

Nebraska Annual Traffic Safety Study 2015

THE NEBRASKA POLL

FINAL REPORT

For the Nebraska Safety Council
and Nebraska Highway Office of Safety

May 2015



Research Associates • 402.730.6270 • researchassoc@windstream.net

Nebraska Annual Traffic Safety Study 2015

The Nebraska Poll
For Nebraska Safety Council and Nebraska Highway Safety

HIGHLIGHT SUMMARY

Methodology

- Research Associates completed 902 telephone interviews from random samples of Nebraska land and cell phones during April, 2015.
- The purpose of the study was to measure Nebraskans' attitudes toward various highway safety issues.
- The statewide study has a maximum error range of +/- 3.3% at a 95% confidence level.

Impaired Driving

- Core Q: 23% of the respondents indicated they had driven within two hours after drinking alcoholic beverages at least once in the last 60 days.
- Core Q: About a third (34%) indicated the chances of getting arrested if they drive after drinking are somewhat likely.
- Core Q: Just less than half of the respondents (48%) indicated awareness of drunk driving enforcement by police in the last 30 days.
- Just over half (53%) indicated awareness of drunk driving messages in the past 60 days.

Seat Belts

- Core Q: A majority (76%) of respondents indicated they always wear safety belts when they drive or ride.
- Core Q: A majority (60%) indicated the chances of getting a ticket for not wearing a seat belt are unlikely (32% somewhat unlikely plus 28% very unlikely).
- Core Q: 27% indicated they had heard something about seat belt law enforcement in the last 60 days.
- About half (52%) indicated awareness of Click It Or Ticket messages in the past 60 days. A plurality of those (45%) had seen the messages on TV.

Speed

- Core Q: A plurality (48%) indicated they rarely drive faster than 35 mph on a road posted at 30 mph.
- Core Q: Less than half (42%) indicated they never drive faster than 70 mph on a road posted at 65 mph.
- Core Q: 37% indicated awareness of speed enforcement in the past 30 days.
- Core Q: A plurality (36%) indicated the chances of getting a ticket for speeding are somewhat likely.

Distracted Driving

- About two-thirds (65%) support a law banning drivers from talking on a cell phone while driving.
- Just less than two-thirds (61%) support a law allowing a ticket solely for cell phone use while driving.
- Nine out of ten respondents (90%) support a law allowing drivers to be stopped and ticketed solely for texting while driving.

Other Highway Safety Issues

- Most respondents (74%) indicated that Nebraska's law requiring motorcycle helmets should be continued.
- A majority (64%) indicated that drivers over the age of 70 should be required to drive as part of their test every time they renew their license.

Demographics

- Most respondents (98%) indicated they have a driver's license.
- Age (controlled): 16-20, 3%; 21-34, 8%; 35-49, 33%; 50-64, 40%; 65 up, 16%.
- Annual household incomes: under \$40,000, 19%; \$20,000-40,000, 36%; over \$40,000, 45%.
- Phone type: 78% from traditional landlines; 22% from cell phones.
- Congressional District (controlled): District One, 33%; District Two, 33%; District Three, 34%.
- Gender: 48% male and 52% female (controlled).



Nebraska Annual Traffic Safety Study, 2015

THE NEBRASKA POLL
May 2015

STUDY DESCRIPTION (N=902)

The purpose of this edition of The Nebraska Poll was to measure the attitudes of Nebraskans relative to highway safety issues, including but not limited to the issues of impaired driving, seat belt safety, speed and driving while distracted (using cell phones and other electronic devices while driving).

Research Associates designed the survey instrument with input and final approval from Nebraska Safety Council staff and the Nebraska Office of Highway Safety.

The instrument designed to meet the purpose of this study was administered by telephone to a total sample of 902 randomly selected Nebraskans during April 2015. The sample was geographically representative by Nebraska congressional district. Completed interviews were from both cell phones (22%) and landlines (78%).

A sample of 902 randomly selected respondents provides results that are statistically accurate within an expected maximum error range of plus or minus 3.3% at a 95% confidence level. In other words, if this survey were administered to the entire population studied, the results would vary no more than plus or minus 3.3% from the results reported here, 95 times out of 100. The actual expected maximum error ranges vary based on the specific percentage results obtained for each response, as shown below.

Expected Maximum Error Ranges at a 95% Confidence Level for a Sample of 902 (Statewide Data)

10%	20%	30%	40%	50%	60%	70%	80%	90%
+/-2.0%	+/-2.6%	+/-3.0%	+/-3.2%	+/-3.3%	+/-3.2%	+/-3.0%	+/-2.6%	+/-2.0%

Expected Maximum Error Ranges at a 95% Confidence Level for a Sample of 300 (Congressional District Data)

10%	20%	30%	40%	50%	60%	70%	80%	90%
+/-3.4%	+/-4.5%	+/-5.2%	+/-5.5%	+/-5.7%	+/-5.5%	+/-5.2%	+/-4.5%	+/-3.4%

The figures in this report represent percentages unless specifically stated otherwise. Columns may not always total exactly 100% due to rounding.

Terminology: the mean score is the same as the average score. The "N" is the number of cases (respondents) in the study. An open-ended question is one in which no response choices were provided to respondents and captured responses were then categorized into groups or ranges.



Nebraska Annual Traffic Safety Study, 2015

THE NEBRASKA POLL
May 2015

NARRATIVE SUMMARY (N=902)

Impaired Driving

The first question was open-ended and asked respondents to name the biggest problem in Nebraska today. Roads and safety-related responses included: distracted driving (14%); drunk/impaired driving (4%); traffic and road conditions (16%).

Respondents were next asked to rate three specified problems on a 1 to 5 scale, with 5 as very important and 1 as not at all important. Both distracted driving (with a mean score of 4.40) and drunk driving (mean score of 4.32) were named as bigger problems than the fuel tax situation (3.48) by a significant margin.

Respondents were then asked an open-ended question about the best way to solve the drunk driving problem. Although responses were fairly scattered, more severe penalties led the list with a 29% plurality, followed by stricter law enforcement (19%) and education (17%).

A majority of respondents (55%) indicated Nebraska penalties for drunk driving are not tough enough, while 40% indicated they are about right and 5% indicated they are too tough.

Respondents were next asked whether they favor or oppose each of six specific penalties for drunk driving. All six were favored by a majority of respondents. Leading the list of those favoring was jail terms for previous offenses (81%), followed by mandatory treatment for offenders (75%); then mandatory interlock ignition for all first-time offenders (72%); mandatory sentencing for offenders (67%); eliminating plea bargaining for drunk driving offenses (61%); and losing license for first offense (52%).

Eleven percent (11%) of respondents indicated they had driven while impaired by alcohol but still under the legal limit in the last 60 days.

CORE QUESTION: About one-fourth of the respondents (23%) indicated they had driven within two hours after drinking alcoholic beverages at least once in the last 60 days.

CORE QUESTION: About a third of the respondents (34%) indicated the chances of getting arrested if they drive after drinking are somewhat likely, while another 30% indicated chances of that are somewhat unlikely.

CORE QUESTION: Less than half of the respondents (48%) indicated they had seen or heard something about drunk driving enforcement by police in the last 30 days.

Over half of the respondents (53%) indicated they had read, seen or heard drunk driving-related messages in the past 60 days.



Seat Belts

CORE QUESTION: About three-fourths of respondents (76%) indicated they always wear safety belts when they drive or ride.

CORE QUESTION: A majority (60%) of respondents thought the chances of getting a ticket for not wearing a seat belt were unlikely (32% somewhat unlikely plus 28% very unlikely).

CORE QUESTION: About a fourth of respondents (27%) indicated they had seen or heard something about seat belt law enforcement by police in the last 60 days.

About half the respondents (52%) indicated they had read, seen or heard Click It Or Ticket seat belt messages in the past 60 days. In an open ended question, a plurality of respondents (45%) indicated they had seen the Click It Or Ticket ads on TV, while 21% indicated billboards and 17% indicated they had heard the radio ads.

For the sixth straight year, a majority of respondents (54%) indicated that law enforcement officers should be allowed to stop drivers and ticket them for not wearing a seat belt.

Respondents were asked what level of fine would be most effective to get people to wear their seat belt all the time. A plurality (35%) indicated \$25-50, 28% indicated \$51-100, 19% indicated more than \$200, 13% indicated \$101-150 and 5% indicated \$151-200.

Nearly a fourth of the respondents (24%) indicated they have children weighing between 40 and 80 pounds, and of those 71% indicated they use a booster seat for their child in the car.

Speed

CORE QUESTION: A plurality of respondents (48%) indicated they rarely drive faster than 35 mph on a road posted at 30 mph, while 21% indicated never, 17% indicated half the time and 14% indicated most of the time.

CORE QUESTION: A plurality of respondents (42%) indicated they never drive faster than 70 mph on a road posted at 65 mph, while another 42% indicated they rarely drive faster than 70 on those roads, 10% indicated half the time and 6% indicated most of the time.

CORE QUESTION: Just over a third of respondents (37%) indicated they had heard something about speed enforcement by police in the past 30 days.

CORE QUESTION: A plurality of respondents (36%) indicated the chances of getting a ticket if you drive over the speed limit are somewhat likely; 25% indicated likely, 19% unlikely, 16% very likely and 5% very unlikely.

Distracted Driving Laws

About two-thirds of the respondents (65%) indicated they would support a law banning drivers from talking on a cell phone while driving. About one-third (32%) opposed that idea and 3% had no opinion.

A majority of respondents (61%) also supported a law allowing law enforcement to stop a driver and ticket them solely for talking on a cell phone while driving, while 37% opposed that and 2% had no opinion.

Nine out of ten respondents (90%) indicated support for a law that would allow law enforcement to stop a driver and ticket them solely for texting while driving, while 9% opposed that and 1% had no opinion.



Miscellaneous

Most respondents (74%) again indicated that Nebraska's law requiring motorcycle helmets should be continued, while 22% said it should be repealed and 4% had no opinion.

A majority of respondents (64%) indicated that drivers over the age of 70 should be required to drive as part of their test every time they renew their license, while 34% opposed such a requirement and 3% had no opinion.

Demographics

Nearly all respondents (98%) indicated they have a driver's license.

Respondents indicated distribution in the following age categories (controlled variable): 16-20, 3%; 21-34, 8%; 35-49, 33%; 50-64, 40%; 65 up, 16%.

Respondents indicated distribution in the following annual household income categories: under \$40,000, 19%; \$40,000-80,000, 36%; over \$80,000, 45%.

Cell phone numbers were included in the sample; 78% of the respondents were interviewed on their traditional landlines and 22% on their cell phones.

Respondents were distributed among Nebraska's three congressional districts as follows (controlled variable): District One, 33%; District Two, 33%; District Three, 34%.

By control, respondents were 48% male and 52% female.



Nebraska Annual Traffic Safety Study, 2015
THE NEBRASKA POLL
May 2015

TABULAR SUMMARY
(N=902)

	Percentage			
	State N=902	Dist One N=294	Dist Two N=301	Dist Three N=307
<i>General</i>				
1. First, what do you think is the biggest problem in Nebraska today? (open-ended)				
Others (government/politicians; social issues like Immigration/health care; pipeline; various others)	19	20	20	18
Economy, drought, water, jobs, wages, etc.	9	8	8	11
Crime drugs, gangs, vandalism, etc.	11	10	16	8
Taxes, spending, budget, school funding, etc.	27	26	24	33
Roads, streets, traffic, speeding, etc.	16	14	17	15
Distracted drivers	14	18	12	12
Drunk drivers	4	5	2	3
Mean				
2-4. On a 1-5 scale, with 5 as very important and 1 as not at all important, how would you rate the following problems in Nebraska:				
How important is the problem of distraction caused by drivers using cell phones or other electronic devices?	4.40	4.46	4.33	4.42
How important is the problem of driving while intoxicated or drunk driving?	4.32	4.39	4.36	4.22
How important is the fuel tax situation?	3.48	3.33	3.50	3.62



	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Impaired Driving</i>				
5. What do you think is the best way to solve the drunk driving problem? (open-ended)				
More severe penalties	29	29	34	24
Education	17	16	16	18
Stricter law enforcement	19	20	14	22
High visibility law enforcement	6	6	6	7
Mandatory sentences for convictions	4	3	4	4
Others (bar responsibility, designated driver, etc.)	26	26	26	26
6. Do you think the Nebraska penalties for drunk driving are:				
Too tough	5	3	5	7
About right	40	40	37	44
Not tough enough	55	57	59	49
7-12. Would you favor or oppose each of the following penalties for drunk driving: (rotated)				
Jail terms for previous offenses				
Favor	81	80	81	82
Don't know	5	4	6	4
Opposed	14	16	13	14
Mandatory treatment for drunk driving offenders				
Favor	75	76	74	76
Don't know	4	4	6	3
Opposed	20	20	20	21
Mandatory interlock ignition for all first-time offenders				
Favor	72	72	71	74
Don't know	3	2	3	3
Opposed	25	26	26	24
Mandatory sentencing for drunk driving offenses				
Favor	67	67	63	69
Don't know	7	5	9	7
Opposed	26	27	28	24
Eliminating plea bargaining for drunk driving offenses				
Favor	61	63	58	62
Don't know	6	5	8	4
Opposed	33	31	34	34
Lose license for first offense				
Favor	52	50	50	55
Don't know	3	4	3	3
Opposed	45	46	48	41



	State	Percentage		
		Dist One	Dist Two	Dist Three
13. In the last 60 days, have you ever driven while impaired by alcohol, but still under the legal limit?				
Yes	11	10	12	11
No	89	90	88	90
14. What do you think the chances are of someone getting arrested if they drive after drinking? (Core Q. A-3)				
Very likely	10	9	9	13
Somewhat likely	34	33	33	35
Likely	16	16	16	16
Somewhat unlikely	30	32	33	25
Very unlikely	10	9	11	11
15. In the past 60 days, how many times have you driven a motor vehicle within 2 hours after drinking alcoholic beverages? (Core Q. A-1)				
Never	76	78	71	79
Once	11	13	13	8
2-5 times	10	7	13	11
More than 5 times	2	3	2	2
16. In the past 30 days, have you read, seen or heard anything about alcohol impaired driving (or drunk driving) enforcement by police? (Core Q. A-2)				
Yes	48	45	47	52
No	52	55	53	48
17. In the past 60 days, have you read, seen or heard any drunk driving-related messages?				
Yes	53	57	51	51
No	47	43	49	49



