green Infrastructure:** Incorporation of sustainable practices such as permeable pavements, green roofs, and urban forestry in transportation projects.
- **Innovative Transportation Technologies:** Projects that implement smart city technologies, such as intelligent transportation systems (ITS) and connected vehicle infrastructure.

Key Considerations

- **Alignment with Federal and State Priorities:** Ensure that the project aligns with federal and state transportation goals, including safety, sustainability, equity, and innovation.
 - **Demonstrated Need and Impact:** Clearly articulate the need for the project and its expected impact on the community, including benefits to safety, mobility, and economic development.
 - **Stakeholder Support:** Garner support from local stakeholders, including community members, businesses, and other governmental agencies.
 - **Technical Feasibility and Readiness:** Provide evidence of the project's technical feasibility, including engineering studies, design plans, and readiness for implementation.
 - **Matching Funds and Leveraged Resources:** Highlight any matching funds and leveraged resources that demonstrate the community's commitment to the project and enhance its competitiveness.
- 

Types of Smaller Local Projects Not Suited for Federal Discretionary Grants

The following projects are not suitable for discretionary grant funding:

Routing Maintenance and Minor Repairs

- Pothole repairs
- Minor resurfacing or repaving projects
- Routine bridge inspections and minor repairs

Projects with Limited Scope or Impact

- Small-scale projects that affect only a very localized area without broader regional or national benefits.
- Projects that do not significantly improve traffic flow, safety, or connectivity.

Projects Lacking Innovation or Modernization

- Traditional road widening projects that do not incorporate smart technology or multimodal transportation options.
- Bridge replacement projects that do not include enhancements for resilience or modern materials.

Non-Sustainable Projects

- Projects that do not incorporate environmentally sustainable practices or materials.
- Roadway expansions that encourage increased vehicle emissions without incorporating measures to mitigate environmental impact.

Projects Without Clear Safety Improvements

- Projects that do not address known safety issues or do not result in significant safety enhancements.
- Road or bridge projects that fail to meet modern safety standards.

Projects with Insufficient Planning or Community Support

- Projects that lack comprehensive planning or do not have a clear implementation strategy.
- Projects that face significant opposition from local communities or do not have adequate stakeholder engagement.

Projects with Poor Cost-Benefit Ratios

- Projects where the cost significantly outweighs the anticipated benefits in terms of traffic flow, safety, or economic impact.
- Projects that do not provide a clear return on investment or significant public benefits.

Examples of Unsuitable Projects

- **Local Road Resurfacing:** Simple resurfacing of a local road without any additional improvements for safety, technology integration, or environmental benefits.
- **Minor Bridge Repairs:** Small-scale bridge repair projects that do not address structural deficiencies or enhance resilience against natural disasters.
- **Narrow-Scope Sidewalk Improvements:** Sidewalk repair or replacement projects that do not improve connectivity or accessibility for pedestrians on a broader scale.
- **Limited-Impact Traffic Signal Upgrades:** Upgrading a few traffic signals in a small area without implementing an integrated, city-wide intelligent traffic management system.
- **Non-Innovative Road Expansions:** Traditional road widening projects that do not incorporate bike lanes, pedestrian paths, or green infrastructure elements.

Considerations for Alternative Funding Sources

Looking for other options or no federal grant opportunities returned? You may either adjust your criteria by clicking on "back to grant pairing" at the top left of the screen, start over with a new project by clicking on "Grant Pairing" on the top right of the screen, or consider the alternative funding sources below.

Local Taxes and Bonds

Utilize local tax revenue, special assessments, or municipal bonds to finance smaller projects.

State Grants and Programs

Explore state-level grant programs and funding opportunities tailored for smaller-scale projects.

- [Federal Fund Purchase Program \(FFPP\)](#)
 - [Highway Safety Improvement Program \(HSIP\)](#)
 - [Off-system Bridge Program \(BRO\)](#)
 - [Transportation Alternative Program \(TAP\)](#)
 - [County Bridge Match Program \(CBMP\)](#)
 - [State Rec Road \(SRR\)](#)
 - [Economic Opportunity Program \(EOP\)](#)
- 



GRANT PAIRING PORTAL

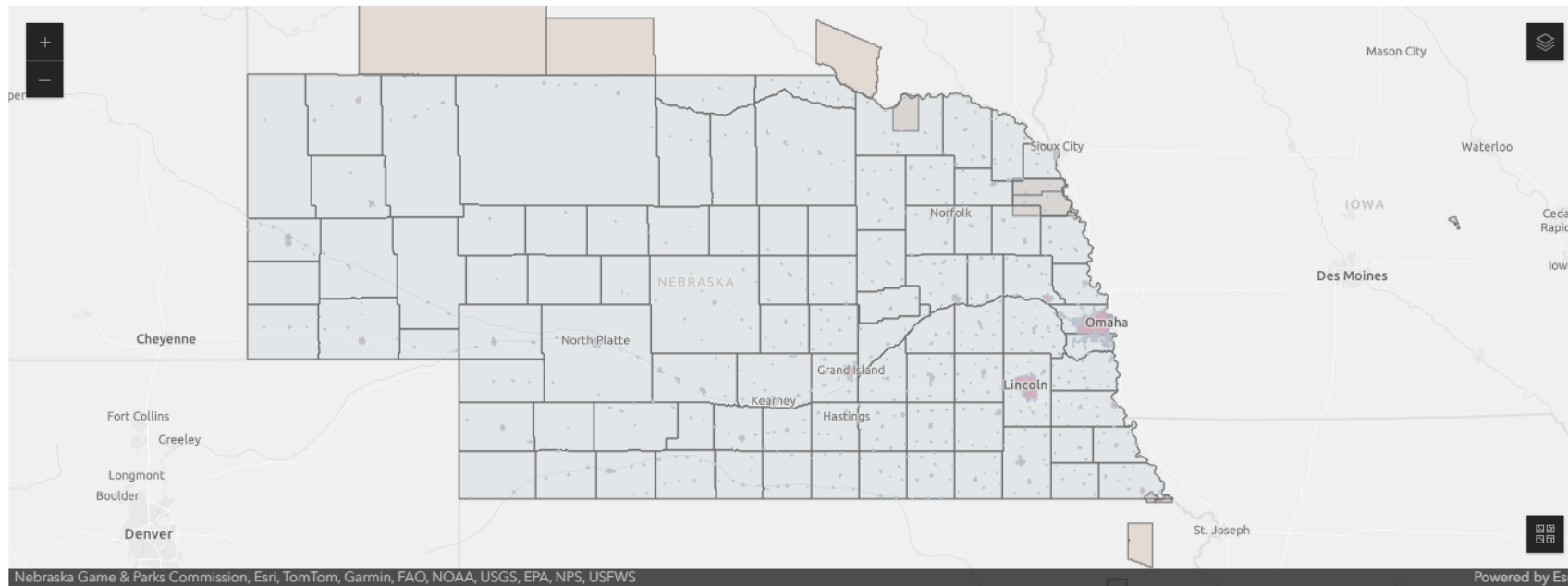
If you need to see what grants fit your project

[click here](#)



Select your Organization

Using your mouse, click and drag to move the map. Then, either scroll or use the "+ / -" buttons on the top left corner of the map to zoom to your organization's location. Once ready, click on the map to select where your organization is based. Select your organization and then click "Next."



Reset All

- 1 Agency Information
- 2 Project Location
- 3 Project Phase
- 4 Primary Components
- 5 Project Size

Select your organization to continue



Select your organization below by clicking on the appropriate button.

Garden (County)

Don't see your organization above? Select one of the following.

Select an entity



Select an entity

State

Local Government

Local Agency (transportation, economic development, etc)

Tribal Nation

Coalition of Agencies or Governments

Metropolitan Planning Organization

Nonprofit Organization

Project Location

Draw your project location if applicable

If you know your project location, select the "Draw Project Location" button below, then click on the map to start drawing. Continue clicking to add multiple points along your project location.