	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Seat Belts</i>				
18. How often do you use safety belts when you drive or ride in a car, van, sport utility vehicle or pickup? (Core Q. B-1)				
Always	76	83	84	63
Nearly always	15	12	9	24
Sometimes	5	3	4	7
Seldom	2	1	1	4
Never	2	1	2	3
19. Should law enforcement officers be allowed to stop drivers and ticket them for not wearing a seat belt?				
Yes	54	52	55	54
Don't know	2	2	2	2
No	44	45	44	44
20. What do you think the chances are of getting a ticket if you don't wear your safety belt? (Core Q. B-3)				
Very likely	9	6	9	10
Somewhat likely	20	20	17	23
Likely	12	10	10	15
Somewhat unlikely	32	33	35	29
Very unlikely	28	30	30	23
21. What level of fine do you think would be most effective way to get people to wear their seat belt all the time?				
\$25-50	35	33	35	37
\$51-100	28	27	28	30
\$101-150	13	14	11	15
\$151-200	5	7	5	3
Over \$200	19	19	22	15
22. Do you have any children weighing between 40 and 80 pounds?				
Yes	24	25	22	24
No	76	75	78	76
23. (IF YES) Do you use a booster seat for your child in the car?				
Yes	71	74	67	73
No	29	26	33	27
24. In the past 60 days, have you read, seen or heard anything about seat belt law enforcement by police? (Core Q. B-2)				
Yes	27	21	24	34
No	73	79	76	66



	State	Percentage		
		Dist One	Dist Two	Dist Three
25. In the past 60 days, have you read, seen or heard any Click It Or Ticket seat belt messages?				
Yes	52	51	50	53
No	48	49	50	47
26. (IF YES) Do you remember where that was?				
TV	45	49	41	44
Radio	17	16	11	24
Billboards/roadside signs	21	18	29	15
Others (newspaper, internet, posters)	11	13	8	12
Don't know	7	4	12	5



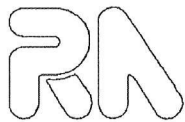
	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Speed</i>				
27. On a local road with a speed limit of 30 mph, how often do you drive faster than 35 mph: (Core Q. S-1a)				
Most of the time	14	14	18	11
Half the time	17	15	18	17
Rarely	48	51	46	46
Never	21	20	18	26
28. On a local road with a speed limit of 65 mph, how often do you drive faster than 70 mph: (Core Q. S-1b)				
Most of the time	6	8	8	3
Half the time	10	9	13	8
Rarely	42	41	45	40
Never	42	42	35	49
29. In the past 30 days, have you read, seen or heard anything about speed enforcement by police? (Core Q. S-2)				
Yes	37	38	34	38
No	63	62	66	62
30. What do you think the chances are of getting a ticket if you drive over the speed limit? (Core Q. S-3)				
Very likely	16	12	15	21
Somewhat likely	36	37	37	32
Likely	25	26	21	29
Somewhat unlikely	19	22	21	13
Very unlikely	5	3	5	6



	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Distracted Driving</i>				
31. Would you support or oppose a law banning drivers from talking on a cell phone while driving?				
Support	65	63	69	63
Oppose	32	33	30	34
Don't know	3	4	1	4
32. Would you support or oppose a law that allows law enforcement to stop a driver and ticket them solely for talking on a cell phone while driving?				
Support	61	57	66	60
Oppose	37	42	33	38
Don't know	2	1	1	3
33. Would you support or oppose a law that allows law enforcement to stop a driver and ticket them solely for texting while driving?				
Support	90	87	91	91
Oppose	9	12	8	8
Don't know	1	1	1	2



	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Miscellaneous</i>				
34. Should the Nebraska law requiring motorcycle helmets be repealed or continued?				
Continued	74	77	74	70
Repealed	22	19	22	25
Don't know	4	4	4	5
35. Should drivers over age 70 be required to drive as part of their test every time they renew their license?				
Yes	64	61	66	64
Don't know	3	3	2	3
No	34	36	32	33



	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Demographics</i>				
36. Which of these age groups are you in:				
16-20	3	4	4	2
21-34	8	9	7	9
35-49	33	32	35	32
50-64	40	42	36	42
65 up	16	13	19	16
37. Do you have a driver's license?				
Yes	98	98	99	98
No	2	2	1	2
38. Which of these annual household income groups are you in? *				
Under \$40,000	19	20	18	17
\$40,000-80,000	36	37	32	41
Over \$80,000	45	43	50	42
39. (NOTE CONGRESSIONAL DISTRICT):				
One	33	100	0	0
Two	33	0	100	0
Three	34	0	0	100
40. (NOTE PHONE TYPE):				
Land line	78	80	81	75
Cell phone	22	20	19	25
41. Gender				
Male	48	48	48	49
Female	52	52	52	51

###



Study Notes on the Longitudinal Report

Research Associates has conducted a statewide study on Nebraskans' attitudes toward highway safety issues periodically for more than two decades.

The following section presents the results of the studies conducted since 1995, wherever similar questions were asked.

A similar methodology was employed in all the studies: households were randomly selected proportionate to population within each of Nebraska's three congressional districts. A total of at least 900 telephone interviews were completed for each study.

Longitudinal comparisons are based on the statewide database.



TABULAR SUMMARY, STATEWIDE DATA, LONGITUDINAL COMPARISONS (N=900)*
1995 - 2015

	Percentage											
	15	14	13	12	11	10	08	04	02	01	00	95
General												
1. <i>What do you think is the most important problem in Nebraska today? (open-ended)</i>												
Taxes, spending, budget, school funding, etc.	27	17	24	16	26	19	23	35	27	29	31	15
Traffic, traffic violations, road conditions	16	26	14	6	17	23	28	20	25	31	27	
Distracted driving	14	19	4	7								
Crime, drugs, gangs, teen violence, etc.	11	5	7	8	6	5	11	9	18	17	12	34
Economy, unemployment, wages, etc.	9	11	19	40	23	28	15	20	17	13	14	16
Drunk driving	4	5	3	7	6	3	6	4	8	6	8	7
All others	19	18	29	16	22	22	17	12	6	5	8	28
2-4. <i>On a 1-5 scale, with 5 as very important and 1 as not at all important, how would you rate the following problems in Nebraska</i>												
Driving while intoxicated or drunk driving	4.3	4.4	4.3	4.5	4.6	4.5	4.4	4.3			4.1	4.3
Distraction caused by drivers using cell phones	4.4	4.5	4.3	4.4	4.4	4.3						
The fuel tax situation	3.5	3.6	3.6	3.6	4.0	3.7						

Impaired Driving

	Percentage											
5. <i>What do you think is the best way to solve the drunk driving problem?</i>												
More severe penalties	29	21	23	36	42	37	47	51		45	44	45
Stricter enforcement	19	14	13	24	13	15	19	18		28	27	30
Education	17	9	8	10	8	10	10	12		20	16	20
High visibility law enforcement	6	2	3	9	1	3	3					
Mandatory sentences for convictions	4	3	2	9	4	4						
Others (bar responsibility, designated driver, etc.)	26	51	52	13	32	32	22	18		7	13	6
6. <i>Do you think the Nebraska penalties for drunk driving are:</i>												
Too tough	5	3	4	3	3	3	3	3		2	1	2
About right	40	37	39	35	28	38	31	36		36	42	56
Not tough enough	55	60	57	62	69	60	67	61		62	57	42

* 2014: N=950



	Percentage											
	15	14	13	12	11	10	08	04	02	01	00	95
7-12. Would you favor or oppose each of the following penalties for drunk driving offenses:												
<i>Jail terms for previous offenses</i>												
Favor	81	83	84	82	84	81	77		73		77	
Don't know	5	4	2	4	4	5	8		7		8	
Oppose	14	13	14	13	12	14	14		20		15	
<i>Mandatory treatment</i>												
Favor	75	78	76	80	82	80	81					
Don't know	4	3	2	3	4	2	6					
Oppose	20	20	23	17	14	18	13					
<i>Mandatory sentencing</i>												
Favor	67	67	69	71	73	71	70	73			81	
Don't know	7	6	3	7	7	4	8	6			6	
Oppose	26	27	28	22	20	24	21	21			13	
<i>Mandatory interlock ignition for all first-time offenders</i>												
Favor	72	70	66	72	68	63	63					
Don't know	3	4	4	4	6	6	12					
Oppose	25	27	31	25	26	31	26					
<i>Eliminating plea bargaining</i>												
Favor	61	63	60	62	60	66	68	65			76	
Don't know	6	5	3	7	6	4	7	7			6	
Oppose	33	32	37	32	33	31	25	28			18	
<i>Lose license for first offense</i>												
Favor	52	51	51	55	57	58	58					
Don't know	3	3	2	3	4	3	4					
Oppose	45	46	47	42	38	39	38					
13. In the last 60 days, have you ever driven while impaired by alcohol, but still under the legal limit?												
Yes	11	8	9	7	6	7						
No	89	92	91	93	94	93						
14. What do you think the chances are of someone getting arrested if they drive after drinking? (Core Q. A-3)												
Very likely	10	8	5	4	4	2						
Somewhat likely	34	31	8	7	7	4						
Likely	16	14	48	48	49	45						
Somewhat unlikely	30	32	37	40	38	47						
Very unlikely	10	15	2	1	2	1						
15. In the past 60 days, how many times have you driven a motor vehicle within 2 hours after drinking alcoholic beverages? (Core Q. A-1)												
Never (none)	76	79	74	78	81	79						
Once	11	10	15	12	9	9						
2-5 times	10	9	9	9	8	10						
More than 5 times	2	2	2	1	1	3						



	Percentage											
	15	14	13	12	11	10	08	04	02	01	00	95
16. In the past 30 days, have you read, seen or heard anything about alcohol impaired driving enforcement by police?*												
(Core Q. A-2)												
Yes	48	46	50	56	62	51	38	32				45
No	52	54	50	44	38	49	62	68				55
17. In the past 60 days, have you read, seen or heard any drunk driving-related messages?												
Yes	53											
No	47											

Seat Belts

18. How often do you use safety belts when you drive or ride in a car, van, sport utility vehicle or pickup?												
(Core Q. B-1)												
Always	76	80	78	77	78	76	81	77	70		70	68
Nearly always	15	11	13	13	13	14	10	10	18		14	14
Sometimes	5	4	5	6	5	6	2	4	9		5	6
Seldom	2	2	2	3	2	2	--	--	--		--	--
Never	2	2	2	2	2	2	6	10	3		11	13

(Previous to 2010: "How often do you wear a seat belt when you drive?" Always; usually; sometimes; never)

19. Should law enforcement officers be allowed to stop drivers and ticket them for not wearing a seat belt?*												
Yes	54	58	53	68	60	55	41	39	44	37	31	29
Don't know	2	2	1	1	4	1	6	5	7	12	4	5
No	44	40	45	30	36	44	53	56	49	51	64	66

*Previous to 2012: "Should law enforcement officers be allowed to stop drivers and ticket them for not wearing a seat belt, if the drivers have broken no other laws?"

20. What do you think the chances are of getting a ticket if you don't wear your safety belt? (Core Q. B-3)												
Very likely	9	8	6	5	7	5						
Somewhat likely	20	19	23	25	22	22						
Likely	12	12	12	18	13	11						
Somewhat unlikely	32	34	37	33	37	36						
Very unlikely	28	28	22	19	21	27						

21. What level of fine do you think would be most effective to get people to wear their seat belt all the time?												
\$25-50	35	31	28	30								
\$51-100	28	25	30	28								
\$101-150	13	18	16	18								
\$151-200	5	6	5	6								
Over \$200	19	21	21	18								



	Percentage											
	15	14	13	12	11	10	08	04	02	01	00	95
22. Do you have children weighing between 40 and 80 pounds?												
Yes	24	18	20	18	13	22	12	17	23	19	21	
No	76	82	80	82	87	78	88	83	76	82	79	
23. (IF YES) Do you use a booster seat for your child in the car?												
Yes	71	71	76	71	75	74	75	71	74	50	49	
No	29	29	24	29	25	26	25	29	26	50	52	
24. In the past 60 days, have you read, seen or heard anything about seat belt law enforcement by police?*												
(Core Q. B-2)												
Yes	27	24	23	29	29	24	49	37				
No	73	76	77	71	71	76	51	63				
25. In the past 60 days, have you read, seen or heard any Click It Or Ticket seat belt messages?												
Yes	52											
No	48											
26. Do you remember where that was?												
TV	45											
Radio	17											
Billboard/road sign	21											
Others (newspaper, internet)	11											
Don't know	7											
	<i>Speed</i>											
27. On a local road with a speed limit of 30 mph, how often do you drive faster than 35 mph: (Core Q. S-1a)												
Most of the time	14	11	13	10	8	8						
Half the time	17	16	16	15	13	15						
Rarely	48	49	48	53	55	54						
Never	21	25	23	22	24	23						
28. On a local road with a speed limit of 65 mph, how often do you drive faster than 70 mph: (Core Q. S-1b)												
Most of the time	6	6	4	4	3	3						
Half the time	10	9	9	6	5	8						
Rarely	42	35	39	45	37	42						
Never	42	50	47	45	56	47						
29. In the past 30 days, have you read, seen or heard anything about speed enforcement by police? (Core Q. S-2)												
Yes	37	37	40	40	42	38						
No	63	63	60	60	58	62						



	Percentage											
	15	14	13	12	11	10	08	04	02	01	00	95
30. What do you think the chances are of getting a ticket if you drive over the speed limit? (Core Q. S-3)												
Very likely	16	16	12	14	13	12						
Somewhat likely	36	38	40	41	38	39						
Likely	25	22	25	26	27	24						
Somewhat unlikely	19	16	18	14	17	19						
Very unlikely	5	8	6	5	5	6						

Distracted Driving

31. Would you support or oppose a law banning drivers from talking on a cell phone while driving?												
Support	65	70	68	68								
Oppose	32	27	31	29								
Don't know	3	3	1	3								
32. Would you support or oppose a law that allows law enforcement to stop a driver ticket them solely for talking on a cell phone while driving?												
Support	61	66	62	65								
Oppose	37	32	37	32								
Don't know	2	2	1	3								
33. Would you support or oppose a law that allows law enforcement to stop a driver and ticket them solely for texting while driving?												
Support	90	92	88	89								
Oppose	9	7	11	10								
Don't know	1	1	1	1								

Miscellaneous

34. Should the Nebraska law requiring motorcycle helmets be repealed or continued?												
Continued	74	77	81	83	86	76	78	79				
Repealed	22	19	18	14	11	19	14	15				
Don't know	4	5	2	3	4	5	8	7				
35. Should drivers over age 70 be required to drive as part of their test every time they renew their license?												
Yes	64	56	58	62	54	64	53	65			74	56
Don't know	3	4	2	3	4	2	4	4			4	6
No	34	40	40	35	42	34	42	31			23	38



Percentage
15 14 13 12 11 10 08 04 02 01 00 95

Demographics

36. Which of these age groups are you in:												
16-20	3	4	6	4	2	3	3	4	5	4	4	4
21-34	8	18	14	8	6	7	4	13	18	16	23	24
35-49	33	25	28	27	19	39	19	27	27	27	31	31
50-64	40	30	33	35	34	30	33	29	24	26	18	20
65 up	16	23	20	25	40	20	41	26	27	28	23	21
37. Do you have a driver's license?												
Yes	98	96	98	97	97	96	95	97	97	97	97	97
No	2	4	2	3	3	4	5	3	3	3	3	3
38. Which of these annual household income groups are you in:*												
Under \$40,000	19	24	10	10	13	15	20	21	24	22	18	23
\$40,000-80,000	36	37	20	21	23	16	30	29	39	47	51	43
Over \$80,000	45	39	70	69	65	69	50	51	37	31	31	34
39. District												
One	33	31	30	30	34	34	33	33	33	33	33	33
Two	33	37	35	35	36	35	33	33	34	34	35	36
Three	34	32	35	35	30	30	33	33	33	33	32	31
40. Phone Type												
Traditional landline	78	81	87	94								
Cell phone	22	19	13	6								
41. Gender												
Male	48	50	49	48	47	48	48	47	48	45	47	46
Female	52	50	51	52	53	52	52	53	52	55	53	54

* * *

* '04-'13: under \$20,000; \$20,000-40,000; over \$40,000
 '01 and '02: under \$25,000; \$25,000-50,000; over \$50,000
 '95: under \$18,000; \$18,000-36,000; over \$36,000
 '93: under \$15,000; \$15,000-30,000; over \$30,000



Nebraska Annual Traffic Safety Study, 2015

THE NEBRASKA POLL

May 2015

TABULAR REPORT

(N=902)

	Percentage			
	State N=902	Dist One N=294	Dist Two N=301	Dist Three N=307
<i>General</i>				
2. First, what do you think is the biggest problem in Nebraska today? (open-ended)				
Others (government/politicians; social issues like Immigration/health care; pipeline; various others)	19	20	20	18
Economy, drought, water, jobs, wages, etc.	9	8	8	11
Crime drugs, gangs, vandalism, etc.	11	10	16	8
Taxes, spending, budget, school funding, etc.	27	26	24	33
Roads, streets, traffic, speeding, etc.	16	14	17	15
Distracted drivers	14	18	12	12
Drunk drivers	4	5	2	3
Mean				
2-4. On a 1-5 scale, with 5 as very important and 1 as not at all important, how would you rate the following problems in Nebraska:				
How important is the problem of distraction caused by drivers using cell phones or other electronic devices?	4.40	4.46	4.33	4.42
How important is the problem of driving while intoxicated or drunk driving?	4.32	4.39	4.36	4.22
How important is the fuel tax situation?	3.48	3.33	3.50	3.62



Age * Problem Crosstabulation

% within Age

		Problem							
		Other	Econ	Crime	Taxes	Roads	Distracted drivers	Drunk drivers	Gov
Age	16-20	9.5%	4.8%	4.8%	4.8%	38.1%	19.0%	14.3%	4.8%
	21-34	21.2%	5.8%	13.5%	11.5%	30.8%	13.5%		3.8%
	35-49	8.2%	9.5%	11.6%	31.9%	13.4%	13.8%	3.0%	8.6%
	50-64	10.8%	8.7%	9.8%	32.2%	13.3%	12.6%	3.1%	9.4%
	65 up	8.2%	9.8%	15.6%	18.9%	13.9%	17.2%	4.9%	11.5%
Total		10.2%	8.8%	11.5%	27.5%	15.4%	14.0%	3.5%	9.0%

Income * Problem Crosstabulation

% within Income

		Problem							
		Other	Econ	Crime	Taxes	Roads	Distracted drivers	Drunk drivers	Gov
Income	Under \$40,000	9.2%	10.8%	17.5%	15.8%	19.2%	15.8%	5.0%	6.7%
	\$40,000-80,000	12.3%	9.8%	10.6%	21.7%	14.5%	16.2%	4.3%	10.6%
	Over \$80,000	9.6%	8.0%	9.6%	37.5%	13.6%	10.6%	2.3%	8.6%
Total		10.5%	9.1%	11.4%	27.9%	14.9%	13.6%	3.5%	9.0%

Gender * Problem Crosstabulation

% within Gender

		Problem							
		Other	Econ	Crime	Taxes	Roads	Distracted drivers	Drunk drivers	Gov
Gender	Male	9.4%	8%	7.2%	29.2%	18.1%	13.1%	3.3%	11.7%
	Female	11.0%	10%	15.4%	25.6%	12.9%	15.2%	3.6%	6.6%
Total		10.2%	9%	11.3%	27.4%	15.5%	14.1%	3.5%	9.1%

District * Problem Crosstabulation

% within District

		Problem							
		Other	Econ	Crime	Taxes	Roads	Distracted drivers	Drunk drivers	Gov
District	One	11.9%	7.8%	10.2%	25.8%	14.3%	17.6%	4.5%	7.8%
	Two	9.8%	8.2%	15.9%	24.1%	17.1%	12.2%	2.4%	10.2%
	Three	9.0%	10.7%	7.7%	32.5%	15.0%	12.4%	3.4%	9.4%
Total		10.2%	8.9%	11.3%	27.4%	15.5%	14.1%	3.5%	9.1%

Phone Type * Problem Crosstabulation

% within Phone Type

		Problem							
		Other	Econ	Crime	Taxes	Roads	Distracted drivers	Drunk drivers	Gov
Phone Type	Land	8.6%	8.7%	11.2%	29.1%	14.5%	14.7%	3.1%	10.1%
	Cell	16.7%	9.3%	12.0%	20.7%	19.3%	12.0%	4.7%	5.3%
Total		10.2%	8.9%	11.3%	27.4%	15.5%	14.1%	3.5%	9.1%

Age * Distracted Driver Rating Crosstabulation

% within Age

		Distracted Driver Rating				
		Not at all important	2	3	4	Very important
Age	16-20			25.0%	14.3%	60.7%
	21-34	5.5%	5.5%	6.8%	34.2%	47.9%
	35-49	1.7%	3.8%	11.0%	23.7%	59.8%
	50-64	2.0%	4.5%	9.0%	18.6%	65.8%
	65 up	4.2%	0.7%	4.2%	13.4%	77.5%
Total		2.5%	3.6%	9.2%	20.6%	64.1%

Income * Distracted Driver Rating Crosstabulation

% within Income

		Distracted Driver Rating				
		Not at all important	2	3	4	Very important
Income	Under \$40,000	6.0%	2.0%	8.0%	14.7%	69.3%
	\$40,000-80,000	0.3%	2.4%	7.8%	23.6%	65.9%
	Over \$80,000	2.2%	4.9%	10.4%	21.5%	61.0%
Total		2.2%	3.4%	9.0%	21.0%	64.3%

Gender * Distracted Driver Rating Crosstabulation

% within Gender

		Distracted Driver Rating				
		Not at all important	2	3	4	Very important
Gender	Male	3.4%	5.3%	9.9%	22.3%	59.1%
	Female	1.5%	1.9%	8.8%	19.1%	68.7%
Total		2.4%	3.5%	9.3%	20.6%	64.1%

District * Distracted Driver Rating Crosstabulation

% within District

		Distracted Driver Rating				
		Not at all important	2	3	4	Very important
District	One	0.7%	4.4%	8.5%	20.7%	65.6%
	Two	3.3%	3.7%	12.0%	18.9%	62.1%
	Three	3.3%	2.6%	7.5%	22.1%	64.5%
Total		2.4%	3.5%	9.3%	20.6%	64.1%

Phone Type * Distracted Driver Rating Crosstabulation

% within Phone Type

		Distracted Driver Rating				
		Not at all important	2	3	4	Very important
Phone Type	Land	2.0%	3.4%	9.8%	19.4%	65.4%
	Cell	4.1%	4.1%	7.7%	25.0%	59.2%
Total		2.4%	3.5%	9.3%	20.6%	64.1%

Age * Drunk Driving Rating Crosstabulation

% within Age

		Drunk Driving Rating				
		Not at all important	2	3	4	Very important
Age	16-20				7.1%	92.9%
	21-34	5.5%	2.7%	9.6%	21.9%	60.3%
	35-49	1.4%	5.2%	18.2%	17.2%	58.1%
	50-64	2.5%	7.9%	12.1%	16.4%	61.0%
	65 up	2.8%	3.5%	4.9%	11.3%	77.5%
Total		2.4%	5.6%	12.4%	16.0%	63.6%

Income * Drunk Driving Rating Crosstabulation

% within Income

		Drunk Driving Rating				
		Not at all important	2	3	4	Very important
Income	Under \$40,000	3.3%	0.7%	11.3%	12.0%	72.7%
	\$40,000-80,000	0.7%	5.4%	11.8%	19.3%	62.8%
	Over \$80,000	3.0%	7.6%	13.4%	14.7%	61.3%
Total		2.2%	5.5%	12.4%	15.9%	64.0%

Gender * Drunk Driving Rating Crosstabulation

% within Gender

		Drunk Driving Rating				
		Not at all important	2	3	4	Very important
Gender	Male	3.0%	8.5%	14.0%	18.9%	55.6%
	Female	1.9%	2.8%	11.1%	13.7%	70.4%
Total		2.4%	5.5%	12.5%	16.2%	63.3%

District * Drunk Driving Rating Crosstabulation

% within District

		Drunk Driving Rating				
		Not at all important	2	3	4	Very important
District	One	2.0%	4.1%	10.5%	19.4%	63.9%
	Two	1.7%	5.3%	13.3%	14.6%	65.1%
	Three	3.6%	7.2%	13.7%	14.7%	60.9%
Total		2.4%	5.5%	12.5%	16.2%	63.3%

Phone Type * Drunk Driving Rating Crosstabulation

% within Phone Type

		Drunk Driving Rating				
		Not at all important	2	3	4	Very important
Phone Type	Land	2.0%	5.8%	12.6%	15.3%	64.3%
	Cell	4.1%	4.6%	12.2%	19.4%	59.7%
Total		2.4%	5.5%	12.5%	16.2%	63.3%

Age * Fuel Tax Rating Crosstabulation

% within Age

		Fuel Tax Rating				
		Not at all important	2	3	4	Very important
Age	16-20	7.1%	14.3%	35.7%	28.6%	14.3%
	21-34	12.3%	8.2%	45.2%	12.3%	21.9%
	35-49	10.7%	9.3%	30.6%	20.3%	29.2%
	50-64	10.2%	9.9%	28.0%	19.5%	32.5%
	65 up	12.7%	9.2%	31.0%	12.7%	34.5%
Total		10.8%	9.6%	31.0%	18.4%	30.3%

Income * Fuel Tax Rating Crosstabulation

% within Income

		Fuel Tax Rating				
		Not at all important	2	3	4	Very important
Income	Under \$40,000	10.0%	8.0%	33.3%	14.7%	34.0%
	\$40,000-80,000	8.8%	9.5%	31.1%	19.6%	31.1%
	Over \$80,000	12.5%	11.2%	27.8%	19.9%	28.6%
Total		10.7%	10.0%	30.0%	18.8%	30.5%

Gender * Fuel Tax Rating Crosstabulation

% within Gender

		Fuel Tax Rating				
		Not at all important	2	3	4	Very important
Gender	Male	12.9%	10.6%	27.6%	17.0%	32.0%
	Female	8.8%	8.6%	34.0%	19.1%	29.6%
Total		10.8%	9.5%	30.9%	18.1%	30.7%

District * Fuel Tax Rating Crosstabulation

% within District

		Fuel Tax Rating				
		Not at all important	2	3	4	Very important
District	One	14.3%	11.6%	29.3%	16.7%	28.2%
	Two	8.6%	10.6%	32.6%	18.6%	29.6%
	Three	9.4%	6.5%	30.9%	18.9%	34.2%
Total		10.8%	9.5%	30.9%	18.1%	30.7%

Phone Type * Fuel Tax Rating Crosstabulation

% within Phone Type

		Fuel Tax Rating				
		Not at all important	2	3	4	Very important
Phone Type	Land	10.3%	9.1%	31.6%	18.3%	30.7%
	Cell	12.2%	11.2%	28.6%	17.3%	30.6%
Total		10.8%	9.5%	30.9%	18.1%	30.7%

	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Impaired Driving</i>				
5. What do you think is the best way to solve the drunk driving problem? (open-ended)				
More severe penalties	29	29	34	24
Education	17	16	16	18
Stricter law enforcement	19	20	14	22
High visibility law enforcement	6	6	6	7
Mandatory sentences for convictions	4	3	4	4
Others (bar responsibility, designated driver, etc.)	26	26	26	26
6. Do you think the Nebraska penalties for drunk driving are:				
Too tough	5	3	5	7
About right	40	40	37	44
Not tough enough	55	57	59	49
7-12. Would you favor or oppose each of the following penalties for drunk driving: (rotated)				
Jail terms for previous offenses				
Favor	81	80	81	82
Don't know	5	4	6	4
Opposed	14	16	13	14
Mandatory treatment for drunk driving offenders				
Favor	75	76	74	76
Don't know	4	4	6	3
Opposed	20	20	20	21
Mandatory interlock ignition for all first-time offenders				
Favor	72	72	71	74
Don't know	3	2	3	3
Opposed	25	26	26	24
Mandatory sentencing for drunk driving offenses				
Favor	67	67	63	69
Don't know	7	5	9	7
Opposed	26	27	28	24
Eliminating plea bargaining for drunk driving offenses				
Favor	61	63	58	62
Don't know	6	5	8	4
Opposed	33	31	34	34
Lose license for first offense				
Favor	52	50	50	55
Don't know	3	4	3	3
Opposed	45	46	48	41



Age * Solving Drunk Driving Crosstabulation

% within Age

		Solving Drunk Driving						
		Severe penalties	Education	Stricter enforcement	High viz enforcement	Mandatory sentencing	Others	DK/NR
Age	16-20	14.3%	10.7%	10.7%	17.9%	3.6%	25.0%	17.9%
	21-34	16.4%	11.0%	9.6%	6.8%	4.1%	31.5%	20.5%
	35-49	23.7%	12.4%	17.5%	5.8%	1.4%	18.2%	21.0%
	50-64	26.0%	15.5%	14.7%	3.7%	3.4%	17.5%	19.2%
	65 up	19.7%	10.6%	12.0%	2.1%	4.2%	23.2%	28.2%
Total		23.1%	13.2%	14.6%	4.8%	2.9%	20.0%	21.3%