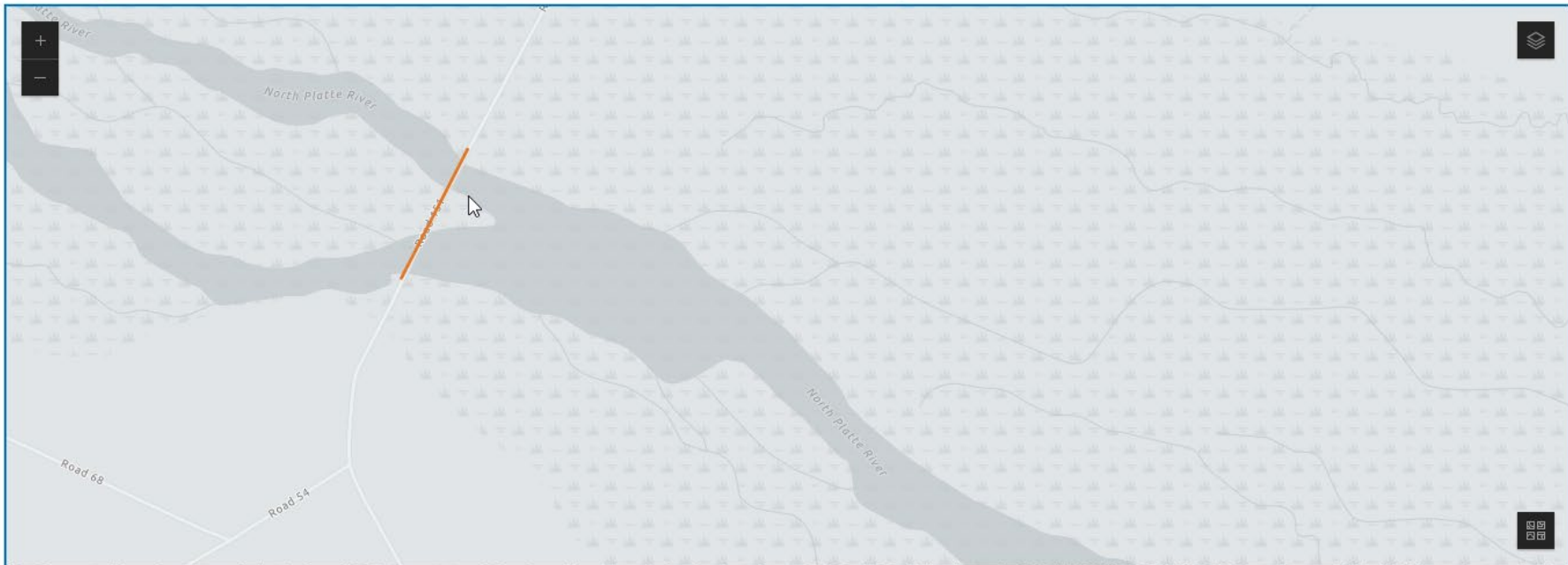
Double click when you're finished drawing, then click "Next." If you make a mistake, click "Restart" to start over. If you don't have a project location, click "I don't have a project location," then click "Next."

Back

Next

Restart

I don't have a project location



Esri Community Maps Contributors, Nebraska Game & Parks Commission, © OpenStreetMap, Microsoft, Esri, TomTom, Garmin, SafeGraph, GeoTechnologies, Inc, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS Powered by Esri

Reset All

- 1 Agency Information
 - Submitting Entity Type: Local Government
- 2 Project Location
 - Project Location: Yes
- 3 Project Phase
- 4 Primary Components
- 5 Project Size

Matched Grants: 0

Project Phase

Grants can fund the cost of Planning phase activities, such as project design or the production of planning documents; or Capital phase activities, which primarily includes construction costs. Some grants have separate applications and available funds for Planning and Capital projects. A few grants will fund both Capital and Planning phase activities based on a single application. To help direct you to the proper grant, please select one button below that best indicates the phase(s) of activities you would be interested in funding through a grant, then click "Next".

◀ Back

Next ▶

- Planning
- Capital
- Capital and Planning

Reset All

1 Agency Information

- Submitting Entity Type: Local Government

2 Project Location

- Project Location: Yes

3 Project Phase

4 Primary Components

5 Project Size

Matched Grants: 0

Project Components

Select all components included in your project

Projects frequently include multiple components (i.e. a bridge and a roadway, or a roadway and active transportation infrastructure). Select all components that are included in your project, then answer a few questions about each component. When you are done, click "Next". If you need to remove a component, simply click on the button again. You can review your current selections on the right side of the page; click the down arrow to review your answers. Click on any answer to change it.

Roadway

Bridge

Culvert and/or Stormwater Management

Freight Rail or Intermodal Freight Facility

Passenger Rail or Transit

Active Transportation (bike, ped, etc.)

Marine (port, marine corridor, etc.)

Airport Surface Infrastructure

Safety Plan Development or Implementation

Electric & Alternative Fueling Infrastructure

Back

Next

Reset All

1 Agency Information

- Submitting Entity Type: Local Government

2 Project Location

- Project Location: Yes

3 Project Phase

- Project Phase: Planning

4 Primary Components

5 Project Size

Matched Grants: 0

×

Is your bridge project a public bridge?

No Yes

×

Is your bridge project on any of the following systems?

National Highway System

Multimodal Freight Network

Highway Freight Network

No Yes

×

Is your roadway project a tribal facility that is a federal responsibility?

No Yes



Does a primary component of your project include improving resilience?

Lengthening or raising bridges to increase waterway openings, including to respond to extreme weather

Installing seismic retrofits on bridges

Adding scour protection

Reduce the risk of damage to a bridge on an evacuation route

No

Yes



Does a primary component of your bridge project include any of the following in order to increase connectivity of a divided community?

Removal

Retrofitting

Mitigate impacts

No



Yes

Project Components

Select all components included in your project

Projects frequently include multiple components (i.e. a bridge and a roadway, or a roadway and active transportation infrastructure). Select all components that are included in your project, then answer a few questions about each component. When you are done, click "Next". If you need to remove a component, simply click on the button again. You can review your current selections on the right side of the page; click the down arrow to review your answers. Click on any answer to change it.

Roadway

Bridge

Culvert and/or Stormwater
Management

Freight Rail or Intermodal Freight
Facility

Passenger Rail or Transit

Active Transportation (bike, ped, etc.)

Marine (port, marine corridor, etc.)

Airport Surface Infrastructure

Safety Plan Development or
Implementation

Electric & Alternative Fueling
Infrastructure

Back

Next

Reset All

1 Agency Information

- Submitting Entity Type: Local Government

2 Project Location

- Project Location: Yes

3 Project Phase

- Project Phase: Planning

4 Primary Components

Bridge



5 Project Size

Matched Grants: 0

Estimated Activity Cost

Please choose the estimated cost range of the activity/project phase you would like to fund with a grant

If you are not sure or would like to see all relevant grant opportunities regardless of each grant's funding limitations, please select "Unknown/Not Sure".

Back

Next

Less than \$1 Million

\$1 million - \$1.24 million

\$1.25 million - \$2.9 million

\$3 million - \$5.9 million

\$6 million - \$7.9 million

\$8 million - \$17.9 million

\$18 million or more

Unknown/Not sure

Reset All

1 Agency Information

- Submitting Entity Type: Local Government

2 Project Location

- Project Location: Yes

3 Project Phase

- Project Phase: Planning

4 Primary Components

Bridge



5 Project Size

Matched Grants: 0

← Back to grant pairing

Grant Pairing Results

5 grant results found

This page displays your results from the grant pairing. If you have results, click on "View Grant Details" to view more information on the grant in a new tab and links to additional resources.

If you wish to adjust your pairing criteria select the "Back to grant pairing". **Do not hit the browser's back button.** This will require you to complete the grant pairing anew. This will bring you to a new page with your personalized results. You can save the URL to this page and come back to it whenever.

If you have questions or are ready to move forward with developing a grant application, email NDOT.granthelp@nebraska.gov.

Save Your Results

If you would like to save these results to refer back to later, click the button below then bookmark the URL.

Save Results

Your entire project may be eligible for the following grant(s):

Rebuilding American Infrastructure with Sustainability and Equity (RAISE) Grant

RAISE grants are awarded on a competitive basis to support planning or constructing capital investments in surface transportation that will have a significant local or regional impact. Specifically, RAISE will be awarded for projects that will improve safety; environmental sustainability; quality of life; mobility and community connectivity; economic competitiveness and opportunity, including tourism; the state of good repair; partnership and collaboration; and innovation.

Eligible Components

- Roadway
- **Bridge (Selected)**
- Culvert and/or Stormwater Management
- Freight Rail or Intermodal Freight Facility
- Passenger Rail or Transit
- Active Transportation (bike, ped, etc.)
- Marine (port, marine corridor, etc.)
- Airport Surface Infrastructure

Minimum Planning Project Size: \$0M

Minimum Capital Project Size: \$1.25M

[View Grant Details](#)

Rural Surface Transportation Grant (RURAL), a Multimodal Project Discretionary Grant (MPDG) Opportunity

The Rural Surface Transportation Grant (RURAL), offered under the Multimodal Project Discretionary Grant Opportunity (MPDG) is a grant only for entities located in rural regions. It supports projects that improve and expand the surface transportation infrastructure in rural areas to increase connectivity, improve the safety and reliability of the movement of people and freight, and generate regional economic growth and improve quality of life.