Income * Solving Drunk Driving Crosstabulation

% within Income

		Solving Drunk Driving						
		Severe penalties	Education	Stricter enforce	High viz enforce	Mand. sentence	Others	DK/NR
Income	Under \$40,000	21.3%	11.3%	12.0%	4.7%	2.7%	25.3%	22.7%
	\$40,000-80,000	22.3%	9.8%	14.2%	3.7%	3.7%	23.0%	23.3%
	Over \$80,000	26.2%	15.0%	14.4%	6.0%	2.7%	15.3%	20.4%
Total		23.9%	12.4%	13.9%	4.9%	3.1%	19.9%	21.9%

Gender * Solving Drunk Driving Crosstabulation

% within Gender

		Solving Drunk Driving						
		More severe penalties	Education	Stricter enforcement	High viz enforcement	Mandatory sentencing	Others	DK/NR
Gender	Male	21.4%	14.5%	13.1%	4.1%	2.8%	20.7%	23.4%
	Female	24.8%	12.0%	15.8%	5.4%	3.0%	19.7%	19.3%
Total		23.2%	13.2%	14.5%	4.8%	2.9%	20.2%	21.3%

District * Solving Drunk Driving Crosstabulation

% within District

		Solving Drunk Driving						
		More severe penalties	Education	Stricter enforcement	High viz enforcement	Mandatory sentencing	Others	DK/NR
District	One	23.8%	13.3%	16.0%	4.8%	2.4%	20.7%	19.0%
	Two	28.2%	13.3%	12.0%	4.7%	3.7%	21.3%	16.9%
	Three	17.6%	13.0%	15.6%	4.9%	2.6%	18.6%	27.7%
Total		23.2%	13.2%	14.5%	4.8%	2.9%	20.2%	21.3%

Phone Type * Solving Drunk Driving Crosstabulation

% within Phone Type

		Solving Drunk Driving						
		More severe penalties	Education	Stricter enforcement	High viz enforcement	Mandatory sentencing	Others	DK/NR
Phone Type	Land	23.1%	12.9%	14.4%	4.8%	3.1%	19.0%	22.7%
	Cell	23.5%	14.3%	14.8%	4.6%	2.0%	24.5%	16.3%
Total		23.2%	13.2%	14.5%	4.8%	2.9%	20.2%	21.3%

Age * Drunk Driving Penalties Crosstabulation

% within Age

		Drunk Driving Penalties		
		Too tough	About right	Not tough enough
Age	16-20		32.1%	67.9%
	21-34	6.8%	47.9%	45.2%
	35-49	3.8%	43.3%	52.9%
	50-64	6.5%	42.4%	51.1%
	65 up	4.2%	25.4%	70.4%
Total		5.1%	40.1%	54.8%

Income * Drunk Driving Penalties Crosstabulation

% within Income

		Drunk Driving Penalties		
		Too tough	About right	Not tough enough
Income	Under \$40,000	5.3%	42.0%	52.7%
	\$40,000-80,000	4.7%	39.9%	55.4%
	Over \$80,000	4.6%	38.7%	56.7%
Total		4.8%	39.7%	55.5%

Gender * Drunk Driving Penalties Crosstabulation

% within Gender

		Drunk Driving Penalties		
		Too tough	About right	Not tough enough
Gender	Male	6.7%	46.0%	47.4%
	Female	3.4%	34.9%	61.7%
Total		5.0%	40.2%	54.8%

District * Drunk Driving Penalties Crosstabulation

% within District

		Drunk Driving Penalties		
		Too tough	About right	Not tough enough
District	One	3.4%	39.8%	56.8%
	Two	4.7%	36.5%	58.8%
	Three	6.8%	44.3%	48.9%
Total		5.0%	40.2%	54.8%

Phone Type * Drunk Driving Penalties Crosstabulation

% within Phone Type

		Drunk Driving Penalties		
		Too tough	About right	Not tough enough
Phone Type	Land	5.0%	39.0%	56.1%
	Cell	5.1%	44.9%	50.0%
Total		5.0%	40.2%	54.8%

Age * Jail Crosstabulation

% within Age

		Jail		
		Favor	Don't know	Oppose
Age	16-20	85.7%		14.3%
	21-34	82.2%	1.4%	16.4%
	35-49	83.2%	4.1%	12.7%
	50-64	80.2%	5.6%	14.1%
	65 up	79.6%	6.3%	14.1%
Total		81.4%	4.7%	13.9%

Income * Jail Crosstabulation

% within Income

		Jail		
		Favor	Don't know	Oppose
Income	Under \$40,000	86.0%	2.7%	11.3%
	\$40,000-80,000	78.0%	5.7%	16.2%
	Over \$80,000	82.6%	3.5%	13.9%
Total		81.5%	4.2%	14.3%

Gender * Jail Crosstabulation

% within Gender

		Jail		
		Favor	Don't know	Oppose
Gender	Male	78.9%	3.2%	17.9%
	Female	83.1%	6.0%	10.9%
Total		81.0%	4.7%	14.3%

District * Jail Crosstabulation

% within District

		Jail		
		Favor	Don't know	Oppose
District	One	79.9%	4.1%	16.0%
	Two	81.1%	6.0%	13.0%
	Three	82.1%	3.9%	14.0%
Total		81.0%	4.7%	14.3%

Phone Type * Jail Crosstabulation

% within Phone Type

		Jail		
		Favor	Don't know	Oppose
Phone Type	Land	79.7%	5.4%	14.9%
	Cell	85.7%	2.0%	12.2%
Total		81.0%	4.7%	14.3%

Age * Mandatory Treatment Crosstabulation

% within Age

		Mandatory Treatment		
		Favor	Don't know	Oppose
Age	16-20	75.0%	7.1%	17.9%
	21-34	78.1%	1.4%	20.5%
	35-49	73.9%	2.4%	23.7%
	50-64	71.8%	5.4%	22.9%
	65 up	85.2%	7.0%	7.7%
Total		75.2%	4.4%	20.4%

Income * Mandatory Treatment Crosstabulation

% within Income

		Mandatory Treatment		
		Favor	Don't know	Oppose
Income	Under \$40,000	80.7%	4.0%	15.3%
	\$40,000-80,000	77.4%	3.0%	19.6%
	Over \$80,000	72.5%	3.5%	24.0%
Total		75.8%	3.4%	20.8%

Gender * Mandatory Treatment Crosstabulation

% within Gender

		Mandatory Treatment		
		Favor	Don't know	Oppose
Gender	Male	71.3%	3.9%	24.8%
	Female	79.0%	4.7%	16.3%
Total		75.3%	4.3%	20.4%

District * Mandatory Treatment Crosstabulation

% within District

		Mandatory Treatment		
		Favor	Don't know	Oppose
District	One	75.5%	4.1%	20.4%
	Two	74.1%	6.3%	19.6%
	Three	76.2%	2.6%	21.2%
Total		75.3%	4.3%	20.4%

Phone Type * Mandatory Treatment Crosstabulation

% within Phone Type

		Mandatory Treatment		
		Favor	Don't know	Oppose
Phone Type	Land	76.3%	4.4%	19.3%
	Cell	71.4%	4.1%	24.5%
Total		75.3%	4.3%	20.4%

Age * Mandatory Interlock Ignition Crosstabulation

% within Age

		Mandatory Interlock Ignition		
		Favor	Don't know	Oppose
Age	16-20	71.4%	10.7%	17.9%
	21-34	72.6%	2.7%	24.7%
	35-49	77.3%	1.4%	21.3%
	50-64	67.2%	2.8%	29.9%
	65 up	75.4%	2.8%	21.8%
Total		72.4%	2.6%	25.0%

Income * Mandatory Interlock Ignition Crosstabulation

% within Income

		Mandatory Interlock Ignition		
		Favor	Don't know	Oppose
Income	Under \$40,000	70.7%	2.0%	27.3%
	\$40,000-80,000	70.9%	2.7%	26.4%
	Over \$80,000	74.9%	1.9%	23.2%
Total		72.7%	2.2%	25.1%

Gender * Mandatory Interlock Ignition Crosstabulation

% within Gender

		Mandatory Interlock Ignition		
		Favor	Don't know	Oppose
Gender	Male	68.7%	2.3%	29.0%
	Female	75.6%	2.8%	21.6%
Total		72.3%	2.5%	25.2%

District * Mandatory Interlock Ignition Crosstabulation

% within District

		Mandatory Interlock Ignition		
		Favor	Don't know	Oppose
District	One	71.8%	2.0%	26.2%
	Two	71.1%	3.0%	25.9%
	Three	73.9%	2.6%	23.5%
Total		72.3%	2.5%	25.2%

Phone Type * Mandatory Interlock Ignition Crosstabulation

% within Phone Type

		Mandatory Interlock Ignition		
		Favor	Don't know	Oppose
Phone Type	Land	71.1%	2.7%	26.2%
	Cell	76.5%	2.0%	21.4%
Total		72.3%	2.5%	25.2%

Age * Mandatory Sentencing Crosstabulation

% within Age

		Mandatory Sentencing		
		Favor	Don't know	Oppose
Age	16-20	75.0%		25.0%
	21-34	74.0%	4.1%	21.9%
	35-49	64.3%	8.2%	27.5%
	50-64	64.4%	6.8%	28.8%
	65 up	70.4%	9.2%	20.4%
Total		66.4%	7.2%	26.4%

Income * Mandatory Sentencing Crosstabulation

% within Income

		Mandatory Sentencing		
		Favor	Don't know	Oppose
Income	Under \$40,000	76.7%	2.7%	20.7%
	\$40,000-80,000	66.9%	7.1%	26.0%
	Over \$80,000	63.2%	6.8%	30.0%
Total		67.0%	6.2%	26.8%

Gender * Mandatory Sentencing Crosstabulation

% within Gender

		Mandatory Sentencing		
		Favor	Don't know	Oppose
Gender	Male	61.1%	6.0%	32.9%
	Female	71.7%	8.1%	20.1%
Total		66.6%	7.1%	26.3%

District * Mandatory Sentencing Crosstabulation

% within District

		Mandatory Sentencing		
		Favor	Don't know	Oppose
District	One	67.3%	5.4%	27.2%
	Two	63.1%	9.3%	27.6%
	Three	69.4%	6.5%	24.1%
Total		66.6%	7.1%	26.3%

Phone Type * Mandatory Sentencing Crosstabulation

% within Phone Type

		Mandatory Sentencing		
		Favor	Don't know	Oppose
Phone Type	Land	65.3%	7.9%	26.8%
	Cell	71.4%	4.1%	24.5%
Total		66.6%	7.1%	26.3%

Age * No Plea Bargaining Crosstabulation

% within Age

		No Plea Bargaining		
		Favor	Don't know	Oppose
Age	16-20	57.1%	3.6%	39.3%
	21-34	58.9%	5.5%	35.6%
	35-49	60.1%	6.2%	33.7%
	50-64	59.6%	7.1%	33.3%
	65 up	66.2%	4.2%	29.6%
Total		60.7%	6.1%	33.2%

Income * No Plea Bargaining Crosstabulation

% within Income

		No Plea Bargaining		
		Favor	Don't know	Oppose
Income	Under \$40,000	56.7%	7.3%	36.0%
	\$40,000-80,000	58.4%	5.1%	36.5%
	Over \$80,000	64.9%	5.4%	29.7%
Total		61.0%	5.7%	33.3%

Gender * No Plea Bargaining Crosstabulation

% within Gender

		No Plea Bargaining		
		Favor	Don't know	Oppose
Gender	Male	58.6%	4.4%	37.0%
	Female	63.2%	7.5%	29.3%
Total		61.0%	6.0%	33.0%

District * No Plea Bargaining Crosstabulation

% within District

		No Plea Bargaining		
		Favor	Don't know	Oppose
District	One	63.3%	5.4%	31.3%
	Two	58.1%	8.3%	33.6%
	Three	61.6%	4.2%	34.2%
Total		61.0%	6.0%	33.0%

Phone Type * No Plea Bargaining Crosstabulation

% within Phone Type

		No Plea Bargaining		
		Favor	Don't know	Oppose
Phone Type	Land	60.3%	6.4%	33.3%
	Cell	63.3%	4.6%	32.1%
Total		61.0%	6.0%	33.0%

Age * Lose License Crosstabulation

% within Age

		Lose License		
		Favor	Don't know	Oppose
Age	16-20	64.3%		35.7%
	21-34	47.9%	2.7%	49.3%
	35-49	53.3%	2.7%	44.0%
	50-64	51.4%	4.0%	44.6%
	65 up	50.0%	4.2%	45.8%
Total		51.9%	3.4%	44.7%

Income * Lose License Crosstabulation

% within Income

		Lose License		
		Favor	Don't know	Oppose
Income	Under \$40,000	50.0%	2.0%	48.0%
	\$40,000-80,000	51.0%	3.0%	45.9%
	Over \$80,000	52.3%	3.3%	44.4%
Total		51.4%	3.0%	45.6%

Gender * Lose License Crosstabulation

% within Gender

		Lose License		
		Favor	Don't know	Oppose
Gender	Male	51.0%	3.0%	46.0%
	Female	52.5%	3.6%	43.9%
Total		51.8%	3.3%	44.9%

District * Lose License Crosstabulation

% within District

		Lose License		
		Favor	Don't know	Oppose
District	One	50.0%	4.1%	45.9%
	Two	49.8%	2.7%	47.5%
	Three	55.4%	3.3%	41.4%
Total		51.8%	3.3%	44.9%

Phone Type * Lose License Crosstabulation

% within Phone Type

		Lose License		
		Favor	Don't know	Oppose
Phone Type	Land	50.6%	3.4%	46.0%
	Cell	56.1%	3.1%	40.8%
Total		51.8%	3.3%	44.9%

	State	Percentage		
		Dist One	Dist Two	Dist Three
13. In the last 60 days, have you ever driven while impaired by alcohol, but still under the legal limit?				
Yes	11	10	12	11
No	89	90	88	90
14. What do you think the chances are of someone getting arrested if they drive after drinking? (Core Q. A-3)				
Very likely	10	9	9	13
Somewhat likely	34	33	33	35
Likely	16	16	16	16
Somewhat unlikely	30	32	33	25
Very unlikely	10	9	11	11
15. In the past 60 days, how many times have you driven a motor vehicle within 2 hours after drinking alcoholic beverages? (Core Q. A-1)				
Never	76	78	71	79
Once	11	13	13	8
2-5 times	10	7	13	11
More than 5 times	2	3	2	2
16. In the past 30 days, have you read, seen or heard anything about alcohol impaired driving (or drunk driving) enforcement by police? (Core Q. A-2)				
Yes	48	45	47	52
No	52	55	53	48
17. In the past 60 days, have you read, seen or heard any drunk driving-related messages?				
Yes	53	57	51	51
No	47	43	49	49



Age * Driven While Impaired Crosstabulation

% within Age

		Driven While Impaired	
		Yes	No
Age	16-20	3.6%	96.4%
	21-34	13.7%	86.3%
	35-49	11.8%	88.2%
	50-64	11.6%	88.4%
	65 up	4.9%	95.1%
Total		10.5%	89.5%