Eligible Components

- Roadway
- **Bridge (Selected)**
- Passenger Rail or Transit

Minimum Planning Project Size: \$0M

Minimum Capital Project Size: \$0M

[View Grant Details](#)

Bridge Investment Program

The Bridge Investment Program (BIP) provides funding for bridge replacement, rehabilitation, preservation, and protection projects that reduce the number of bridges in poor condition, or in fair condition at risk of declining into poor condition. The goals of the program are to 1) improve the safety, efficiency, and reliability of the movement of people and freight over bridges; 2) improve the condition of bridges in the United States; and 3) provide financial assistance that leverages and encourages non-Federal contributions from sponsors and stakeholders involved in the planning, design, and construction of eligible projects.

Eligible Components

- **Bridge (Selected)**
- Culvert and/or Stormwater Management

Minimum Planning Project Size: \$0M

Minimum Capital Project Size: \$3.125M

[View Grant Details](#)

Rural and Tribal Assistance Pilot Program

The Rural and Tribal Assistance Pilot Program is a grant only for entities located in rural regions. It provides funds for states, local governments, and tribal governments with grants to support project development leading to future applications to DOT credit or grant programs. This early-stage development and planning assistance can be used for either the hiring of staff or the procurement of expert firms to provide financial, technical, and legal assistance; assistance with development phase activities; and information regarding innovative financing best practices and case studies.

Eligible Components

- Roadway
- **Bridge (Selected)**
- Culvert and/or Stormwater Management
- Freight Rail or Intermodal Freight Facility
- Passenger Rail or Transit
- Active Transportation (bike, ped, etc.)
- Safety Plan Development or Implementation
- Electric & Alternative Fueling Infrastructure
- Marine (port, marine corridor, etc.)
- Airport Surface Infrastructure

Minimum Planning Project Size: \$0M

Minimum Capital Project Size: \$0M

[View Grant Details](#)

Promoting Resilient Operations for Transformative, Efficient, and Cost-Saving Transportation (PROTECT)

The vision of the Promoting Resilient Operations for Transformative, Efficient, and Cost-saving Transportation (PROTECT) discretionary grant program is to fund projects that ensure surface transportation resilience to natural hazards including climate change, sea level rise, flooding, extreme weather events, and other natural disasters through support of planning activities, resilience improvements, community resilience and evacuation routes, and at-risk coastal infrastructure. PROTECT is meant for surface transportation systems, including highways, public transportation, ports, and intercity passenger rail. Projects selected under this program should be grounded in the best available scientific understanding of climate change risks, impacts, and vulnerabilities. They should support the continued operation or rapid recovery of crucial local, regional, or national surface transportation facilities. Furthermore, selected projects should utilize innovative and collaborative approaches to risk reduction, including the use of natural infrastructure.

Eligible Components

- Roadway
- **Bridge (Selected)**
- Culvert and/or Stormwater Management
- Passenger Rail or Transit
- Marine (port, marine corridor, etc.)

Minimum Planning Project Size: \$0.125M

Minimum Capital Project Size: \$0.625M

[View Grant Details](#)

Considerations for Alternative Funding Sources

Looking for other options or no federal grant opportunities returned? You may either adjust your criteria by clicking on "back to grant pairing" at the top left of the screen, start over with a new project by clicking on "Grant Pairing" on the top right of the screen, or consider the alternative funding sources below.

Local Taxes and Bonds

Utilize local tax revenue, special assessments, or municipal bonds to finance smaller projects.

State Grants and Programs

Explore state-level grant programs and funding opportunities tailored for smaller-scale projects.

- [Federal Fund Purchase Program \(FFPP\)](#)
- [Highway Safety Improvement Program \(HSIP\)](#)
- [Off-system Bridge Program \(BRO\)](#)
- [Transportation Alternative Program \(TAP\)](#)
- [County Bridge Match Program \(CBMP\)](#)
- [State Rec Road \(SRR\)](#)
- [Economic Opportunity Program \(EOP\)](#)



LETTER OF SUPPORT

If you need a letter of support for you project
[click here](#)



Information

Organization Requesting Support

Select a County

Project Name

Contact Number

Contact Email

Local Agency Contact Name

Select The Competitive Grant Program You Are Applying For

Where Should The Letter of Support Be Emailed?


Select The Day Letter Is Needed By

Application Narrative - Please Upload a PDF, Docx, or JPG File Below

Choose Files No file chosen

SUBMIT

NDOT Managed Grants

- 120th Street - Stonegate Drive and Roanoke Boulevard (Construction) \$16,960,000
 - Blair South Bypass (Construction) \$7,560,000
 - York Project Access (Construction) \$15,625,000
 - Court Street Access and Safety Transformation (Construction) \$21,389,41
 - Lincoln County Crossroads (Planning) \$750,000
 - Ames Avenue (Planning) \$1,500,000
 - Fork Crook Road (Planning/PEL) \$440,000
 - Platte County Loup Canal Bridges (Construction) \$12,000,000
 - Congressional Direct Spending Grants – 8 Grants - \$33,000,000
- 

NDOT Grant Portal Tracking

Note: Data does not include entries with blank entity.

55

Grant Portal Entries

CreationDate

8/19/2024

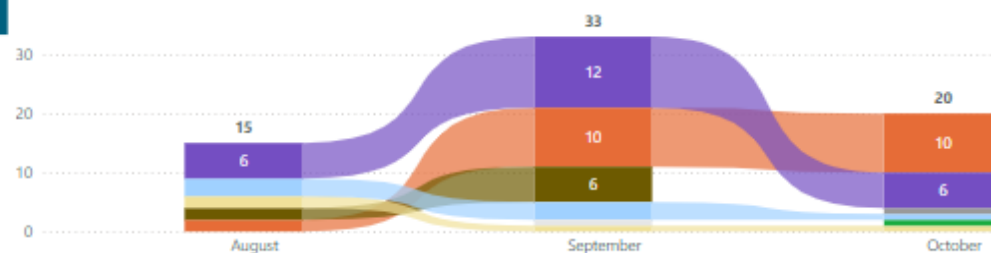
10/18/2024



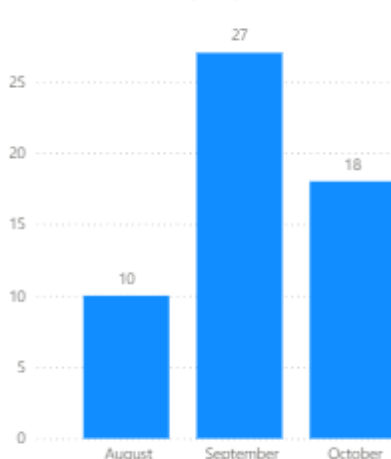
Grant Portal Usage by Month and Project Type

Note: Some entries may include more than one project type.

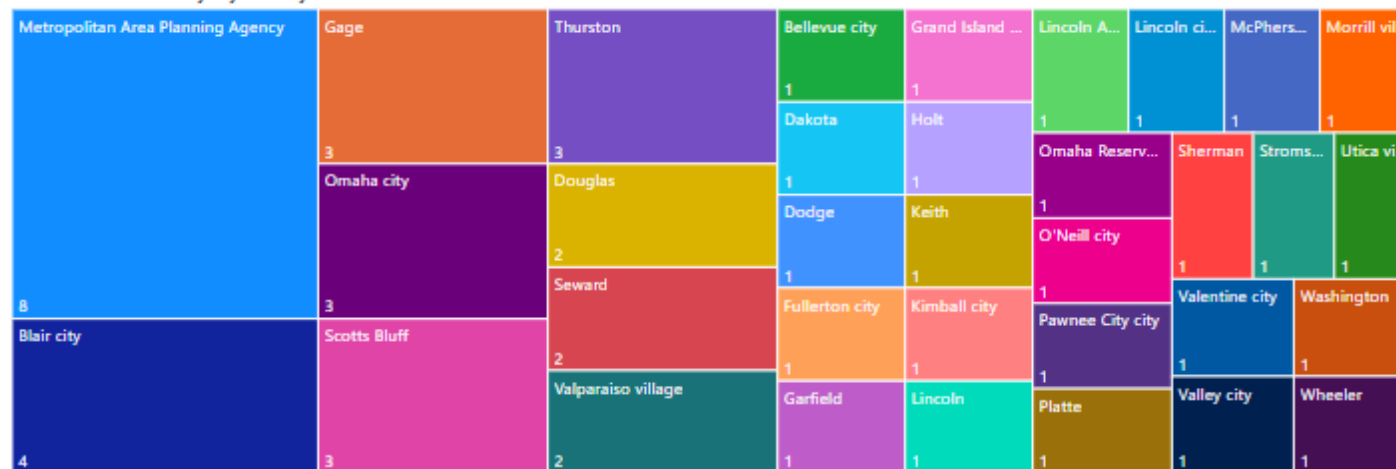
Active Transport Airport Bridge Safety Sustainability Culvert/ Storm water Marine Passenger Rail Freight Rail Roadway