Income * Driven While Impaired Crosstabulation

% within Income

		Driven While Impaired	
		Yes	No
Income	Under \$40,000	6.0%	94.0%
	\$40,000-80,000	7.1%	92.9%
	Over \$80,000	13.2%	86.8%
Total		9.6%	90.4%

Gender * Driven While Impaired Crosstabulation

% within Gender

		Driven While Impaired	
		Yes	No
Gender	Male	13.6%	86.4%
	Female	8.0%	92.0%
Total		10.7%	89.3%

District * Driven While Impaired Crosstabulation

% within District

		Driven While Impaired	
		Yes	No
District	One	9.9%	90.1%
	Two	11.7%	88.3%
	Three	10.5%	89.5%
Total		10.7%	89.3%

Phone Type * Driven While Impaired Crosstabulation

% within Phone Type

		Driven While Impaired	
		Yes	No
Phone Type	Land	10.2%	89.8%
	Cell	12.3%	87.7%
Total		10.7%	89.3%

Age * Drunk Driving Arrest Chance Crosstabulation

% within Age

		Drunk Driving Arrest Chance				
		Very likely	Somewhat likely	Likely	Somewhat unlikely	Very unlikely
Age	16-20	10.7%	42.9%	21.4%	17.9%	7.1%
	21-34	6.8%	34.2%	16.4%	35.6%	6.8%
	35-49	8.6%	33.7%	16.2%	32.3%	9.3%
	50-64	9.6%	35.9%	15.5%	29.9%	9.0%
	65 up	14.8%	26.1%	14.8%	26.8%	17.6%
Total		9.9%	33.7%	15.9%	30.3%	10.2%

Income * Drunk Driving Arrest Chance Crosstabulation

% within Income

		Drunk Driving Arrest Chance				
		Very likely	Somewhat likely	Likely	Somewhat unlikely	Very unlikely
Income	Under \$40,000	12.0%	34.0%	18.0%	28.0%	8.0%
	\$40,000-80,000	7.8%	36.1%	13.9%	30.4%	11.8%
	Over \$80,000	9.3%	32.7%	15.0%	34.1%	9.0%
Total		9.2%	34.2%	15.1%	31.6%	9.8%

Gender * Drunk Driving Arrest Chance Crosstabulation

% within Gender

		Drunk Driving Arrest Chance				
		Very likely	Somewhat likely	Likely	Somewhat unlikely	Very unlikely
Gender	Male	9.4%	32.6%	16.8%	29.2%	12.0%
	Female	10.9%	34.7%	15.2%	30.4%	8.8%
Total		10.2%	33.7%	16.0%	29.8%	10.3%

District * Drunk Driving Arrest Chance Crosstabulation

% within District

		Drunk Driving Arrest Chance				
		Very likely	Somewhat likely	Likely	Somewhat unlikely	Very unlikely
District	One	9.2%	33.3%	16.0%	32.3%	9.2%
	Two	8.6%	32.6%	15.6%	32.6%	10.6%
	Three	12.7%	35.2%	16.3%	24.8%	11.1%
Total		10.2%	33.7%	16.0%	29.8%	10.3%

Phone Type * Drunk Driving Arrest Chance Crosstabulation

% within Phone Type

		Drunk Driving Arrest Chance				
		Very likely	Somewhat likely	Likely	Somewhat unlikely	Very unlikely
Phone Type	Land	10.3%	34.3%	15.3%	29.5%	10.6%
	Cell	9.7%	31.6%	18.4%	31.1%	9.2%
Total		10.2%	33.7%	16.0%	29.8%	10.3%

Age * Driven after 2 hrs of Drinking Crosstabulation

% within Age

		Driven after 2 hrs of Drinking			
		Never	Once	2-5 times	More than 5 times
Age	16-20	96.4%	3.6%		
	21-34	63.0%	13.7%	19.2%	4.1%
	35-49	73.9%	10.3%	13.7%	2.1%
	50-64	75.4%	14.7%	8.2%	1.7%
	65 up	85.2%	6.3%	4.9%	3.5%
Total		76.1%	11.5%	10.1%	2.3%

Income * Driven after 2 hrs of Drinking Crosstabulation

% within Income

		Driven after 2 hrs of Drinking			
		Never	Once	2-5 times	More than 5 times
Income	Under \$40,000	84.7%	8.0%	5.3%	2.0%
	\$40,000-80,000	79.4%	9.8%	9.1%	1.7%
	Over \$80,000	68.7%	15.0%	13.6%	2.7%
Total		75.5%	11.8%	10.5%	2.2%

Gender * Driven after 2 hrs of Drinking Crosstabulation

% within Gender

		Driven after 2 hrs of Drinking			
		Never	Once	2-5 times	More than 5 times
Gender	Male	71.7%	11.7%	12.9%	3.7%
	Female	80.1%	10.9%	7.9%	1.1%
Total		76.1%	11.3%	10.3%	2.3%

District * Driven after 2 hrs of Drinking Crosstabulation

% within District

		Driven after 2 hrs of Drinking			
		Never	Once	2-5 times	More than 5 times
District	One	78.2%	12.6%	6.5%	2.7%
	Two	71.4%	13.3%	13.3%	2.0%
	Three	78.5%	8.1%	11.1%	2.3%
Total		76.1%	11.3%	10.3%	2.3%

Phone Type * Driven after 2 hrs of Drinking Crosstabulation

% within Phone Type

		Driven after 2 hrs of Drinking			
		Never	Once	2-5 times	More than 5 times
Phone Type	Land	77.6%	11.5%	9.1%	1.8%
	Cell	70.4%	10.7%	14.8%	4.1%
Total		76.1%	11.3%	10.3%	2.3%

Age * Drunk Driving Enforcement Awareness Crosstabulation

% within Age

		Drunk Driving Enforcement Awareness	
		Yes	No
Age	16-20	42.9%	57.1%
	21-34	45.2%	54.8%
	35-49	47.9%	52.1%
	50-64	50.0%	50.0%
	65 up	47.5%	52.5%
Total		48.3%	51.7%

Income * Drunk Driving Enforcement Awareness Crosstabulation

% within Income

		Drunk Driving Enforcement Awareness	
		Yes	No
Income	Under \$40,000	40.3%	59.7%
	\$40,000-80,000	46.9%	53.1%
	Over \$80,000	51.6%	48.4%
Total		47.8%	52.2%

Gender * Drunk Driving Enforcement Awareness Crosstabulation

% within Gender

		Drunk Driving Enforcement Awareness	
		Yes	No
Gender	Male	49.8%	50.2%
	Female	46.3%	53.7%
Total		48.0%	52.0%

District * Drunk Driving Enforcement Awareness Crosstabulation

% within District

		Drunk Driving Enforcement Awareness	
		Yes	No
District	One	44.7%	55.3%
	Two	47.3%	52.7%
	Three	51.8%	48.2%
Total		48.0%	52.0%

Phone Type * Drunk Driving Enforcement Awareness Crosstabulation

% within Phone Type

		Drunk Driving Enforcement Awareness	
		Yes	No
Phone Type	Land	48.9%	51.1%
	Cell	44.6%	55.4%
Total		48.0%	52.0%

Age * Drunk Driving Awareness Crosstabulation

% within Age

		Drunk Driving Awareness	
		Yes	No
Age	16-20	39.3%	60.7%
	21-34	46.6%	53.4%
	35-49	54.0%	46.0%
	50-64	55.0%	45.0%
	65 up	52.2%	47.8%
Total		53.0%	47.0%

Income * Drunk Driving Awareness Crosstabulation

% within Income

		Drunk Driving Awareness	
		Yes	No
Income	Under \$40,000	46.3%	53.7%
	\$40,000-80,000	54.8%	45.2%
	Over \$80,000	57.1%	42.9%
Total		54.2%	45.8%

Gender * Drunk Driving Awareness Crosstabulation

% within Gender

		Drunk Driving Awareness	
		Yes	No
Gender	Male	55.5%	44.5%
	Female	50.4%	49.6%
Total		52.9%	47.1%

District * Drunk Driving Awareness Crosstabulation

% within District

		Drunk Driving Awareness	
		Yes	No
District	One	56.8%	43.2%
	Two	50.7%	49.3%
	Three	51.1%	48.9%
Total		52.9%	47.1%

Phone Type * Drunk Driving Awareness Crosstabulation

% within Phone Type

		Drunk Driving Awareness	
		Yes	No
Phone Type	Land	54.5%	45.5%
	Cell	47.2%	52.8%
Total		52.9%	47.1%

	State	Percentage		
		Dist One	Dist Two	Dist Three
Seat Belts				
18. How often do you use safety belts when you drive or ride in a car, van, sport utility vehicle or pickup? (Core Q. B-1)				
Always	76	83	84	63
Nearly always	15	12	9	24
Sometimes	5	3	4	7
Seldom	2	1	1	4
Never	2	1	2	3
19. Should law enforcement officers be allowed to stop drivers and ticket them for not wearing a seat belt?				
Yes	54	52	55	54
Don't know	2	2	2	2
No	44	45	44	44
20. What do you think the chances are of getting a ticket if you don't wear your safety belt? (Core Q. B-3)				
Very likely	9	6	9	10
Somewhat likely	20	20	17	23
Likely	12	10	10	15
Somewhat unlikely	32	33	35	29
Very unlikely	28	30	30	23
21. What level of fine do you think would be most effective way to get people to wear their seat belt all the time?				
\$25-50	35	33	35	37
\$51-100	28	27	28	30
\$101-150	13	14	11	15
\$151-200	5	7	5	3
Over \$200	19	19	22	15
22. Do you have any children weighing between 40 and 80 pounds?				
Yes	24	25	22	24
No	76	75	78	76
23. (IF YES) Do you use a booster seat for your child in the car?				
Yes	71	74	67	73
No	29	26	33	27
24. In the past 60 days, have you read, seen or heard anything about seat belt law enforcement by police? (Core Q. B-2)				
Yes	27	21	24	34
No	73	79	76	66



Age * Seat Belt Use Crosstabulation

% within Age

		Seat Belt Use				
		Always	Nearly always	Sometimes	Seldom	Never
Age	16-20	67.9%	17.9%	10.7%	3.6%	
	21-34	68.5%	21.9%	5.5%	1.4%	2.7%
	35-49	78.7%	13.7%	4.5%	2.7%	0.3%
	50-64	74.6%	15.3%	5.6%	2.0%	2.5%
	65 up	84.5%	11.3%	1.4%	0.7%	2.1%
Total		76.8%	14.8%	4.7%	2.0%	1.7%

Income * Seat Belt Use Crosstabulation

% within Income

		Seat Belt Use				
		Always	Nearly always	Sometimes	Seldom	Never
Income	Under \$40,000	73.3%	16.0%	3.3%	4.0%	3.3%
	\$40,000-80,000	75.0%	15.9%	5.7%	2.0%	1.4%
	Over \$80,000	78.2%	15.0%	4.4%	1.1%	1.4%
Total		76.1%	15.5%	4.7%	2.0%	1.7%

Gender * Seat Belt Use Crosstabulation

% within Gender

		Seat Belt Use				
		Always	Nearly always	Sometimes	Seldom	Never
Gender	Male	69.4%	16.8%	6.9%	3.2%	3.7%
	Female	82.9%	13.3%	2.8%	0.9%	0.2%
Total		76.4%	15.0%	4.8%	2.0%	1.9%

District * Seat Belt Use Crosstabulation

% within District

		Seat Belt Use				
		Always	Nearly always	Sometimes	Seldom	Never
District	One	83.0%	11.6%	2.7%	1.4%	1.4%
	Two	83.7%	9.3%	4.3%	1.0%	1.7%
	Three	62.9%	23.8%	7.2%	3.6%	2.6%
Total		76.4%	15.0%	4.8%	2.0%	1.9%

Phone Type * Seat Belt Use Crosstabulation

% within Phone Type

		Seat Belt Use				
		Always	Nearly always	Sometimes	Seldom	Never
Phone Type	Land	78.2%	14.0%	3.8%	2.3%	1.7%
	Cell	69.9%	18.4%	8.2%	1.0%	2.6%
Total		76.4%	15.0%	4.8%	2.0%	1.9%

Age * Ticket For Not Buckling Up Crosstabulation

% within Age

		Ticket For Not Buckling Up		
		Yes	Don't know	No
Age	16-20	50.0%		50.0%
	21-34	47.9%	1.4%	50.7%
	35-49	53.3%	0.3%	46.4%
	50-64	49.7%	2.3%	48.0%
	65 up	69.0%	5.6%	25.4%
Total		53.8%	2.0%	44.1%

Income * Ticket For Not Buckling Up Crosstabulation

% within Income

		Ticket For Not Buckling Up		
		Yes	Don't know	No
Income	Under \$40,000	54.7%	2.0%	43.3%
	\$40,000-80,000	49.7%	2.0%	48.3%
	Over \$80,000	54.2%	1.9%	43.9%
Total		52.6%	2.0%	45.4%

Gender * Ticket For Not Buckling Up Crosstabulation

% within Gender

		Ticket For Not Buckling Up		
		Yes	Don't know	No
Gender	Male	46.9%	1.6%	51.5%
	Female	60.0%	2.4%	37.7%
Total		53.7%	2.0%	44.3%

District * Ticket For Not Buckling Up Crosstabulation

% within District

		Ticket For Not Buckling Up		
		Yes	Don't know	No
District	One	52.4%	2.4%	45.2%
	Two	54.8%	1.7%	43.5%
	Three	53.7%	2.0%	44.3%
Total		53.7%	2.0%	44.3%

Phone Type * Ticket For Not Buckling Up Crosstabulation

% within Phone Type

		Ticket For Not Buckling Up		
		Yes	Don't know	No
Phone Type	Land	54.4%	2.3%	43.3%
	Cell	51.0%	1.0%	48.0%
Total		53.7%	2.0%	44.3%

Age * Seat Belt Violation Ticket Chance Crosstabulation

% within Age

		Seat Belt Violation Ticket Chance				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
Age	16-20	7.1%	28.6%	10.7%	28.6%	25.0%
	21-34	11.0%	20.5%	12.3%	26.0%	30.1%
	35-49	9.3%	19.6%	9.6%	34.4%	27.1%
	50-64	6.5%	19.5%	11.3%	31.9%	30.8%
	65 up	11.3%	21.1%	16.2%	31.0%	20.4%
Total		8.6%	20.2%	11.6%	32.0%	27.7%