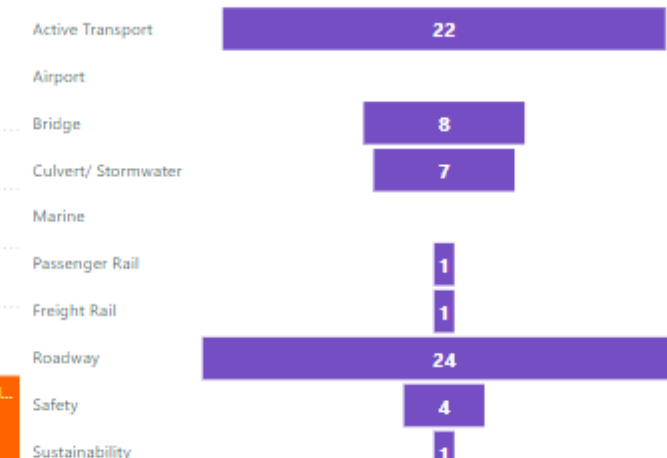
Grant Portal Usage by Month



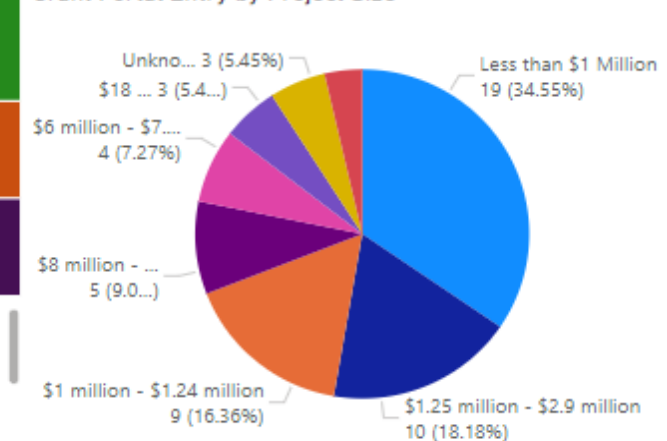
Grant Portal Entry by Entity



Grant Portal Entries by Project Type

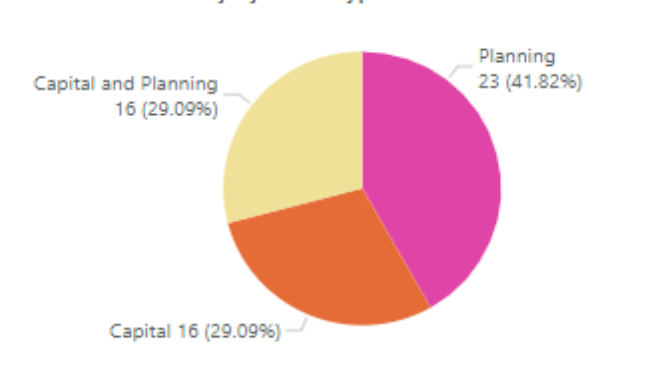


Grant Portal Entry by Project Size



Entity	Entity Type	Creation Date	Entries	Capital and Planning	Project Size	Active Transport	Airport	Bridge	Culvert/ Storm Water	Marine	Passenger Rail	Freight Rail	Roadway	Safety	Sustainability
Gage	applicant_local_gov	9/17/24	1	Capital	\$1 million - \$1.24 million								1		
Holt	applicant_local_gov	9/26/24	1	Capital and Planning	\$1 million - \$1.24 million	1									
Metropolitan Area Planning Agency	applicant_mpo	9/27/24	1	Planning	\$1 million - \$1.24 million								1		
Morrill village	applicant_local_gov	10/18/24	1	Capital	\$1 million - \$1.24 million				1						
Omaha city	applicant_local_gov	9/11/24	1	Planning	\$1 million - \$1.24 million	1									
Omaha Reservation	applicant_tribe	9/11/24	1	Planning	\$1 million - \$1.24 million	1									
Sherman	applicant_local_gov	9/26/24	1	Capital and	\$1 million - \$1.24 million	1									
Total			55			22	8	7	1	1	1	1	24	4	1