Income * Seat Belt Violation Ticket Chance Crosstabulation

% within Income

		Seat Belt Violation Ticket Chance				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
Income	Under \$40,000	14.7%	25.3%	14.0%	30.0%	16.0%
	\$40,000-80,000	5.7%	20.6%	11.8%	35.8%	26.0%
	Over \$80,000	5.7%	18.5%	9.0%	33.0%	33.8%
Total		7.4%	20.5%	10.9%	33.5%	27.7%

Gender * Seat Belt Violation Ticket Chance Crosstabulation

% within Gender

		Seat Belt Violation Ticket Chance				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
Gender	Male	8.5%	17.7%	9.2%	33.8%	30.8%
	Female	8.8%	22.1%	13.7%	30.8%	24.6%
Total		8.6%	20.0%	11.5%	32.3%	27.6%

District * Seat Belt Violation Ticket Chance Crosstabulation

% within District

		Seat Belt Violation Ticket Chance				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
District	One	6.1%	20.1%	10.2%	33.3%	30.3%
	Two	9.3%	16.6%	9.6%	34.6%	29.9%
	Three	10.4%	23.1%	14.7%	29.0%	22.8%
Total		8.6%	20.0%	11.5%	32.3%	27.6%

Phone Type * Seat Belt Violation Ticket Chance Crosstabulation

% within Phone Type

		Seat Belt Violation Ticket Chance				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
Phone Type	Land	8.2%	20.0%	11.5%	31.9%	28.5%
	Cell	10.2%	19.9%	11.7%	33.7%	24.5%
Total		8.6%	20.0%	11.5%	32.3%	27.6%

Age * Seat Belt Fine Crosstabulation

% within Age

		Seat Belt Fine				
		\$25-50	\$51-100	\$101-150	\$151-200	Over \$200
Age	16-20	28.6%	28.6%	10.7%	10.7%	21.4%
	21-34	37.0%	30.1%	15.1%	2.7%	15.1%
	35-49	30.2%	28.9%	12.7%	6.2%	22.0%
	50-64	36.7%	26.3%	14.7%	5.1%	17.2%
	65 up	39.4%	31.7%	10.6%	2.1%	16.2%
Total		34.8%	28.4%	13.3%	5.0%	18.6%

Income * Seat Belt Fine Crosstabulation

% within Income

		Seat Belt Fine				
		\$25-50	\$51-100	\$101-150	\$151-200	Over \$200
Income	Under \$40,000	37.3%	28.7%	11.3%	6.0%	16.7%
	\$40,000-80,000	35.8%	31.1%	14.2%	3.7%	15.2%
	Over \$80,000	30.5%	25.6%	15.0%	5.2%	23.7%
Total		33.7%	28.2%	14.0%	4.8%	19.3%

Gender * Seat Belt Fine Crosstabulation

% within Gender

		Seat Belt Fine				
		\$25-50	\$51-100	\$101-150	\$151-200	Over \$200
Gender	Male	37.9%	26.7%	14.5%	3.7%	17.2%
	Female	32.1%	29.6%	12.4%	6.0%	19.9%
Total		34.9%	28.2%	13.4%	4.9%	18.6%

District * Seat Belt Fine Crosstabulation

% within District

		Seat Belt Fine				
		\$25-50	\$51-100	\$101-150	\$151-200	Over \$200
District	One	33.0%	26.5%	14.3%	6.8%	19.4%
	Two	34.6%	27.6%	11.3%	5.0%	21.6%
	Three	37.1%	30.3%	14.7%	2.9%	15.0%
Total		34.9%	28.2%	13.4%	4.9%	18.6%

Phone Type * Seat Belt Fine Crosstabulation

% within Phone Type

		Seat Belt Fine				
		\$25-50	\$51-100	\$101-150	\$151-200	Over \$200
Phone Type	Land	36.4%	25.8%	13.2%	5.2%	19.4%
	Cell	29.6%	36.7%	14.3%	3.6%	15.8%
Total		34.9%	28.2%	13.4%	4.9%	18.6%

Age * Have Kids 40-80 Pounds Crosstabulation

% within Age

		Have Kids 40-80 Pounds	
		Yes	No
Age	16-20	3.6%	96.4%
	21-34	31.5%	68.5%
	35-49	45.4%	54.6%
	50-64	11.9%	88.1%
	65 up	7.0%	93.0%
Total		23.4%	76.6%

Income * Have Kids 40-80 Pounds Crosstabulation

% within Income

		Have Kids 40-80 Pounds	
		Yes	No
Income	Under \$40,000	18.7%	81.3%
	\$40,000-80,000	21.6%	78.4%
	Over \$80,000	27.8%	72.2%
Total		23.9%	76.1%

Gender * Have Kids 40-80 Pounds Crosstabulation

% within Gender

		Have Kids 40-80 Pounds	
		Yes	No
Gender	Male	24.1%	75.9%
	Female	23.3%	76.7%
Total		23.7%	76.3%

District * Have Kids 40-80 Pounds Crosstabulation

% within District

		Have Kids 40-80 Pounds	
		Yes	No
District	One	24.8%	75.2%
	Two	22.3%	77.7%
	Three	24.1%	75.9%
Total		23.7%	76.3%

Phone Type * Have Kids 40-80 Pounds Crosstabulation

% within Phone Type

		Have Kids 40-80 Pounds	
		Yes	No
Phone Type	Land	22.9%	77.1%
	Cell	26.5%	73.5%
Total		23.7%	76.3%

Age * Use Booster Seat Crosstabulation

% within Age

		Use Booster Seat	
		Yes	No
Age	16-20	100.0%	
	21-34	91.3%	8.7%
	35-49	72.6%	27.4%
	50-64	55.6%	44.4%
	65 up	80.0%	20.0%
Total		71.5%	28.5%

Income * Use Booster Seat Crosstabulation

% within Income

		Use Booster Seat	
		Yes	No
Income	Under \$40,000	82.8%	17.2%
	\$40,000-80,000	74.2%	25.8%
	Over \$80,000	69.5%	30.5%
Total		73.0%	27.0%

Gender * Use Booster Seat Crosstabulation

% within Gender

		Use Booster Seat	
		Yes	No
Gender	Male	69.7%	30.3%
	Female	73.0%	27.0%
Total		71.4%	28.6%

District * Use Booster Seat Crosstabulation

% within District

		Use Booster Seat	
		Yes	No
District	One	74.0%	26.0%
	Two	67.1%	32.9%
	Three	72.7%	27.3%
Total		71.4%	28.6%

Phone Type * Use Booster Seat Crosstabulation

% within Phone Type

		Use Booster Seat	
		Yes	No
Phone Type	Land	68.7%	31.3%
	Cell	79.6%	20.4%
Total		71.4%	28.6%

Age * Seat Belt Enforcement Awareness Crosstabulation

% within Age

		Seat Belt Enforcement Awareness	
		Yes	No
Age	16-20	17.9%	82.1%
	21-34	25.0%	75.0%
	35-49	24.7%	75.3%
	50-64	26.6%	73.4%
	65 up	35.0%	65.0%
Total		26.9%	73.1%

Income * Seat Belt Enforcement Awareness Crosstabulation

% within Income

		Seat Belt Enforcement Awareness	
		Yes	No
Income	Under \$40,000	28.9%	71.1%
	\$40,000-80,000	25.9%	74.1%
	Over \$80,000	26.6%	73.4%
Total		26.7%	73.3%

Gender * Seat Belt Enforcement Awareness Crosstabulation

% within Gender

		Seat Belt Enforcement Awareness	
		Yes	No
Gender	Male	31.3%	68.8%
	Female	22.5%	77.5%
Total		26.7%	73.3%

District * Seat Belt Enforcement Awareness Crosstabulation

% within District

		Seat Belt Enforcement Awareness	
		Yes	No
District	One	21.3%	78.7%
	Two	24.1%	75.9%
	Three	34.4%	65.6%
Total		26.7%	73.3%

Phone Type * Seat Belt Enforcement Awareness Crosstabulation

% within Phone Type

		Seat Belt Enforcement Awareness	
		Yes	No
Phone Type	Land	27.5%	72.5%
	Cell	24.0%	76.0%
Total		26.7%	73.3%

	State	Percentage		
		Dist One	Dist Two	Dist Three
25. In the past 60 days, have you read, seen or heard any Click It Or Ticket seat belt messages?				
Yes	52	51	50	53
No	48	49	50	47
26. (IF YES) Do you remember where that was?				
TV	45	49	41	44
Radio	17	16	11	24
Billboards/roadside signs	21	18	29	15
Others (newspaper, internet, posters)	11	13	8	12
Don't know	7	4	12	5



Age * CIOT Awareness Crosstabulation

% within Age

		CIOT Awareness	
		Yes	No
Age	16-20	46.4%	53.6%
	21-34	52.8%	47.2%
	35-49	48.3%	51.7%
	50-64	53.6%	46.4%
	65 up	55.3%	44.7%
Total		51.8%	48.2%

Income * CIOT Awareness Crosstabulation

% within Income

		CIOT Awareness	
		Yes	No
Income	Under \$40,000	55.7%	44.3%
	\$40,000-80,000	48.6%	51.4%
	Over \$80,000	52.2%	47.8%
Total		51.5%	48.5%

Gender * CIOT Awareness Crosstabulation

% within Gender

		CIOT Awareness	
		Yes	No
Gender	Male	57.5%	42.5%
	Female	46.0%	54.0%
Total		51.6%	48.4%

District * CIOT Awareness Crosstabulation

% within District

		CIOT Awareness	
		Yes	No
District	One	51.4%	48.6%
	Two	50.2%	49.8%
	Three	53.1%	46.9%
Total		51.6%	48.4%

Phone Type * CIOT Awareness Crosstabulation

% within Phone Type

		CIOT Awareness	
		Yes	No
Phone Type	Land	51.9%	48.1%
	Cell	50.3%	49.7%
Total		51.6%	48.4%

Age * CIOT Recall Crosstabulation

% within Age

		CIOT Recall				
		Others	TV	Radio	Boards/road signs	Don't know
Age	16-20	7.7%	53.8%	23.1%	15.4%	
	21-34		26.3%	23.7%	39.5%	10.5%
	35-49	10.7%	48.6%	16.4%	17.1%	7.1%
	50-64	9.6%	45.7%	19.1%	20.2%	5.3%
	65 up	21.8%	41.0%	7.7%	20.5%	9.0%
Total		11.2%	44.4%	16.8%	20.8%	6.8%

Income * CIOT Recall Crosstabulation

% within Income

		CIOT Recall				
		Others	TV	Radio	Boards/road signs	Don't know
Income	Under \$40,000	16.9%	43.4%	15.7%	18.1%	6.0%
	\$40,000-80,000	14.8%	40.8%	21.1%	16.2%	7.0%
	Over \$80,000	7.3%	44.3%	16.1%	24.5%	7.8%
Total		11.8%	42.9%	17.7%	20.4%	7.2%

Gender * CIOT Recall Crosstabulation

% within Gender

		CIOT Recall				
		Others	TV	Radio	Boards/road signs	Don't know
Gender	Male	7.7%	46.0%	22.2%	17.7%	6.5%
	Female	15.0%	43.0%	10.7%	23.8%	7.5%
Total		11.0%	44.6%	16.9%	20.6%	6.9%

District * CIOT Recall Crosstabulation

% within District

		CIOT Recall				
		Others	TV	Radio	Boards/road signs	Don't know
District	One	13.3%	48.7%	16.0%	18.0%	4.0%
	Two	8.0%	40.7%	10.7%	28.7%	12.0%
	Three	11.7%	44.4%	23.5%	15.4%	4.9%
Total		11.0%	44.6%	16.9%	20.6%	6.9%

Phone Type * CIOT Recall Crosstabulation

% within Phone Type

		CIOT Recall				
		Others	TV	Radio	Boards/road signs	Don't know
Phone Type	Land	12.1%	46.2%	16.5%	18.7%	6.6%
	Cell	7.1%	38.8%	18.4%	27.6%	8.2%
Total		11.0%	44.6%	16.9%	20.6%	6.9%

	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Speed</i>				
27. On a local road with a speed limit of 30 mph, how often do you drive faster than 35 mph: (Core Q. S-1a)				
Most of the time	14	14	18	11
Half the time	17	15	18	17
Rarely	48	51	46	46
Never	21	20	18	26
28. On a local road with a speed limit of 65 mph, how often do you drive faster than 70 mph: (Core Q. S-1b)				
Most of the time	6	8	8	3
Half the time	10	9	13	8
Rarely	42	41	45	40
Never	42	42	35	49
29. In the past 30 days, have you read, seen or heard anything about speed enforcement by police? (Core Q. S-2)				
Yes	37	38	34	38
No	63	62	66	62
30. What do you think the chances are of getting a ticket if you drive over the speed limit? (Core Q. S-3)				
Very likely	16	12	15	21
Somewhat likely	36	37	37	32
Likely	25	26	21	29
Somewhat unlikely	19	22	21	13
Very unlikely	5	3	5	6



Age * Speed at Posted 35 Crosstabulation

% within Age

		Speed at Posted 35			
		Most times	Half of the time	Rarely	Never
Age	16-20	35.7%	17.9%	28.6%	17.9%
	21-34	28.8%	20.5%	34.2%	16.4%
	35-49	14.1%	16.8%	48.8%	20.3%
	50-64	11.0%	18.9%	50.6%	19.5%
	65 up	9.9%	9.9%	49.3%	31.0%
Total		14.1%	16.9%	47.7%	21.3%

Income * Speed at Posted 35 Crosstabulation

% within Income

		Speed at Posted 35			
		Most times	Half of the time	Rarely	Never
Income	Under \$40,000	20.7%	12.0%	41.3%	26.0%
	\$40,000-80,000	8.8%	15.5%	49.0%	26.7%
	Over \$80,000	14.2%	20.2%	51.2%	14.4%
Total		13.4%	17.0%	48.6%	21.0%

Gender * Speed at Posted 35 Crosstabulation

% within Gender

		Speed at Posted 35			
		Most times	Half of the time	Rarely	Never
Gender	Male	16.8%	16.6%	46.2%	20.5%
	Female	12.2%	17.1%	48.6%	22.1%
Total		14.4%	16.9%	47.5%	21.3%

District * Speed at Posted 35 Crosstabulation

% within District

		Speed at Posted 35			
		Most times	Half of the time	Rarely	Never
District	One	13.6%	15.0%	51.4%	20.1%
	Two	18.3%	18.3%	45.5%	17.9%
	Three	11.4%	17.3%	45.6%	25.7%
Total		14.4%	16.9%	47.5%	21.3%

Phone Type * Speed at Posted 35 Crosstabulation

% within Phone Type

		Speed at Posted 35			
		Most times	Half of the time	Rarely	Never
Phone Type	Land	13.0%	16.3%	48.0%	22.7%
	Cell	19.4%	18.9%	45.4%	16.3%
Total		14.4%	16.9%	47.5%	21.3%