Grant Portal Entry by Grant Type



Stakeholder Coordination

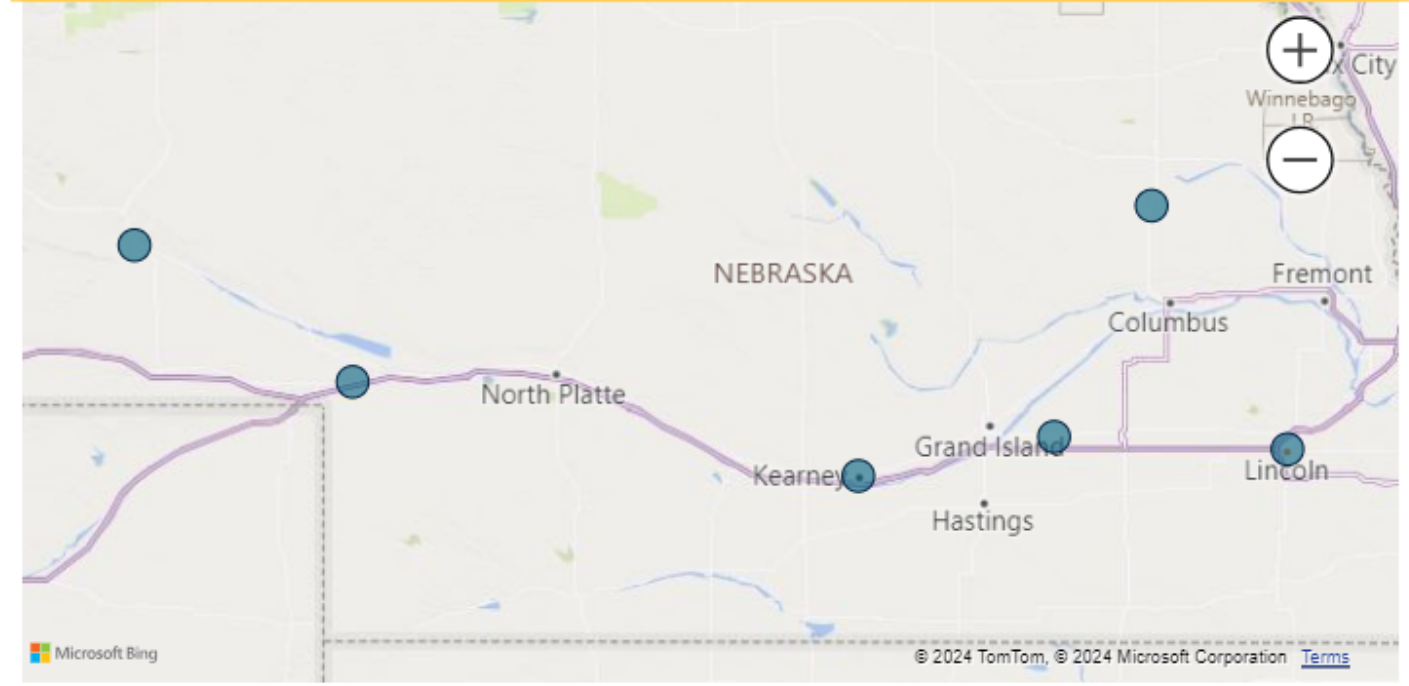


Launched in August 2024, The Nebraska Infrastructure Hub, the state's first-ever intergovernmental entity for empowering public agencies in competing for federal discretionary grants, aims to increase Nebraska's grant competitiveness and provide support to state and local partners in seeking similar opportunities.

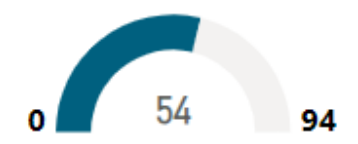
The information on this page is a summary of the Stakeholder Coordination with Locals that has occurred through the Hub.

Stakeholders include: County, City or Municipality Reps, Economic Development Districts, MPO's, Organizations, Businesses, etc.

Infrastructure Hub Visits to Locals



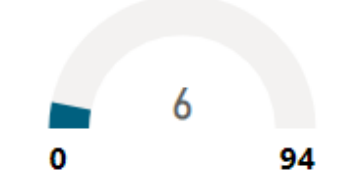
Counties HUB has Met with a Local Representative



Stakeholders Met With

81

Counties Visited



Emails Sent

134



Phone Calls

3



Meetings and Events (In Person or Virtual)

27



Miles Traveled to Meet with Locals

2,345



Hours of Assistance Provided through Meetings/ Calls

29.00



Website Visits

1,773



Need Grant Help?

- Grant Pairing Portal
- Grants 101
- Letters of Support
- Questions