Age * Speed at Posted 65 Crosstabulation

% within Age

		Speed at Posted 65			
		Most times	Half the time	Rarely	Never
Age	16-20	25.0%	14.3%	28.6%	32.1%
	21-34	6.8%	23.3%	35.6%	34.2%
	35-49	6.2%	12.4%	46.7%	34.7%
	50-64	4.5%	7.1%	46.9%	41.5%
	65 up	4.2%	2.8%	28.9%	64.1%
Total		5.9%	9.7%	42.5%	42.0%

Income * Speed at Posted 65 Crosstabulation

% within Income

		Speed at Posted 65			
		Most times	Half the time	Rarely	Never
Income	Under \$40,000	5.3%	6.7%	40.0%	48.0%
	\$40,000-80,000	4.4%	8.8%	40.2%	46.6%
	Over \$80,000	4.9%	12.8%	46.6%	35.7%
Total		4.8%	10.2%	43.1%	41.9%

Gender * Speed at Posted 65 Crosstabulation

% within Gender

		Speed at Posted 65			
		Most times	Half the time	Rarely	Never
Gender	Male	7.1%	10.8%	44.1%	37.9%
	Female	5.4%	9.0%	39.8%	45.8%
Total		6.2%	9.9%	41.9%	42.0%

District * Speed at Posted 65 Crosstabulation

% within District

		Speed at Posted 65			
		Most times	Half the time	Rarely	Never
District	One	7.8%	9.2%	41.2%	41.8%
	Two	7.6%	12.6%	44.5%	35.2%
	Three	3.3%	7.8%	40.1%	48.9%
Total		6.2%	9.9%	41.9%	42.0%

Phone Type * Speed at Posted 65 Crosstabulation

% within Phone Type

		Speed at Posted 65			
		Most times	Half the time	Rarely	Never
Phone Type	Land	5.2%	9.3%	42.4%	43.1%
	Cell	9.7%	11.7%	40.3%	38.3%
Total		6.2%	9.9%	41.9%	42.0%

Age * Speed Enforcement Awareness Crosstabulation

% within Age

		Speed Enforcement Awareness	
		Yes	No
Age	16-20	22.2%	77.8%
	21-34	38.4%	61.6%
	35-49	35.2%	64.8%
	50-64	37.3%	62.7%
	65 up	42.3%	57.7%
Total		37.0%	63.0%

Income * Speed Enforcement Awareness Crosstabulation

% within Income

		Speed Enforcement Awareness	
		Yes	No
Income	Under \$40,000	40.0%	60.0%
	\$40,000-80,000	34.5%	65.5%
	Over \$80,000	36.2%	63.8%
Total		36.3%	63.7%

Gender * Speed Enforcement Awareness Crosstabulation

% within Gender

		Speed Enforcement Awareness	
		Yes	No
Gender	Male	39.9%	60.1%
	Female	33.9%	66.1%
Total		36.8%	63.2%

District * Speed Enforcement Awareness Crosstabulation

% within District

		Speed Enforcement Awareness	
		Yes	No
District	One	37.7%	62.3%
	Two	34.2%	65.8%
	Three	38.5%	61.5%
Total		36.8%	63.2%

Phone Type * Speed Enforcement Awareness Crosstabulation

% within Phone Type

		Speed Enforcement Awareness	
		Yes	No
Phone Type	Land	36.7%	63.3%
	Cell	37.2%	62.8%
Total		36.8%	63.2%

Age * Speeding Violation Ticket Chances Crosstabulation

% within Age

		Speeding Violation Ticket Chances				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
Age	16-20	35.7%	17.9%	17.9%	25.0%	3.6%
	21-34	15.1%	45.2%	23.3%	13.7%	2.7%
	35-49	15.5%	36.8%	25.4%	18.6%	3.8%
	50-64	16.4%	34.7%	24.0%	20.6%	4.2%
	65 up	12.0%	33.8%	28.9%	15.5%	9.9%
Total		15.9%	35.6%	25.0%	18.7%	4.8%

Income * Speeding Violation Ticket Chances Crosstabulation

% within Income

		Speeding Violation Ticket Chances				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
Income	Under \$40,000	20.7%	34.0%	26.7%	15.3%	3.3%
	\$40,000-80,000	13.2%	37.8%	25.7%	16.9%	6.4%
	Over \$80,000	15.0%	34.9%	24.0%	22.1%	4.1%
Total		15.4%	35.8%	25.1%	18.9%	4.8%

Gender * Speeding Violation Ticket Chances Crosstabulation

% within Gender

		Speeding Violation Ticket Chances				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
Gender	Male	12.9%	35.6%	24.1%	21.8%	5.5%
	Female	18.8%	35.3%	26.1%	15.4%	4.3%
Total		16.0%	35.5%	25.2%	18.5%	4.9%

District * Speeding Violation Ticket Chances Crosstabulation

% within District

		Speeding Violation Ticket Chances				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
District	One	12.2%	37.4%	25.5%	21.8%	3.1%
	Two	15.0%	37.2%	21.3%	21.3%	5.3%
	Three	20.5%	31.9%	28.7%	12.7%	6.2%
Total		16.0%	35.5%	25.2%	18.5%	4.9%

Phone Type * Speeding Violation Ticket Chances Crosstabulation

% within Phone Type

		Speeding Violation Ticket Chances				
		Very likely	Somewhat likely	Likely	Unlikely	Very unlikely
Phone Type	Land	15.7%	34.0%	26.5%	18.7%	5.1%
	Cell	16.8%	40.8%	20.4%	17.9%	4.1%
Total		16.0%	35.5%	25.2%	18.5%	4.9%

	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Distracted Driving</i>				
31. Would you support or oppose a law banning drivers from talking on a cell phone while driving?				
Support	65	63	69	63
Oppose	32	33	30	34
Don't know	3	4	1	4
32. Would you support or oppose a law that allows law enforcement to stop a driver and ticket them solely for talking on a cell phone while driving?				
Support	61	57	66	60
Oppose	37	42	33	38
Don't know	2	1	1	3
33. Would you support or oppose a law that allows law enforcement to stop a driver and ticket them solely for texting while driving?				
Support	90	87	91	91
Oppose	9	12	8	8
Don't know	1	1	1	2



Age * Ban on Cell Phone Use While Driving Crosstabulation

% within Age

		Ban on Cell Phone Use While Driving		
		Support	Oppose	Don't know
Age	16-20	57.1%	35.7%	7.1%
	21-34	45.2%	52.1%	2.7%
	35-49	55.0%	40.9%	4.1%
	50-64	67.8%	29.7%	2.5%
	65 up	88.7%	10.6%	0.7%
Total		64.8%	32.3%	2.9%

Income * Ban on Cell Phone Use While Driving Crosstabulation

% within Income

		Ban on Cell Phone Use While Driving		
		Support	Oppose	Don't know
Income	Under \$40,000	70.0%	27.3%	2.7%
	\$40,000-80,000	68.2%	28.7%	3.0%
	Over \$80,000	58.9%	38.7%	2.5%
Total		64.3%	33.0%	2.7%

Gender * Ban on Cell Phone Use While Driving Crosstabulation

% within Gender

		Ban on Cell Phone Use While Driving		
		Support	Oppose	Don't know
Gender	Male	58.6%	37.9%	3.4%
	Female	70.2%	27.2%	2.6%
Total		64.6%	32.4%	3.0%

District * Ban on Cell Phone Use While Driving Crosstabulation

% within District

		Ban on Cell Phone Use While Driving		
		Support	Oppose	Don't know
District	One	62.6%	33.0%	4.4%
	Two	68.8%	30.2%	1.0%
	Three	62.5%	33.9%	3.6%
Total		64.6%	32.4%	3.0%

Phone Type * Ban on Cell Phone Use While Driving Crosstabulation

% within Phone Type

		Ban on Cell Phone Use While Driving		
		Support	Oppose	Don't know
Phone Type	Land	67.0%	29.7%	3.3%
	Cell	56.1%	41.8%	2.0%
Total		64.6%	32.4%	3.0%

Age * Ticket for Cell Phone Use While Driving Crosstabulation

% within Age

		Ticket for Cell Phone Use While Driving		
		Support	Oppose	Don't know
Age	16-20	35.7%	53.6%	10.7%
	21-34	42.5%	57.5%	
	35-49	50.9%	47.4%	1.7%
	50-64	64.7%	33.3%	2.0%
	65 up	88.0%	11.3%	0.7%
Total		61.1%	37.0%	1.8%

Income * Ticket for Cell Phone Use While Driving Crosstabulation

% within Income

		Ticket for Cell Phone Use While Driving		
		Support	Oppose	Don't know
Income	Under \$40,000	66.0%	32.7%	1.3%
	\$40,000-80,000	63.9%	33.4%	2.7%
	Over \$80,000	56.7%	42.5%	0.8%
Total		61.0%	37.4%	1.6%

Gender * Ticket for Cell Phone Use While Driving Crosstabulation

% within Gender

		Ticket for Cell Phone Use While Driving		
		Support	Oppose	Don't know
Gender	Male	57.2%	41.6%	1.1%
	Female	64.7%	33.0%	2.4%
Total		61.1%	37.1%	1.8%

District * Ticket for Cell Phone Use While Driving Crosstabulation

% within District

		Ticket for Cell Phone Use While Driving		
		Support	Oppose	Don't know
District	One	57.1%	41.5%	1.4%
	Two	66.1%	32.6%	1.3%
	Three	59.9%	37.5%	2.6%
Total		61.1%	37.1%	1.8%

Phone Type * Ticket for Cell Phone Use While Driving Crosstabulation

% within Phone Type

		Ticket for Cell Phone Use While Driving		
		Support	Oppose	Don't know
Phone Type	Land	64.2%	34.0%	1.8%
	Cell	50.0%	48.5%	1.5%
Total		61.1%	37.1%	1.8%

Age * Ticket for Texting While Driving Crosstabulation

% within Age

		Ticket for Texting While Driving		
		Support	Oppose	Don't know
Age	16-20	67.9%	25.0%	7.1%
	21-34	84.9%	15.1%	
	35-49	91.1%	7.9%	1.0%
	50-64	90.7%	8.5%	0.8%
	65 up	92.3%	7.0%	0.7%
Total		89.9%	9.1%	1.0%

Income * Ticket for Texting While Driving Crosstabulation

% within Income

		Ticket for Texting While Driving		
		Support	Oppose	Don't know
Income	Under \$40,000	90.0%	9.3%	0.7%
	\$40,000-80,000	91.9%	6.8%	1.4%
	Over \$80,000	90.7%	8.7%	0.5%
Total		91.0%	8.1%	0.9%

Gender * Ticket for Texting While Driving Crosstabulation

% within Gender

		Ticket for Texting While Driving		
		Support	Oppose	Don't know
Gender	Male	86.9%	11.7%	1.4%
	Female	92.3%	6.6%	1.1%
Total		89.7%	9.1%	1.2%

District * Ticket for Texting While Driving Crosstabulation

% within District

		Ticket for Texting While Driving		
		Support	Oppose	Don't know
District	One	87.1%	11.9%	1.0%
	Two	91.4%	7.6%	1.0%
	Three	90.6%	7.8%	1.6%
Total		89.7%	9.1%	1.2%

Phone Type * Ticket for Texting While Driving Crosstabulation

% within Phone Type

		Ticket for Texting While Driving		
		Support	Oppose	Don't know
Phone Type	Land	89.5%	9.2%	1.3%
	Cell	90.3%	8.7%	1.0%
Total		89.7%	9.1%	1.2%

	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Miscellaneous</i>				
34. Should the Nebraska law requiring motorcycle helmets be repealed or continued?				
Continued	74	77	74	70
Repealed	22	19	22	25
Don't know	4	4	4	5
35. Should drivers over age 70 be required to drive as part of their test every time they renew their license?				
Yes	64	61	66	64
Don't know	3	3	2	3
No	34	36	32	33



Age * Helmet Law Crosstabulation

% within Age

		Helmet Law		
		Repeal	Don't know	Continue
Age	16-20	14.3%	3.6%	82.1%
	21-34	24.7%	1.4%	74.0%
	35-49	23.4%	4.5%	72.2%
	50-64	25.7%	3.7%	70.6%
	65 up	9.9%	4.2%	85.9%
Total		22.0%	3.8%	74.2%

Income * Helmet Law Crosstabulation

% within Income

		Helmet Law		
		Repeal	Don't know	Continue
Income	Under \$40,000	18.7%	3.3%	78.0%
	\$40,000-80,000	20.3%	3.7%	76.0%
	Over \$80,000	24.5%	3.3%	72.2%
Total		21.9%	3.4%	74.7%

Gender * Helmet Law Crosstabulation

% within Gender

		Helmet Law		
		Repeal	Don't know	Continue
Gender	Male	33.3%	6.2%	60.5%
	Female	11.6%	2.8%	85.7%
Total		22.1%	4.4%	73.5%

District * Helmet Law Crosstabulation

% within District

		Helmet Law		
		Repeal	Don't know	Continue
District	One	19.4%	4.1%	76.5%
	Two	21.6%	4.0%	74.4%
	Three	25.1%	5.2%	69.7%
Total		22.1%	4.4%	73.5%

Phone Type * Helmet Law Crosstabulation

% within Phone Type

		Helmet Law		
		Repeal	Don't know	Continue
Phone Type	Land	20.7%	4.8%	74.5%
	Cell	27.0%	3.1%	69.9%
Total		22.1%	4.4%	73.5%

Age * Older Drivers Testing Law Crosstabulation

% within Age

		Older Drivers Testing Law		
		Yes	Don't know	No
Age	16-20	78.6%	7.1%	14.3%
	21-34	79.5%		20.5%
	35-49	75.3%	2.1%	22.7%
	50-64	59.6%	2.5%	37.9%
	65 up	41.5%	2.8%	55.6%
Total		64.1%	2.4%	33.6%

Income * Older Drivers Testing Law Crosstabulation

% within Income

		Older Drivers Testing Law		
		Yes	Don't know	No
Income	Under \$40,000	66.0%	1.3%	32.7%
	\$40,000-80,000	63.5%	1.4%	35.1%
	Over \$80,000	65.1%	2.7%	32.2%
Total		64.7%	2.0%	33.3%

Gender * Older Drivers Testing Law Crosstabulation

% within Gender

		Older Drivers Testing Law		
		Yes	Don't know	No
Gender	Male	62.3%	2.8%	34.9%
	Female	65.1%	2.6%	32.3%
Total		63.7%	2.7%	33.6%

District * Older Drivers Testing Law Crosstabulation

% within District

		Older Drivers Testing Law		
		Yes	Don't know	No
District	One	61.2%	2.7%	36.1%
	Two	65.8%	2.3%	31.9%
	Three	64.2%	2.9%	32.9%
Total		63.7%	2.7%	33.6%

Phone Type * Older Drivers Testing Law Crosstabulation

% within Phone Type

		Older Drivers Testing Law		
		Yes	Don't know	No
Phone Type	Land	61.0%	2.7%	36.3%
	Cell	73.5%	2.6%	24.0%
Total		63.7%	2.7%	33.6%

	State	Percentage		
		Dist One	Dist Two	Dist Three
<i>Demographics</i>				
36. Which of these age groups are you in:				
16-20	3	4	4	2
21-34	8	9	7	9
35-49	33	32	35	32
50-64	40	42	36	42
65 up	16	13	19	16
37. Do you have a driver's license?				
Yes	98	98	99	98
No	2	2	1	2
38. Which of these annual household income groups are you in? *				
Under \$40,000	19	20	18	17
\$40,000-80,000	36	37	32	41
Over \$80,000	45	43	50	42
39. (NOTE CONGRESSIONAL DISTRICT):				
One	33	100	0	0
Two	33	0	100	0
Three	34	0	0	100
40. (NOTE PHONE TYPE):				
Land line	78	80	81	75
Cell phone	22	20	19	25
41. Gender				
Male	48	48	48	49
Female	52	52	52	51

###



Income * Age Crosstabulation

% within Income

		Age				
		16-20	21-34	35-49	50-64	65 up
Income	Under \$40,000	7.3%	20.7%	20.0%	22.0%	30.0%
	\$40,000-80,000	1.4%	8.1%	33.2%	41.0%	16.3%
	Over \$80,000	1.9%	4.1%	40.3%	45.8%	7.9%
Total		2.7%	8.6%	34.0%	39.6%	15.1%

Gender * Age Crosstabulation

% within Gender

		Age				
		16-20	21-34	35-49	50-64	65 up
Gender	Male	3.8%	10.7%	31.8%	39.3%	14.5%
	Female	2.6%	6.0%	33.7%	40.3%	17.4%
Total		3.2%	8.2%	32.8%	39.9%	16.0%

District * Age Crosstabulation

% within District

		Age				
		16-20	21-34	35-49	50-64	65 up
District	One	3.5%	9.0%	31.9%	42.4%	13.2%
	Two	4.4%	7.0%	34.6%	35.6%	18.5%
	Three	1.7%	8.6%	31.8%	41.7%	16.2%
Total		3.2%	8.2%	32.8%	39.9%	16.0%

Phone Type * Age Crosstabulation

% within Phone Type

		Age				
		16-20	21-34	35-49	50-64	65 up
Phone Type	Land	3.2%	3.3%	32.2%	42.4%	18.9%
	Cell	3.2%	26.3%	34.7%	30.5%	5.3%
Total		3.2%	8.2%	32.8%	39.9%	16.0%

Age * License Crosstabulation

% within Age

		License	
		Yes	No
Age	16-20	96.4%	3.6%
	21-34	98.6%	1.4%
	35-49	100.0%	
	50-64	99.4%	0.6%
	65 up	96.5%	3.5%
Total		99.0%	1.0%

Income * License Crosstabulation

% within Income

		License	
		Yes	No
Income	Under \$40,000	97.3%	2.7%
	\$40,000-80,000	98.6%	1.4%
	Over \$80,000	100.0%	
Total		99.0%	1.0%

Gender * License Crosstabulation

% within Gender

		License	
		Yes	No
Gender	Male	98.4%	1.6%
	Female	98.5%	1.5%
Total		98.4%	1.6%

District * License Crosstabulation

% within District

		License	
		Yes	No
District	One	97.6%	2.4%
	Two	99.3%	0.7%
	Three	98.4%	1.6%
Total		98.4%	1.6%

Phone Type * License Crosstabulation

% within Phone Type

		License	
		Yes	No
Phone Type	Land	98.4%	1.6%
	Cell	98.5%	1.5%
Total		98.4%	1.6%

Age * Income Crosstabulation

% within Age

		Income		
		Under \$40,000	\$40,000-80,000	Over \$80,000
Age	16-20	50.0%	18.2%	31.8%
	21-34	44.3%	34.3%	21.4%
	35-49	10.9%	35.6%	53.5%
	50-64	10.3%	37.7%	52.0%
	65 up	36.9%	39.3%	23.8%
Total		18.5%	36.4%	45.1%

Gender * Income Crosstabulation

% within Gender

		Income		
		Under \$40,000	\$40,000-80,000	Over \$80,000
Gender	Male	15.5%	35.3%	49.2%
	Female	21.2%	37.4%	41.4%
Total		18.5%	36.4%	45.1%

District * Income Crosstabulation

% within District

		Income		
		Under \$40,000	\$40,000-80,000	Over \$80,000
District	One	19.6%	37.0%	43.4%
	Two	18.4%	31.6%	50.0%
	Three	17.4%	40.6%	42.0%
Total		18.5%	36.4%	45.1%

Phone Type * Income Crosstabulation

% within Phone Type

		Income		
		Under \$40,000	\$40,000-80,000	Over \$80,000
Phone Type	Land	15.8%	37.4%	46.8%
	Cell	27.9%	33.0%	39.1%
Total		18.5%	36.4%	45.1%

Age * Gender Crosstabulation

% within Age

		Gender	
		Male	Female
Age	16-20	57.1%	42.9%
	21-34	61.6%	38.4%
	35-49	46.0%	54.0%
	50-64	46.9%	53.1%
	65 up	43.0%	57.0%
Total		47.5%	52.5%

Income * Gender Crosstabulation

% within Income

		Gender	
		Male	Female
Income	Under \$40,000	40.0%	60.0%
	\$40,000-80,000	46.3%	53.7%
	Over \$80,000	52.0%	48.0%
Total		47.7%	52.3%

District * Gender Crosstabulation

% within District

		Gender	
		Male	Female
District	One	47.6%	52.4%
	Two	48.2%	51.8%
	Three	48.9%	51.1%
Total		48.2%	51.8%

Phone Type * Gender Crosstabulation

% within Phone Type

		Gender	
		Male	Female
Phone Type	Land	45.6%	54.4%
	Cell	57.7%	42.3%
Total		48.2%	51.8%

Age * District Crosstabulation

% within Age

		District		
		One	Two	Three
Age	16-20	35.7%	46.4%	17.9%
	21-34	35.6%	28.8%	35.6%
	35-49	31.6%	35.4%	33.0%
	50-64	34.5%	29.9%	35.6%
	65 up	26.8%	38.7%	34.5%
Total		32.4%	33.6%	34.0%

Income * District Crosstabulation

% within Income

		District		
		One	Two	Three
Income	Under \$40,000	34.7%	33.3%	32.0%
	\$40,000-80,000	33.1%	29.1%	37.8%
	Over \$80,000	31.3%	37.1%	31.6%
Total		32.6%	33.5%	33.9%

Gender * District Crosstabulation

% within Gender

		District		
		One	Two	Three
Gender	Male	32.2%	33.3%	34.5%
	Female	33.0%	33.4%	33.6%
Total		32.6%	33.4%	34.0%

Phone Type * District Crosstabulation

% within Phone Type

		District		
		One	Two	Three
Phone Type	Land	33.1%	34.4%	32.4%
	Cell	30.6%	29.6%	39.8%
Total		32.6%	33.4%	34.0%

Age * Phone Type Crosstabulation

% within Age

		Phone Type	
		Land	Cell
Age	16-20	78.6%	21.4%
	21-34	31.5%	68.5%
	35-49	77.3%	22.7%
	50-64	83.6%	16.4%
	65 up	93.0%	7.0%
Total		78.6%	21.4%

Income * Phone Type Crosstabulation

% within Income

		Phone Type	
		Land	Cell
Income	Under \$40,000	66.7%	33.3%
	\$40,000-80,000	80.1%	19.9%
	Over \$80,000	80.9%	19.1%
Total		78.0%	22.0%

Gender * Phone Type Crosstabulation

% within Gender

		Phone Type	
		Land	Cell
Gender	Male	74.0%	26.0%
	Female	82.2%	17.8%
Total		78.3%	21.7%

District * Phone Type Crosstabulation

% within District

		Phone Type	
		Land	Cell
District	One	79.6%	20.4%
	Two	80.7%	19.3%
	Three	74.6%	25.4%
Total		78.3%	21.7%

Nebraska Annual Traffic Safety Questionnaire, 2015 (The Nebraska Poll)

Hello, this is _____ calling for The Nebraska Poll. We're doing a statewide study on traffic safety. I'm not selling anything, I'm only interested in your opinions, and I need to talk to someone 16 years of age or older in your household. Is that you? Good.

1. First, what do you think is the biggest problem in Nebraska today? (DO NOT READ)
- 1) Economy/drought/water/jobs/wages, etc.
 - 2) Crime/drugs/gangs/vandalism, etc.
 - 3) Taxes/spending/budget, education funding, etc.
 - 5) Roads/streets/traffic, etc.
 - 6) Distracted driving
 - 7) Drunk drivers
 - 8) Government, politicians, etc.
 - 0) Others (specify)
 - 9) DK/NR

On a 1-5 scale, with 5 as very important and 1 as not at all important, how would you rate the following problems in Nebraska:

2. ___How important is the fuel tax situation?
3. ___How important is the problem of distraction caused by drivers using cell phones or other electronic devices?
4. ___How important is the problem of driving while intoxicated or drunk driving?

5. What do you think is the best way to solve the drunk driving problem? (DO NOT READ)
- 1) More severe penalties
 - 2) Education
 - 3) Stricter law enforcement
 - 4) High visibility law enforcement
 - 5) Mandatory sentences for convictions
 - 6) Others
 - 9) DK/NR

6. Do you think the Nebraska penalties for drunk driving are:(READ):
- 1) Too tough
 - 2) About right
 - or 3) Not tough enough?

Would you favor or oppose each of the following penalties for drunk driving: (ROTATE 7-12)

- 7. Lose license for first offense 1) Favor 2) Don't know 3) Oppose
- 8. Jail terms for previous offenses 1) Favor 2) Don't know 3) Oppose
- 9. Mandatory sentencing for drunk driving offenses 1) Favor 2) Don't know 3) Oppose
- 10. Eliminating plea bargaining for drunk driving offenses 1) Favor 2) Don't know 3) Oppose
- 11. Mandatory treatment for drunk driving offenders 1) Favor 2) Don't know 3) Oppose
- 12. Mandatory interlock ignition for all first-time offenders 1) Favor 2) Don't know 3) Oppose

13. In the last 60 days, have you ever driven while impaired by alcohol, but still under the legal limit? 1) Yes 2) No

14. What do you think the chances are of someone getting arrested if they drive after drinking? (Core Q. A-3)
(READ) 1) Very likely 2) Somewhat likely 3) Likely 4) Somewhat unlikely 5) Very unlikely

15. In the past 60 days, how many times have you driven a motor vehicle within 2 hours after drinking alcoholic beverages?
(Core Q. A-1) 1) Never 2) Once 3) 2-5 times 4) More than 5 times

16. In the past 30 days, have you read, seen or heard anything about alcohol impaired driving (or drunk driving) enforcement by police? (Core Q. A-2) 1) Yes 2) Don't know 3) No

17. In the past 60 days, have you read, seen or heard any drunk driving-related messages?
1) Yes 2) Don't know 3) No

18. How often do you use safety belts when you drive or ride in a car, van, sport utility vehicle or pickup? (Core Q. B-1)
(READ) 1) Always 2) Nearly always 3) Sometimes 4) Seldom 5) Never

19. Should law enforcement officers be allowed to stop drivers and ticket them for not wearing a seat belt?
1) Yes 2) Don't know 3) No

20. What do you think the chances are of getting a ticket if you don't wear your safety belt? (Core Q. B-3)
(READ): 1) Very likely 2) Somewhat likely 3) Likely 4) Unlikely 5) Very unlikely

21. What level of fine do you think would be most effective to get people to wear their seat belt all the time? (READ)
1) \$25-50 2) \$51-100 3) \$101-150 4) \$151-200 5) Over \$200

22. Do you have children weighing between 40 and 80 pounds? 1) Yes 2) No

23. (IF YES) Do you use a booster seat for your child in the car? 1) Yes 2) No

24. In the past 60 days, have you read, seen or heard anything about seat belt law enforcement by police? (Core Q. B-2)
1) Yes 2) Don't know 3) No
25. In the past 60 days, have you read, seen or heard any Click It Or Ticket seat belt messages?
1) Yes 2) Don't know 3) No
26. (IF YES) Do you remember where that was? (OPEN ENDED--DO NOT READ) 1) TV 2) Radio
9) Don't know 0) All other (specify)
-
27. On a local road with a speed limit of 30 mph, how often do you drive faster than 35 mph: (READ)
1) Most of the time 2) Half the time 3) Rarely 4) Never (Core Q. S-1a)
28. On a local road with a speed limit of 65 mph, how often do you drive faster than 70 mph: (READ)
1) Most of the time 2) Half the time 3) Rarely 4) Never (Core Q. S-1b)
29. In the past 30 days, have you read, seen or heard anything about speed enforcement by police? (Core Q. S-2)
1) Yes 2) Don't know 3) No
30. What do you think the chances are of getting a ticket if you drive over the speed limit? (Core Q. S-3)
(READ): 1) Very likely 2) Somewhat likely 3) Likely 4) Unlikely 5) Very unlikely
-
31. Would you support or oppose a law banning drivers from talking on a cell phone while driving?
1) Support 2) Oppose 3) Don't know
32. Would you support or oppose a law that allows law enforcement to stop a driver and ticket them solely for talking on a cell phone while driving?
1) Support 2) Oppose 3) Don't know
33. Would you support or oppose a law that allows law enforcement to stop a driver and ticket them solely for texting while driving?
1) Support 2) Oppose 3) Don't know
-
34. Should the Nebraska law requiring motorcycle helmets be repealed or continued?
1) Repealed 2) Don't know 3) Continued
35. Should drivers over age 70 be required to drive as part of their test every time they renew their license?
1) Yes 2) Don't know 3) No
-
36. Which of these age groups are you in: (READ):
1) 16-20 2) 21-34 3) 35-49 4) 50-64 5) 65 up
37. Do you have a driver's license? 1) Yes 2) No
38. Which of these annual household income groups are you in? (READ):
1) Under \$40,000 2) \$40,000-80,000 3) Over \$80,000? NR
39. (NOTE CONGRESSIONAL DISTRICT): 1) One 2) Two 3) Three
40. (NOTE PHONE TYPE): 1) Land line 2) Cell phone
41. (NOTE GENDER): 1) Male 2) Female (quota: 48% males)

OK, that's it! Thanks very much for your time and help!

NHS/NSC ATS '15